

Report to the Future Melbourne (Planning) Committee

Agenda item 6.8

**Application for Planning Permit: TP-2014-43
Victoria Harbour Precinct, 839-899 Collins Street, Docklands**

6 May 2014

Presenter: Martin Williams, Executive Officer Planning

Purpose and background

1. The purpose of the report is to advise the Committee of an application for use and development of land for a pleasure boat facility and family services hub at 839-899 Collins Street, Docklands. The application proposes a two storey mixed use community building comprising a boating storage area with associated amenities on the ground floor and a family services and multi-purpose function facility on the first floor (refer Attachment 2 – Locality Plan and Attachment 3 – Proposed Plans).
2. The applicant is Contour Consultants Pty Ltd on behalf of Lend Lease Development. The owner is Places Victoria and the architect is Hayball Architects.
3. Notice of the application was given by ordinary mail to the owners and occupiers of surrounding properties and by posting one notice on the site for a 14 day period, in accordance with Section 52 of the *Planning and Environment Act 1987*. No objections have been received.

Key issues

4. The key issues raised by this application are the proposed architectural design response and the layout and circulation arrangements around the building.
5. It is considered that the design and layout of the proposed development provides an appropriate design element which generally satisfies the heights and setbacks requirements and built form design principles of the Victoria Harbour Development Plan (2010).
6. The proposed uses (place of assembly, informal outdoor recreation, medical centre and road) all conform with the uses prescribed in the Victoria Harbour Development Plan (2010) and the context and locational attributes of the site make it highly suitable for these purposes.
7. The size of the building and its architecturally designed commercial appearance will make a substantial contribution to activating Victoria Harbour and accentuating Dock Square and the corner of Bourke and Collins Streets. The design, materials and extent of articulation distinguish the building from the appearance of other building typologies, to create an attractive and inviting community building.
8. Changes are required to the proposed access arrangements, particularly the clearances from built structures (i.e. roof over hangs, lights and street furniture) and the layout and circulation arrangements along the Promenade.
9. Conditions have been included on the permit to ensure that adequate clearances are provided so that vehicles (cars, boat trailers and service vehicles) can manoeuvre around the building without being obstructed by buildings or street furniture.
10. Traffic volumes are expected to be low, and will initially be limited to the unloading of boats and service vehicles. The future unloading and loading of boats for regattas and boating events will occur via the proposed boating ramp to be located at Western Park, on the opposite side of Victoria Harbour with access from Docklands Drive.

Recommendation from management

11. That the Future Melbourne Committee resolves to issue a Planning Permit subject to the conditions included in the delegate's report (refer Attachment 4 – Delegate's Report).

Attachments:

1. Supporting Attachment
2. Locality Plan
3. Proposed Plans
4. Delegate Report

Supporting Attachment

Legal

1. Division 1 of Part 4 of the *Planning and Environment Act 1987* sets out the requirements in relation to applications for permits pursuant to the relevant planning scheme.

Finance

2. There are no direct financial issues arising from the recommendation contained in this report

Conflict of interest

3. No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a direct or indirect interest in relation to the matter of the report.

Stakeholder consultation

4. Formal notification (advertising of the planning application) was carried out for the application. No objections were received.

Relation to Council policy

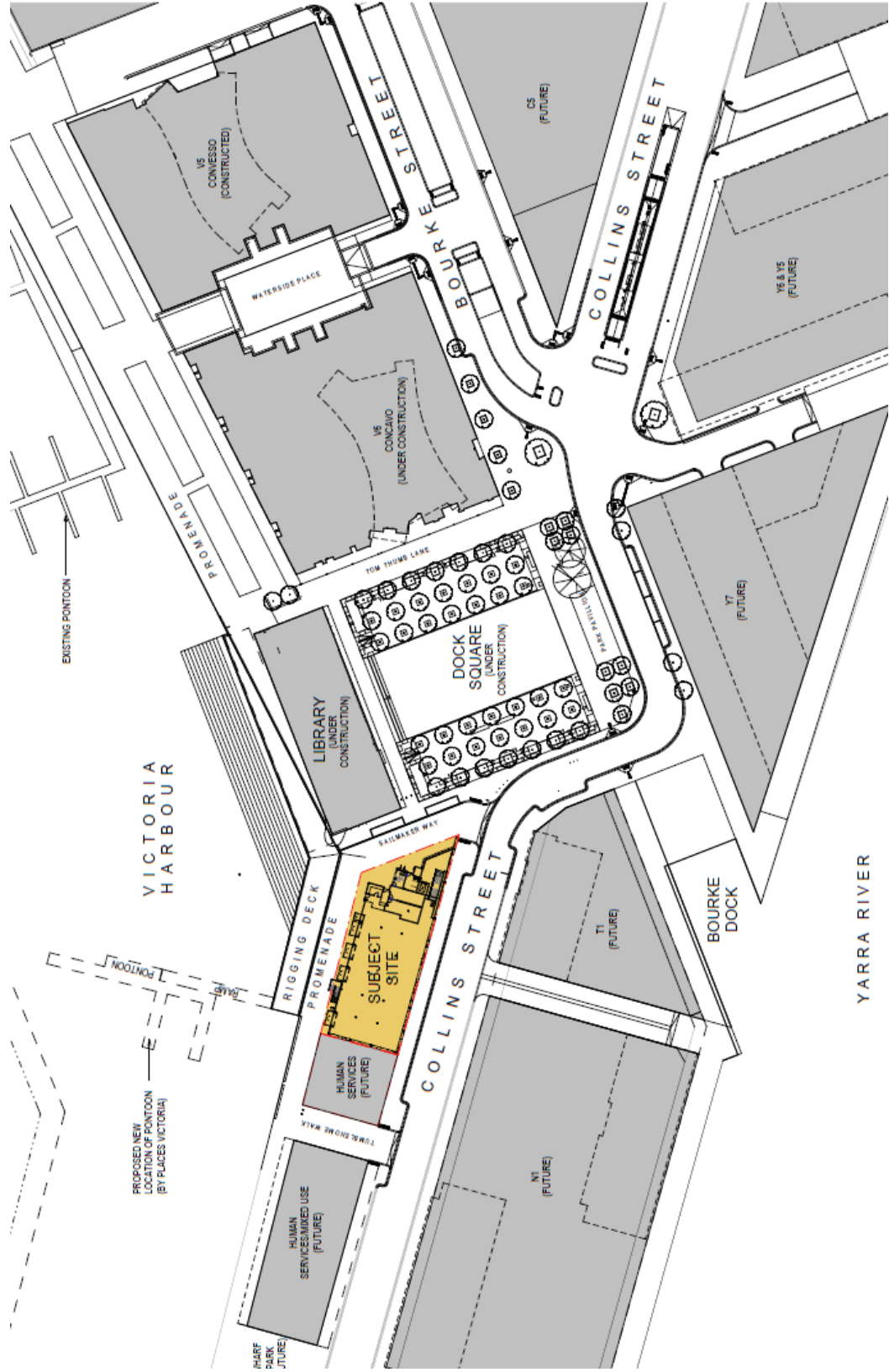
5. Relevant Council policies are discussed in the attached officer report (refer Attachment 4).

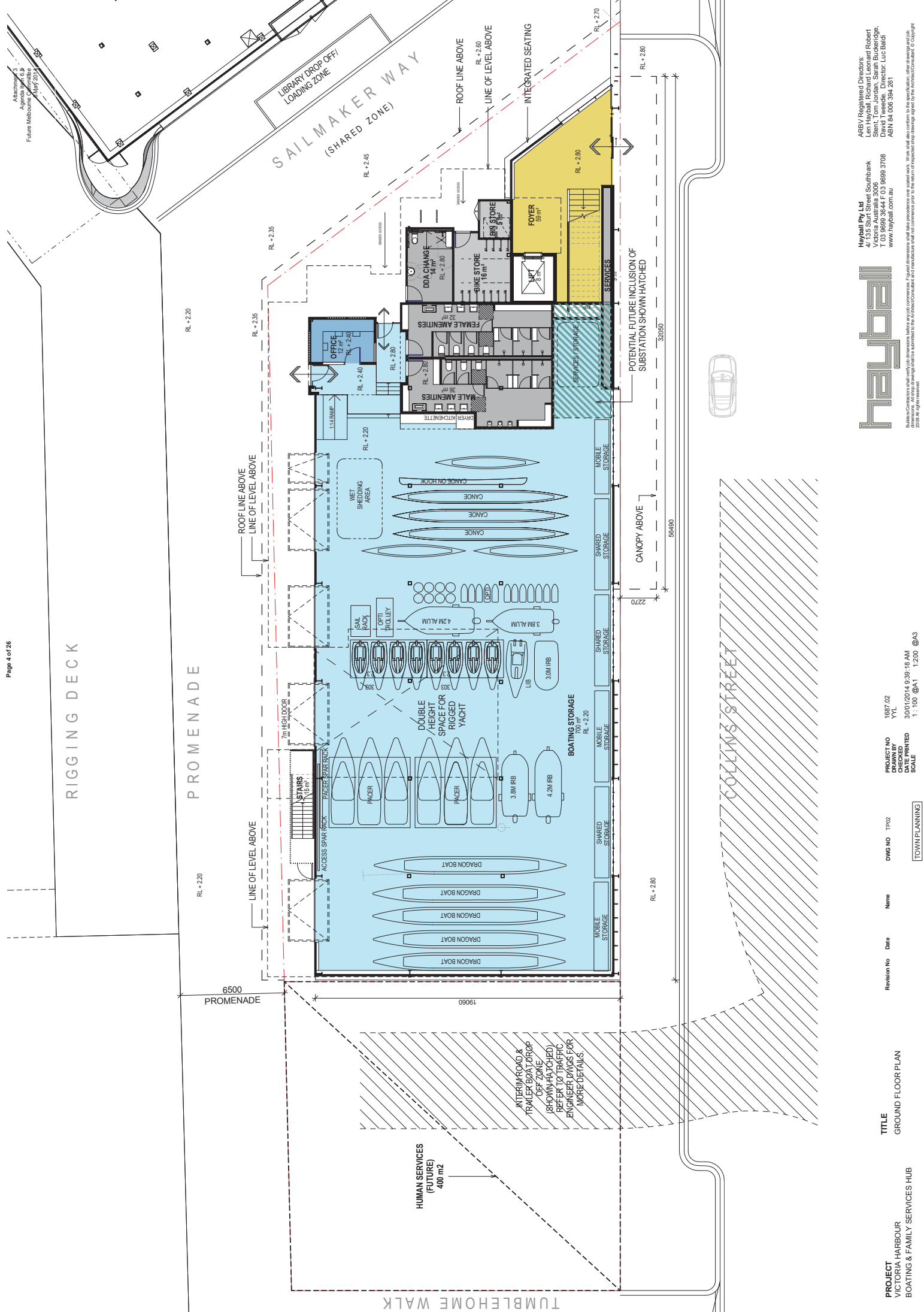
Environmental sustainability

6. Environmental sustainability is discussed in the attached officer report (refer Attachment 4).

Locality Plan

839-899 Collins Street, Docklands





Attachment 3
 Approved on 16/05/2024
 Future Mechanism
 16/05/2024

LIBRARY DROP OFF/
 LOADING ZONE

SAILMAKER WAY
 (SHARED ZONE)

RIGGING DECK

PROMENADE

COLLINS STREET

TUMBLEHOME WALK

HUMAN SERVICES
 (FUTURE)
 400 m2

INTERIM ROAD &
 TRAILER BOAT DROP
 OFF ZONE
 (BROWN-HATCHED)
 REFER TO TRAFFIC
 ENGINEER DRAWS FOR
 MORE DETAILS.

POTENTIAL FUTURE INCLUSION OF
 SUBSTATION SHOWN HATCHED

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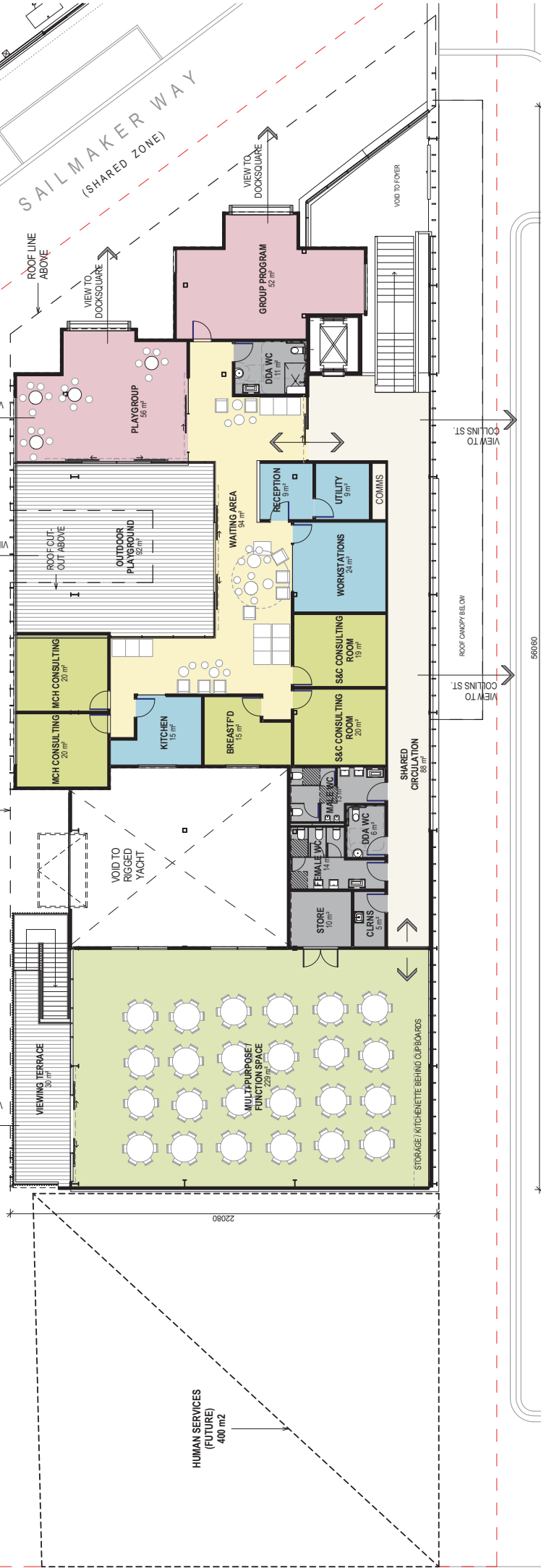
TITLE
 GROUND FLOOR PLAN
 PROJECT
 VICTORIA HARBOUR
 BOATING & FAMILY SERVICES HUB

RIGGING DECK

PROMENADE

COLLINS STREET

TUMBLEHOME WALK



HUMAN SERVICES
(FUTURE)
400 m2

56060

22080

TITLE
LEVEL 1 PLAN

PROJECT
VICTORIA-HARBOUR
BOATING & FAMILY SERVICES HUB

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Submitter declares that this floor plan and any other documents submitted for assessment are true and correct. The documents are not to be used for any other purpose without the prior written consent of the architect. The architect accepts no responsibility for the accuracy or completeness of the information provided. The architect is not responsible for the accuracy or completeness of the information provided. The architect is not responsible for the accuracy or completeness of the information provided.

RIGGING DECK

PROMENADE

SAILMAKER WAY
(SHARED ZONE)

COLLINS STREET

TUMBLEHOME WALK

HUMAN SERVICES
(FUTURE)
400 m2



PROJECT
VICTORIA HARBOUR
BOATING & FAMILY SERVICES HUB

TITLE
ROOF PLAN

Revision No.	Date	Name
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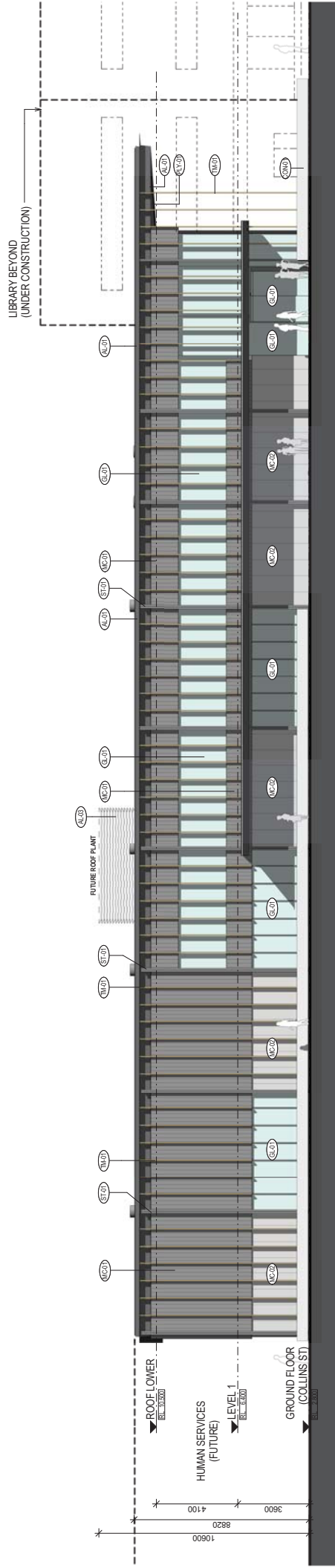
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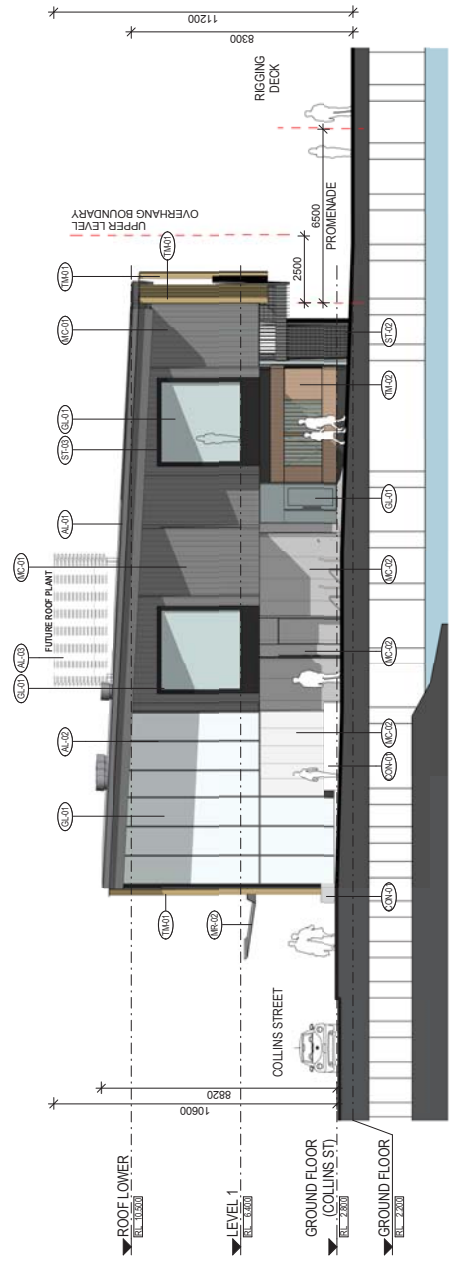


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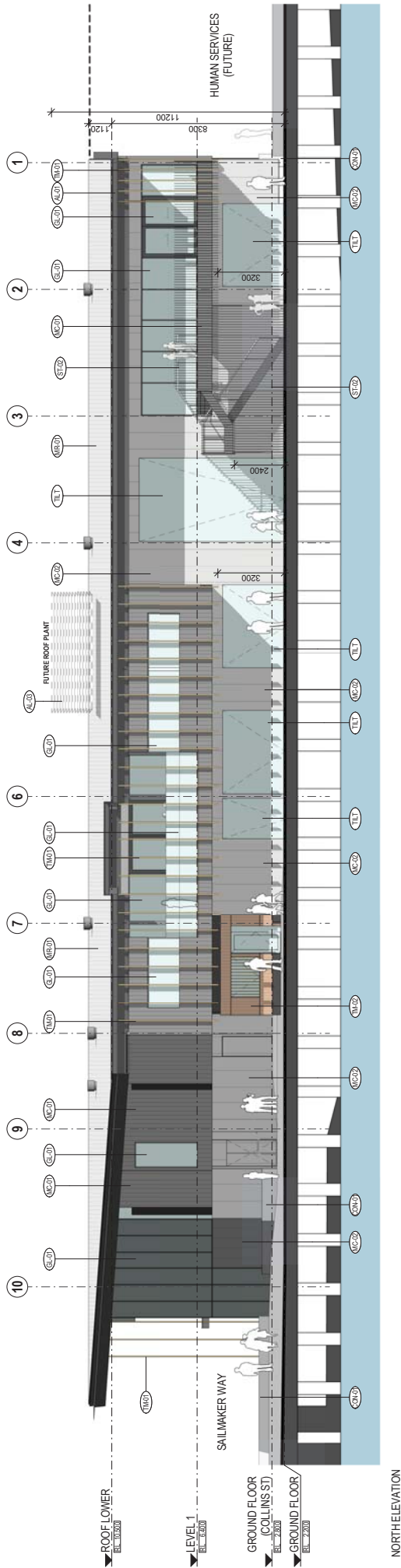


SOUTH ELEVATION

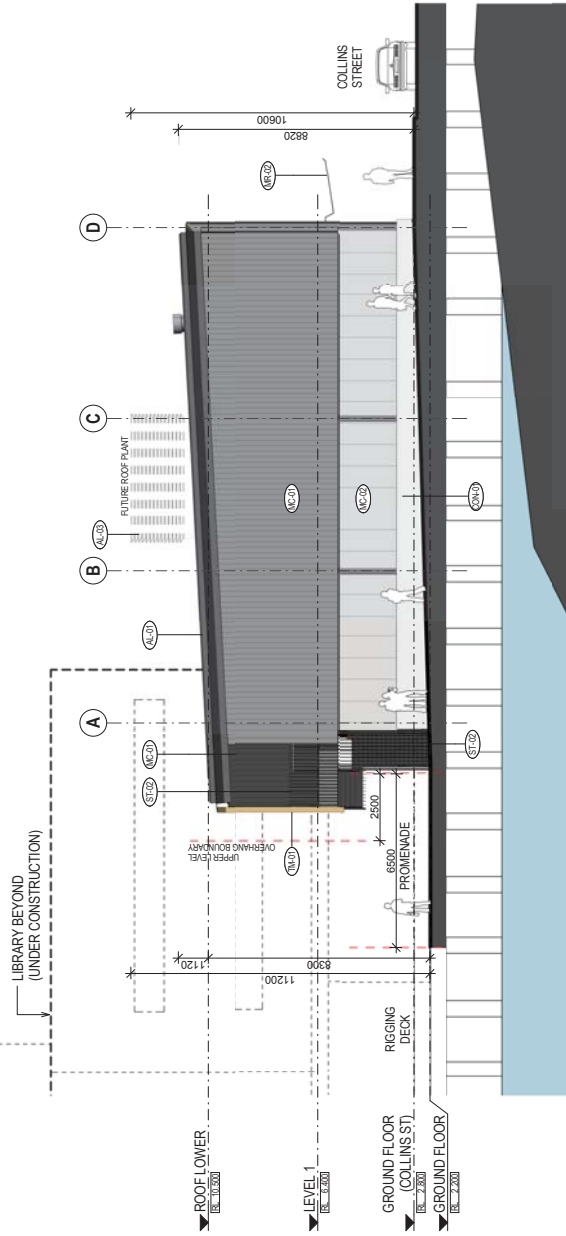
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AL 01	Aluminum Roof Capping and Fascia	
AL 02	Powdercoated Aluminum Window Framing	
AL 03	Powdercoated Aluminum Louvers	
CON 01	Cast In Situ Concrete	
GL 01	Clear Glass Facade Glazing and Doors	
MC 01	Profiled Metal Cladding	
MR 01	Metal Roofing in Selected Colour	
MR 02	Metal Roofing Canopy in Selected Colour	
PLY 01	External grade plywood	
ST 01	Epoxy Structural Steel Framing	
ST 02	Steel Balustrade	
ST 03	Metal Clad Window E. Insulation	
TILT	Tilt up door panel Combined polycarbonate and glazing panels	
TM 01	Pre-weathered Cypress Pine Timber Blades	
TM 02	Timber Cladding	



EAST ELEVATION



NORTHELEVATION



WEST ELEVATION

TP MATERIALS AND FINISHES SCHEDULE	
CODES	TYPE/LOCATION
AL01	Aluminium Roof Capping and Fascia
AL02	Powdercoated Aluminium Window Framing
AL03	Powdercoated Aluminium Louvres
CON01	Cast In Situ Concrete
GL01	Clear Glass Facade Glazing and Doors
MC01	Profiled Metal Cladding
MC02	Folded Profiled Metal Cladding
MR01	Metal Roofing in Selected Colour
MR02	Metal Roofing Canopy in Selected Colour
FLY01	External grade plywood
ST01	External Structural Steel Framing
ST02	Steel Balustrade
ST03	Metal Clad Window E. Trusion
TILT	Tilt up door panel
TILT01	Combined polycarbonate and glazing panels
TILT02	Pre-weathered Cypress Pine Timber Blakes
TM02	Timber Cladding

PROJECT
VICTORIA HARBOUR
BOATING & FAMILY SERVICES HUB

TITLE
ELEVATIONS

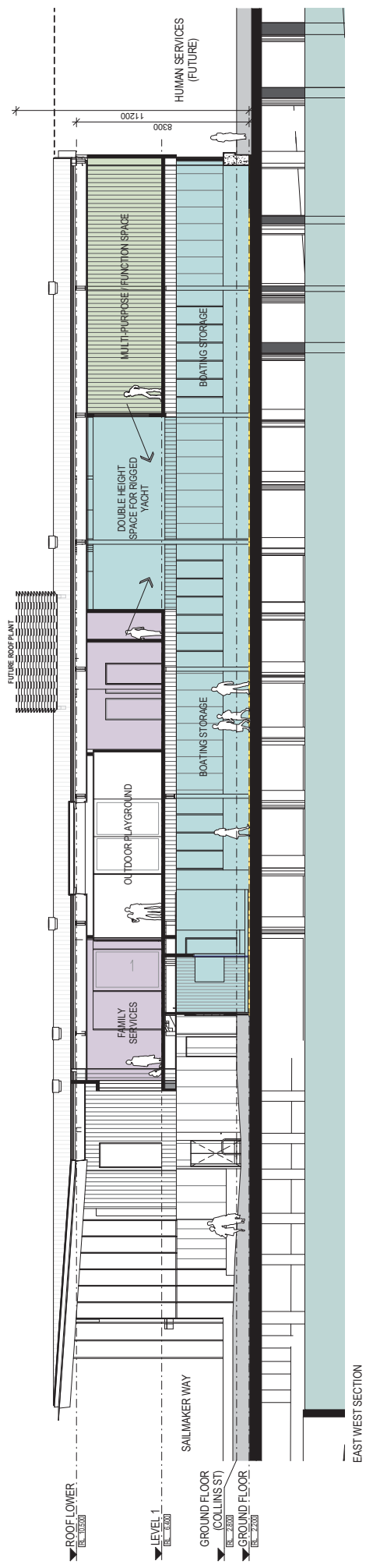
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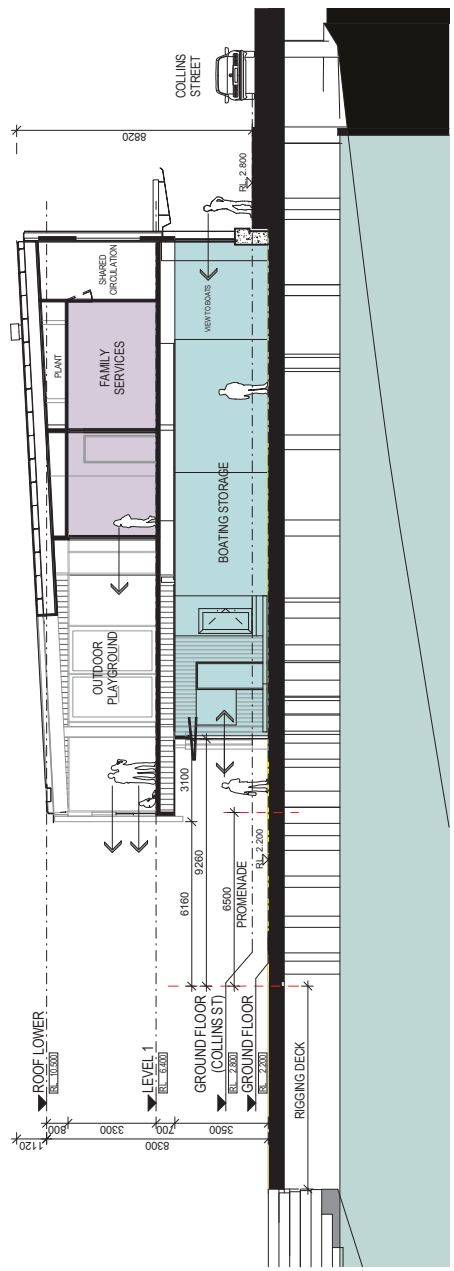
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EAST WEST SECTION



NORTH SOUTH SECTION

DELEGATED PLANNING APPLICATION REPORT

Application number:	TP-2014-43
Applicant:	Lend Lease Development Pty Ltd
Address:	Victoria Harbour Precinct, 839 Collins Street, DOCKLANDS VIC 3008
Proposal:	Use and development of the land for a pleasure boat facility and family services hub in accordance with the plans and information submitted.
Date of application:	31 January 2014
Responsible officer:	Blair Mather

1 SUBJECT SITE AND SURROUNDS

The subject site forms part of the Collins Street Wharf and is located within the Victoria Harbour Precinct in Docklands at the western end of Collins Street. The wharf is located on the Yarra River and separates the river from Victoria Harbour.

Figure 1 below shows the location of the subject site – the red dotted line denotes the extent of the planning application.

FIGURE 1 – LOCATION PLAN

The site is developed in part, by a large marine industrial building known as shed 4, which currently sits vacant. A permit was granted in October 2013 to partially demolish the building to allow for the future development of this land.

Directly to the east are the recently constructed Docklands Library and Dock Square. Dock Square is an urban park with seating, large turf areas and groves of trees and marks the intersection of Bourke and Collins Streets. A tram stop is proposed to be located in close proximity to the Square.

Directly to the north is Waterfront Promenade and Victoria Harbour. Waterfront Promenade is approximately 6.5 metres wide.

Directly to the south is Collins Street which is currently unconstructed in this location. The opposite side of Collins Street is currently vacant however the Victoria Harbour Development Plan foreshadows residential development (as detailed below) on this site.

2 BACKGROUND AND HISTORY

2.1 Pre-application discussions

Pre-application meetings involved all statutory authorities including the CoM and Places Victoria. The key issues raised at the pre-application meeting were:

- Servicing and loading arrangements including interim arrangements whilst boating hub is still being completed.
- Wind assessment and resolution of wind issues.
- Transparency of glazing at ground floor level.
- Public realm responses.
- Building setback in relation to the promenade.
- Treatment of roofscape (fifth elevation) is critical due to surrounding buildings looking down onto proposal. Therefore any servicing requirements (roofplant) must be identified early in the process to ensure it can be integrated into the overall building design.
- The design of the promenade should be considered in context of the Development Plan and details of how the boating facilities will impact the promenade space i.e. number of boat movements across promenade etc should be identified.
- The Collins Street elevation (southern) lacks the quality of interest, materials and articulation of that of the northern elevation.

2.2 Amendments during the process

The application has not been amended during the process.

2.3 Planning Application History

The following applications, listed as considered relevant to the current proposal, have previously been considered for the subject site:

TP Number	Description of Proposal	Decision & Date of Decision
TP-2013-790	Part demolition of an existing building	Permit 16/10/2013
TPD-2010-37	Victoria Harbour Development Plan 2010, Docklands	Completed 21/1/2011

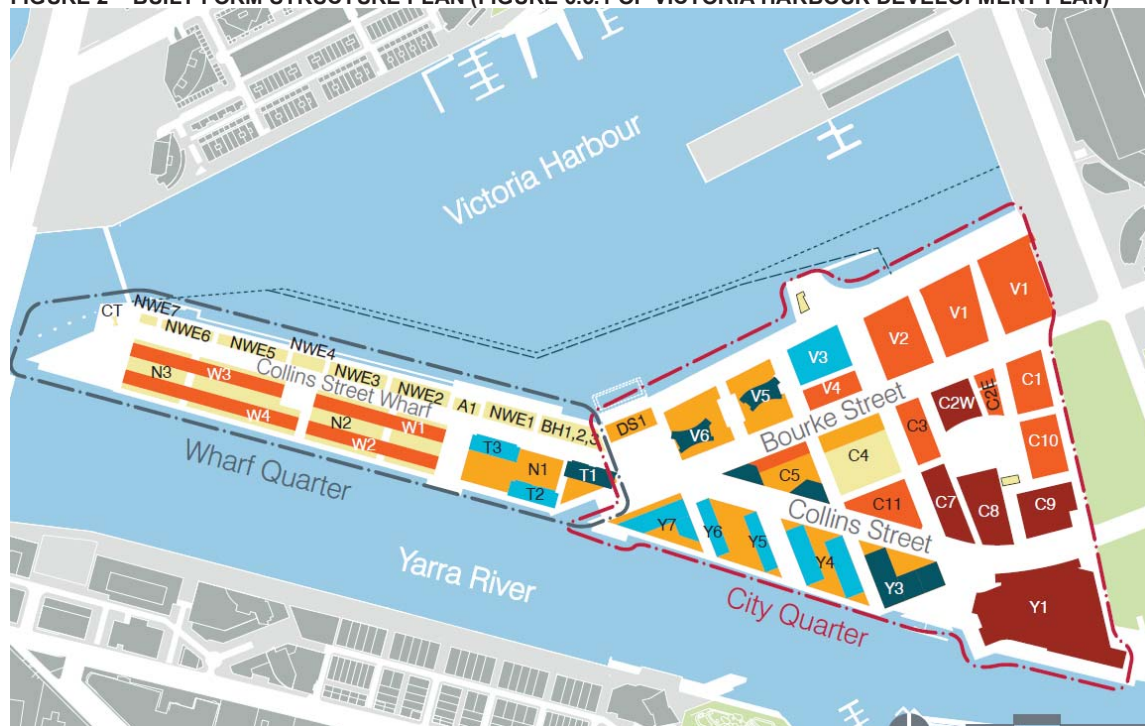
In addition, the following development approvals are in place surrounding the subject site:

- DS1 - 839-899 Collins Street, Docklands
Buildings and works for the development of land for an informal recreation area, library, food and drink premises and public realm works to create Dock Square, Bourke Dock and extensions to Collins and Bourke Streets (nearing completion).
- V6 – 870-892 Bourke Street, Docklands
The site at V6 has been approved for development and construction has commenced. The development comprises a 31 level building for residential purposes with ground floor retail development. Several retail tenancies face onto Dock Square and are earmarked for food and drink premises.

Also adjoining the subject site are a number of future development sites. The Victoria Harbour Development Plan provides the following guidance in relation to built form and use outcomes:

- N1 – (No formal address to date) Maximum height 110m - Residential development comprising 486 apartments (includes T1, T2 and T3).
- Y7 – (No formal address to date) Maximum height 90m, 250 apartments/hotel rooms.
- NWE1 – (No formal address to date) Maximum height 12m, future potential human services development.

FIGURE 2 – BUILT FORM STRUCTURE PLAN (FIGURE 6.5.1 OF VICTORIA HARBOUR DEVELOPMENT PLAN)



3 PROPOSAL

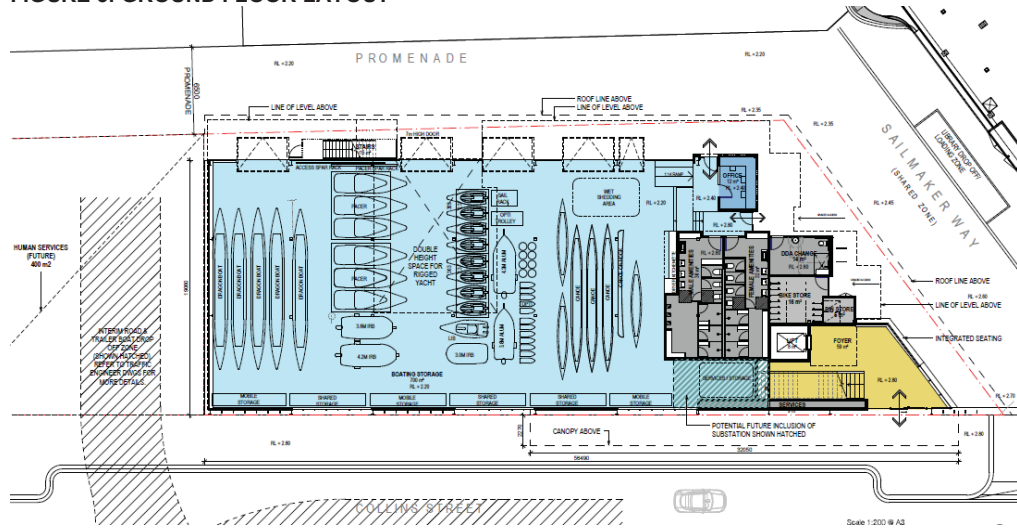
The application seeks approval to construct a two storey building comprising a boating and storage facility at ground floor level and a family services hub (parenting, health and development) on the first floor level. Key features of the proposal are as follows:

3.1 Proposed layout

The ground floor level will comprise a double height common lobby accessed from the corner of Sailmaker Way and Collins Street, approximately 712 square metres of

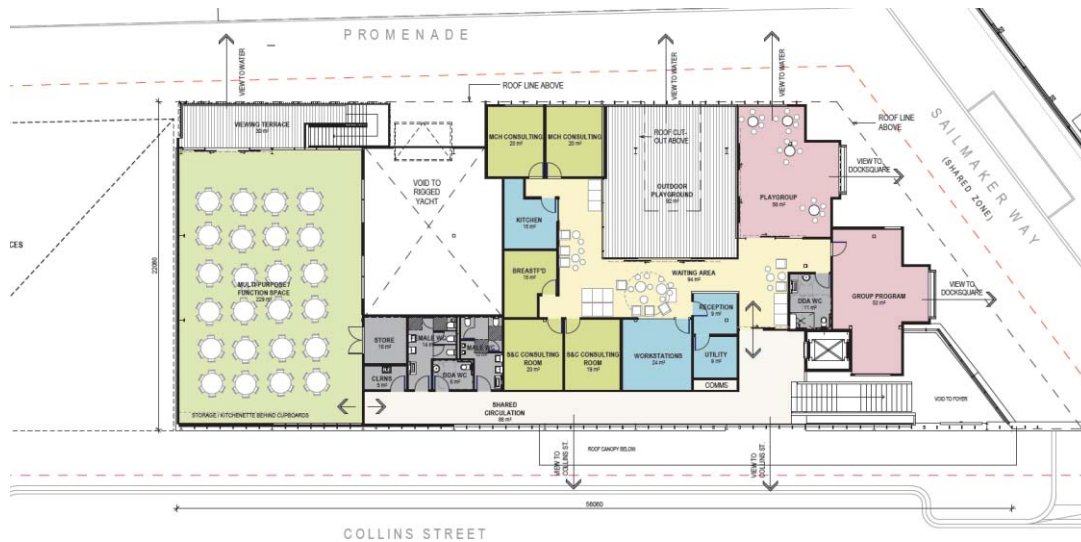
boating storage and associated office and amenities (toilets, showers, bike storage and waste storage). The ground floor level is predominantly open plan and allows for flexibility in the configuration of storage for water craft and equipment and also features a double height space for the provision of masted yachts. Eleven on-site bicycle parking spaces are proposed.

FIGURE 3: GROUND FLOOR LAYOUT



Level 1 contains a 229 square metre multi-purpose/function space and a 540 square metre family services hub comprising a reception area, consulting rooms, play-group room, program room and outdoor playground. The function space is open plan and is provided with direct access to a 30 square metre viewing terrace. Centrally location bathroom facilities serve both the function space and the family services hub.

FIGURE 4: FIRST FLOOR LAYOUT



3.2 Building Style and height

The proposed building style will be contemporary and includes a mix of external materials and finishes including feature timber blades, in-situ concrete, folded metal, exterior grade plywood, steel balustrades, and large window proportions.

The floor plate of the building forms a right angle trapezoid and is 56.5 metres in length and 19.1 metres in depth. The overall building height is 8.82 metres above wharf level, with plant equipment extending an additional 1.8 metres.

The northern elevation of the building features a series of tilt doors which provide access to the promenade and the rigging deck beyond. The first floor level cantilevers over the ground floor to the north and west to provide shelter and articulation.

A stair enclosure at the western end of the northern elevation provides access to a viewing terrace and the upper level of the building.

The southern elevation of the building provides a consistent built form with no setbacks from the title boundary. The ground floor level features large glass panels and allows views through the interior of the building to the waterfront beyond.

A canopy structure will extend along the southern elevation to provide weather protection to the pedestrian entrance and along part of the Collins Street frontage. The canopy sits between 3.0 and 3.7 metres above street level.

3.3 Access

Access to the Promenade via car and boat trailers will be gained via an unnamed laneway located to the west of the site. The laneway is referred to in the application documents as Trumblehome Walk. Trumblehome Walk will be restricted to one-way access by authorised vehicles only and signed as a shared zone.

Waste collection is proposed to take place on-street, along Sailmaker Way.

3.4 Public Realm Improvements

The public realm is to be delivered over two stages as the land to the west is progressively developed in accordance with the Victoria Harbour Development Plan. The interim public realm design response includes the following:

- The completion of Sailmaker Way to the eastern building line in bluestone pavement.
- The construction of the Collins Street footpath up to and adjacent to the entrance of the building.
- The construction of timber edge and seating to the edge of the Promenade.
- The provision of street lighting and bins on the northern side of Collins Street in accordance with City of Melbourne's standards for Docklands.
- The construction of a temporary extension of Collins Street and a roundabout
- The final public realm design response includes the following:
 - The construction of Collins Street within the bounds of the subject site including the construction of the bluestone pavements and car park bays to the north and south side of Collins Street.
 - The provision of street furniture including lighting, bins, seats and bike hoops along Collins Street in accordance with City of Melbourne's standards for Docklands.
 - The provision of street trees along the south side of Collins Street.

Note: the rigging deck has already been constructed as part of the public realm improvements approved under permit for the Docklands Library.

4 STATUTORY CONTROLS

The following clauses in the Melbourne Planning Scheme require a planning permit for this proposal:

Clause	Permit Trigger
<p>Clause 37.05 - Docklands Zone Schedule 2 (Victoria Harbour)</p>	<p>A planning permit is required to construct a building or construct or carry out works. Pursuant to Clause 37.05 a permit is required to use the land for an office.</p> <p>The use of the land for a place of assembly and informal outdoor recreation are Section 1 Uses and do not require a permit.</p>
<p>Clause 43.01 - Heritage Overlay Schedule 915</p>	<p>A permit is not required to develop a place that is listed on the Victoria Heritage Register.</p> <p>Victoria Harbour is listed on the Victorian Heritage Register (harbour including the wharf edge), so a heritage permit is required to extend the promenade into the harbour. An application has been made to Heritage Victoria by the applicant and a permit has been issued.</p>
<p>Clause 43.02 - Design and Development Overlay (DDO50 – Area 7)</p>	<p>Schedule 50 – Area 7</p> <p>A permit is not required to construct a building or construct or carry out works if the requirements of Table 1 and Table 2 to this schedule are met.</p> <p>Table 1 states that for Area 7, the maximum building height is 15 metres with the exception of one building not exceeding 25 metres at the eastern portion of this area.</p> <p>Table 2 states that the minimum widths of waterfront promenades is 6.5 metre wide waterfront promenade with the eastern end promenade to have an average width of 15 metres.</p> <p>The proposal meets the requirements of table 1 and 2, so a permit is not required.</p>
<p>Clause 43.02 - Design and development overlay (DDO12)</p>	<p>Schedule 12:</p> <p>Schedule 12 to the DDO states that a permit is not required for buildings and works other than buildings and works associated with new, refurbished or converted developments for noise sensitive uses. The proposal does not include a noise sensitive use.</p>
<p>Clause 43.04 - Development Plan Overlay 9 Victoria Harbour)</p>	<p>The Victoria Harbour Development Plan 2010, approved on the 7 March 2011 is the approved development plan. A permit is required. A permit granted must be generally in accordance with the development plan.</p>
<p>Clause - Parking Overlay, Schedule 6 (PO6), Docklands – Victoria Harbour</p>	<p>Schedule 6 to the Parking Overlay provides for car parking rates within Victoria Harbour. A permit is required to provide car parking spaces in excess of the maximum number specified in the Table. As the use is not specified, the rate is 1 space to each 100 square metre of gross floor area. The proposal does not provide for parking and therefore is consistent with the Overlay.</p>

5 STRATEGIC FRAMEWORK

5.1 State Planning Policy Framework (SPPF)

The relevant provisions of the SPPF are summarised as follows:

- Clause 15.01-5 (Cultural identity and neighbourhood character)
- Clause 15.03-1 (Heritage conservation)
- Clause 15.02 - Sustainable Development

5.2 Local Planning Policy Framework (LPPF)

5.2.1 Municipal Strategic Statement (MSS)

The relevant provisions of the MSS are summarised as follows:

- Clause 21.04 - Settlement
- Clause 21.05 Environment and Landscape Values
- Clause 21.06 - Built Environment and Heritage
- Clause 21.10 Infrastructure
- Clause 21.13 - Local Areas (Docklands)

5.2.2 Local Policies

The relevant local policies are summarised as follows:

- Clause 22.18 - Urban Design within the Docklands Zone
- Clause 22.19 - Energy Waste and Water Efficiency

6 ZONE

The subject site and surrounding land is located within the Docklands Zone 2 (DZ2). The purpose of the Docklands Zone 2 is:

‘To provide for a range of commercial, residential, recreational, educational, technology, business and leisure uses within a mixed use environment.

To provide a promenade environment for urban art and waterfront events and festivals.

To provide a retail focus for Docklands that complements retailing in the Central City.

To encourage the refurbishment of existing wharves for commercial, institutional and public uses.’

7 OVERLAY(S)

The subject site is affected by the following overlays

- Heritage Overlay - Schedule 915
- Design and Development Overlay - Schedule 50 (Area 7)
- Table 1 to schedule 50 recommends a discretionary height control of 15 metres with the exception of one building not exceeding 25 metres at the eastern portion of this area.
- Table 2 schedule 50 recommends a minimum width of waterfront promenades of 6.5 metre wide with the eastern end promenade to have an average width of 15 metres.

- Parking Overlay Schedule 6 seeks to minimise car ownership associated with residential land use by specifying a maximum parking rate rather than a minimum.

8 PARTICULAR PROVISIONS

The following particular provisions apply to the application:

- Clause 52.06, Car Parking
- Clause 52.07, Loading and Unloading of Vehicles
- Clause 52.34, Bicycle Facilities

9 GENERAL PROVISIONS

The following general provisions apply to the application:

- Clause 65, Decision Guidelines, which includes the matters set out in Section 60 of the Planning and Environment Act 1987.
- Clause 66, Referral and Notice Provisions

10 PUBLIC NOTIFICATION

Pursuant to Section 96(1) of the Act, a responsible authority must obtain a permit from the Minister before carrying out any use or development for which a permit is required under the planning scheme for which it is the responsible authority unless the planning scheme exempts the land, use or development from this subsection.

In this instance, Clause 67 of the Melbourne Planning Scheme exempts the application from the requirements of Section 96 provided the owners and occupiers of adjoining land are given notice of the application in accordance with Section 52(1)(c) of the Act.

Notice of the application was given by ordinary mail to the owners and occupiers of surrounding properties and by posting one notice on the site for a 14 day period, in accordance with Section 52 of the *Planning and Environment Act 1987*.

11 OBJECTIONS

The application has not received any objections.

12 REFERRALS

12.1 Internal

The application was referred to the following areas of Council for comment:

12.1.1 Urban Design Referral (LS) (summarised)

- General support is provided for this modest two-storey building that will introduce important community facilities to this new precinct.
- The facility will bring activation to the area including, importantly, access for community boating activity on Victoria Harbour.
- The ground floor area is dominated by storage space for boats but the design has ensured that windows on Collins Street allow for views into the boat club space.
- The layout of first floor Family Services and multi-purpose function space will interact well with the public realm and should create an engaging building that relates to its context.

- Being a low-rise building that will be overlooked by existing and future taller buildings it will be important that the roofscape of this building is designed with care. Drawings indicate that the architects have given this matter due consideration. Any proposal for 'future roof plant' as shown on the roof plan should be screened from view as indicated on plan/section and elevations.
- The proposal for a 6.5m wide promenade with max 2.5m projection at first floor level (see sections and east elevation) is consistent with the Development Plan for this precinct.

12.1.2 Land Survey Referral (summarised)

- The proposed development plans show structure outside the property boundaries (shown in red on the plans) which does not meet the Road Encroachment Guidelines.
- The line of the level above is outside the property boundary and appears to be only 3.2 metres above the promenade. It is noted that the door for the boating storage is 7 metres clear.
- The promenade will be Crown land with easements of way for access and the Council will be Committee of Management.

12.1.3 Engineering Services Referral (summarised)

The City of Melbourne Engineering Services recommended a number of standard permit conditions to be included should a permit be issued. The Engineering Services Branch also notes that the proposed development includes construction of a portion of Collins Street and that this portion of road must be declared as Public Highway and vested in Council as Road on a plan of subdivision prior to the occupation of the development.

12.1.4 Traffic Engineering Services Referral (summarised)

- A temporary roundabout is proposed as an interim treatment along Collins Street Wharf to allow vehicles to turn around, without using the Boating Hub Loop. At the completion of the development, the roundabout will be removed and Collins Street Wharf will be aligned parallel to the Promenade.
- The removable bollards proposed along the loop at the intersection of Trumblehome Walk / Promenade to restrict unauthorised vehicle access is not supported as the installation and removal of bollards on Council land can only be undertaken by authorised Council officers during set closure hours.
- The proposed development has a statutory requirement to not exceed a total of 14 car parking spaces. The proposed provision of no on-site car parking spaces therefore satisfies the requirements of the Schedule to Clause 52.06 of the Planning Scheme.
- The proposal is not considered to generate a requirement for loading under the Planning Scheme.
- The proposal generates a bicycle parking requirement for 2 visitor spaces under the Planning Scheme. It is noted that 11 bicycle spaces are proposed. It is noted that the positioning of some bicycle parking adjacent walls is not in accordance with the dimensions specified within the Bicycle Victoria product data sheet. As such it is required that some bicycle parking be removed.
- Traffic layout plans provided are conceptual in their form and detailed plans would need to be provided as a condition of permit – in particular, the 8.8m

waste service vehicles are 4.0m high. Access along the promenade clear from over hanging roofs, lights and street furniture must be demonstrated.

- The turning movement diagrams provided indicate that motorists will be required to drive very close to the edge of the promenade and or close to light poles or other street furniture. This is not acceptable as a motorist would not be able to distinguish where the edge of the promenade is located. It is therefore requested that clearer and more detailed diagrams are provided showing measures to protect motorists from the edge of the river and clearances from street furniture.

12.2 External

The application was referred to the following external authority for comment:

12.2.1 Docklands (VicUrban) Referral

Places Victoria has no objection to the grant of a planning permit for the development of the subject property in accordance with the details and plans submitted to us by the developer.

13 ASSESSMENT

The application seeks to construct a multi-purpose two storey building comprising a boating and storage facility at ground floor level and a family services hub (parenting, health and development) on the first floor level.

Having regard to Clause 65 and the various relevant policies of the Melbourne Planning Scheme the key planning considerations pertinent to this application are whether:

- the proposal is consistent with the Victoria Harbour Development Plan
- the proposal is respectful of built form character of the area
- the proposal will have acceptable amenity outcomes
- the proposed traffic and car parking arrangements are satisfactory

13.1 Victoria Harbour Development Plan

A development plan known as the Victoria Harbour Development Plan (2012), has been approved for land. The approved development plan establishes that the site is specifically identified as being appropriate for a Family Services and Boating Hub and the context and locational attributes of the site make it highly suitable for such a purpose.

The proposal is consistent with the approved development plan because it will:

- Deliver a Family Services and Boating Hub which together with the Docklands library, will draw a number of community services together, attracting a wide audience and providing a platform for locals to engage in social activities and enjoy access to community facilities;
- Activate the site by providing an attractive building and achieve the 'gateway' role of the site to the Victoria Harbour;
- Provides for building heights and setbacks which are consistent with those shown on the approved development plan, including the potential for 2.0 metre overhang over public space on the waterfront as per Development Plan sections (refer Figure 5 below);

- Presents an attractive development to Collins Street and the Promenade with articulation of building massing and frontages, and the use of varied materials and finishes;
- Provide sufficient on-site bicycle parking and access in a logical and efficient manner with separate loading and unloading facilities; as confirmed by the review of the Traffic and Transport Assessment prepared by Cardno Victoria Pty Ltd (discussed further below);
- Includes various ESD initiatives to minimise the proposal's environmental impact, as detailed in section 3.12 of the planning report, prepared by Hayball.
- Appropriately manages wind impacts, as detailed in the report by Mel Consultants.
- Provides adequate pedestrian links throughout the site, including appropriate pedestrian signage and shelter above the entrance and along Collins Street; and
- Have all plant/equipment including roof plant carefully integrated into overall design, as the roof form of this low-rise building will be visible from future surrounding taller buildings.
- Include proposed uses (place of assembly, informal outdoor recreation, medical centre and road) which all conform with the uses prescribed in the Development Plan.

The building design is well resolved and of a high architectural quality. The general layout and function of the public spaces and facilities is excellent and will provide an important asset to the local community, the wider Docklands community and the City of Melbourne in the future.

The proposal is, therefore, generally consistent with the Victoria Harbour Development Plan (2010)

13.2 Built Form

The application has been subject to a number of pre-application meetings for the proposal which involved the applicant (Lend Lease Developments), VicUrban, Department of Planning, Transport and Local Infrastructure and the City of Melbourne.

The proposed building design is contemporary and commercial in nature, while also retaining the traditional form of the 'shed' aesthetic. The low rise building typology between Collins Street and the wharf edge is consistent with the requirements of the DDO and appropriate for a new community building on a site that is not constrained in terms of an established character or heritage considerations.

The site layout is functional and the building is aligned to the street grid pattern and property boundaries, in accordance with the approved development plan. The building provides weather protection along pedestrian routes within the development

The proposed building will also create visual interest and make a positive contribution to the site's strategic location (at the entrance to the harbour) and character of the area. Suitable articulation of the facades is proposed, including:

- A double height entrance foyer to define the entrance to the building.
- Cantilevering / projecting elements, noting that the first floor outdoor play area and the double height tilt panel door are important architectural

features of the building and provide a clear break through length and continuity of the façade of the building.

- Varied building materials, including concrete panels, alucobond, glazing, metal louvers, timber battens and zincalume corrugated metal sheeting;
- Varied building colours;
- Varied fenestration, including a generous provision of windows and other openings to activate the façade and provide passive surveillance of the internal workings of the building and pedestrian entry points to ensure a high level of street activation along Collins Street and the Promenade.

Although no significant soft landscaping is proposed, this is consistent with the development plan which provides for a precinct wide response to landscape treatment and includes Dock Square, which is located east of the site.

Overall, the size of the building and its architecturally designed commercial appearance will make a substantial improvement to activating and accentuating Victoria Harbour, Dock Square and the corner of Bourke and Collins Streets. The design, materials and extent of articulation distinguishes the building from the appearance of other building typologies, to create an attractive and inviting community building.

Given the above, the built form of the proposed development represents an appropriate layout for the site through the introduction of an active, attractive gateway building on a site that is currently underutilised and which makes little or no contribution to the desired image of the municipality – noting also that Councils Urban Design Department has not raised any concerns relating to the overall design of development and generally support for the proposal.

It follows that the design and layout of the proposed development will be appropriate to its built form and provides an appropriate design element which generally satisfies the built form design principles of the Development Plan and Clause 22.18 (Urban Design within the Docklands Zone).

13.3 Amenity Impacts

The subject site is somewhat removed from any sensitive land uses, with the nearest (potential) residential development being to the south-east, on the opposite site of Collins Street. The emphasis with this proposal will be on a variety of boating activities and delivering support services for families in the area. With regard to potential amenity impact the following is noted:

- The siting and height of the building negates the potential of off-site amenity impacts such as overshadowing or overlooking to adjoining properties in accordance with the approved development plan;
- Hours of operation for the boating hub and family services hub will reflect typical retail hours;
- Most traffic movements to and from the site will be well away from existing residential areas;
- Noise attenuation measures have been incorporated into the design to reduce noise impacts to public spaces and between internal consulting rooms;
- All loading and unloading of commercial vehicles will be undertaken along the eastern boundary of the site, along Sailmaker way and generally away from public view;

The use of the land for a function centre (Place of Assembly) is a Section 1 Use and is not expected to have unreasonable amenity impacts. It is noted however, that similar function spaces, with outdoor terraces, have caused problems elsewhere in the municipality. It is recommended, therefore, that a condition be included on the permit requiring the submission of an operational management plan to manage any potential amenity impacts associated with the use of the land as a function centre.

Subject to the above condition, it follows that the proposal will not have unreasonable amenity impacts on nearby uses or residential developments.

13.4 Traffic and Access

A review of the traffic and car parking arrangements has been undertaken by the Cardno Group on behalf of Council. The assessment involved a review of the access arrangements to the loading dock and the requirement for bicycle car parking on the site. The assessment concludes:

- The proposed provision of no on-site car parking spaces therefore satisfies the requirements of the Schedule to Clause 52.06 of the Planning Scheme.
- The provision of 11 bicycle spaces will accommodate the bicycle requirements.
- The loading arrangements have been designed to accommodate an 8.6 metre waste service vehicle and for convenient and safe access within and from the site.

Some concern has been expressed regarding the proposed access arrangements, particularly the clearances from built structures (i.e. roof overhangs, lights and street furniture) and the layout and circulation arrangements, including the interaction of car and boat trailers with pedestrians along the Promenade.

Traffic volumes are expected to be low, and will initially be limited to the unloading of boats and movements of service vehicles. Future unloading and loading of boats for regattas and other boating events will occur via the proposed boating ramp to be located at Western Park, on the opposite side of Victoria Harbour with access from Docklands Drive.

Notwithstanding this, a number of conditions have been included to address concerns regarding circulation and access; including a condition requiring the provision of a detailed functional traffic layout plan demonstrating adequate clearance from all buildings, roof projections, street furniture and lighting.

Subject to satisfying this condition, the proposed traffic and access arrangements are considered to be satisfactory.

13.5 Other Matters

13.5.1 Wind

A wind effects statement has been prepared by MEL Consultants dated 5 May 2013 (enclosed). The statement has identified that the wind conditions around the Community Boating Centre were generally either within or on the criterion for walking comfort with the exception of the northern corner of the Centre which had wind conditions that were above the walking criterion but within the waterfront criterion.

Due to its proximity to the waterfront and its likely low utilisation as a thoroughfare to the Boating Hub, the wind conditions at the northern corner are considered to be generally acceptable for its intended use.

13.5.2 Projections

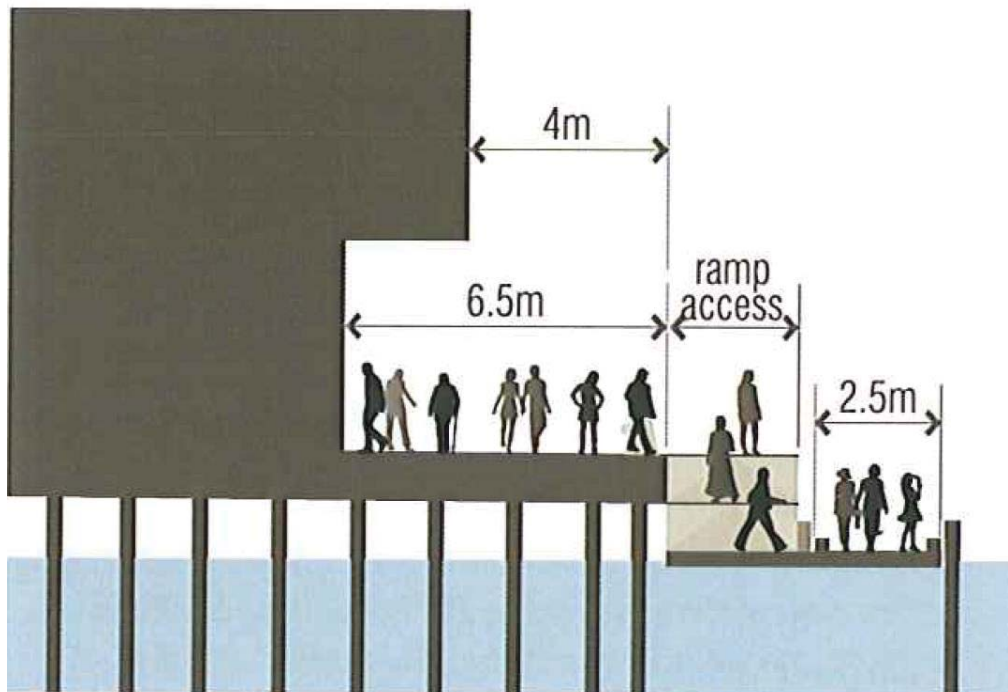
The application proposes a number of projections outside the future proposed title boundaries, notably the proposed habitable rooms at the first floor level.

Council’s Land Survey team has raised a number of concerns relating to this projection, as Council is generally not supportive of habitable floor space that projects outside the boundaries at upper levels. This includes two consulting rooms and the upper level viewing terrace.

It is considered that the projecting first floor element is consistent with the development plan as detailed in Figure 3 below and the projection plays a vital role in providing shelter in inclement weather conditions – noting also that the upper level is set back between 5.2 and 6.0 metres from the edge of the wharf, which exceeds the development plan requirement.

The realignment of boundaries to align with the upper level of the building may be an appropriate solution to deal with issue raised in relation to the projecting first floor level.

FIGURE 5: Indicative Section – Victoria Harbour Promenade (Figure 6.3.23 of the Development Plan)



13.5.3 Heritage

An application for a permit has been lodged with Heritage Victoria for the proposed Family Services and Boating Hub and pursuant to Clause 43.01 no permit is required under the Heritage Overlay once a Heritage Victoria approval has issued.

13.5.4 Environmentally Sustainable Design (ESD)

The proposal includes various ESD initiatives to minimise the proposal’s environmental impact, as detailed in section 3.12 of the planning report, prepared by Hayball.

Clause 22.19 states that mixed use developments should be assessed against the performance measures to each use component of the development. In this case only the 540 sqm family services hub (nested under office use) is required to be considered against the performance measures of Clause 22.19. A condition is

therefore recommended requiring the provision of a report addressing the requirements of Clause 22.19.

14 CONCLUSION

The proposal is consistent with the approved Development Plan for the site, and will provide improved access to services, employment and social interaction opportunities.

Furthermore, the building design is well resolved and of a high architectural quality and will create an appropriate prominence and built form presence.

The general layout and function of the public spaces and facilities is excellent and the proposal will complement the existing Docklands Library to provide an important asset to the local community, the wider Docklands community and the City of Melbourne in the future.

15 RECOMMENDATION

That a Permit be issued subject to the following conditions:

1. Prior to the commencement of the demolition, bulk excavation, construction or carrying out of works on the land, two copies of plans, drawn to scale must be submitted to the Responsible Authority generally in accordance with the plans received on 30 January 2014 but amended to show:
 - a) A detailed functional traffic layout plan demonstrating adequate clearance from all buildings, roof projections, street furniture and lighting for all anticipated vehicle types.
 - b) Street furniture or other devices to protect vehicles/motorists from the edge of The Promenade.
 - c) The removal of the bollards proposed along the loop at the intersection of Trumblehome Walk / The Promenade.
 - d) The reconfiguration of the bike storage area to comply with the dimensions specified within the Bicycle Victoria product data sheet.

These amended plans must be to the satisfaction of the Responsible Authority and when approved shall be the endorsed plans of this permit.

2. The layout and dimensions of Sailmaker Way and Trumblehome Walk must be constructed in accordance with the design guidelines specified within VicRoads Traffic Engineering Manual Vol. 1 Section 4.8.1 - Shared Zones.
3. Prior to the commencement of the development, a statement shall be prepared and submitted to the satisfaction of the Responsible Authority. The Statement must demonstrate that the office component of the building has the preliminary design potential to achieve the following:
 - a) Compliance with the energy efficiency requirements of the Sustainable Design Scorecard or equivalent.
 - b) 3 points for Wat-1 credit under a current version of the Green Building Council of Australia's Green Star – Office rating tool or equivalent.
4. Prior to the commencement of the use the applicant must submit an operational management plan describing:
 - a) Details of the proposed hours of operation of the premises, including the proposed function centre.

- b) Details of the provision of music including the frequency and hours of entertainments provided by live bands, live music (DJ), amplified music and any other forms of entertainment.
- c) Security arrangements including the number of personnel and their hours of operation.
- d) General rubbish storage and removal arrangements including hours of pick up.
- e) A complaint handling process to be put in place to effectively manage complaints received from neighbouring and nearby businesses and residents. This must include details of a Complaints Register to be kept at the premises. The Register must include details of the complaint received, any action taken and the response provided to the complainant.

The management plan must be to the satisfaction of, and be approved by, the Responsible Authority. Once approved, the management plan will form a part of the endorsed documents under this permit. The operation of the use must be carried out in accordance with the endorsed operational management plan unless with the prior written consent of the Responsible Authority.

- 5. No loudspeaker, amplifier, relay or other audio equipment may be installed or used outside the premises.
- 6. All projections over the street alignment must be drained to a legal point of discharge in accordance with plans and specifications first approved by the Responsible Authority - Engineering Services
- 7. Prior to the commencement of the development, a stormwater drainage system incorporating integrated water management design principles must be submitted to, and approved, by the Responsible Authority – Engineering Services. This system must be constructed prior to the occupation of the development and provision made to connect this system to the City of Melbourne's stormwater drainage system.
- 8. Prior to the occupation of the development, the section of Collins Street, located within the application site, must be declared as Public Highway and vested in Council as Road on plan of subdivision. The developer must pay all cost and expenses.
- 9. All new or altered portions of road (including the provision of footpaths, public lighting, street trees, pavement marking, signage) in Collins Street must be constructed prior to the occupation of the development, in accordance with plans and specifications first approved by the Responsible Authority — Engineering Services.
- 10. Existing street levels in Sailmaker Way must not be altered for the purpose of constructing new vehicle crossings or pedestrian entrances without first obtaining approval from the Responsible Authority— Engineering Services
- 11. Existing public street lighting must not be altered without first obtaining the written approval of the Responsible Authority — Engineering Services.
- 12. All wharf structures within the public realm must be designed in accordance with the current Docklands Design and Construction Standards Public Infrastructure Works and constructed or upgraded as necessary in accordance with plans and specifications first approved by the Responsible Authority — Engineering Services.

13. The waste storage and collection arrangements must be in accordance with the Waste Management Plan (WMP) prepared by Leigh Design dated 29 January 2014. The submitted WMP must not be modified or altered without prior consent of the City of Melbourne — Engineering Services.
14. No garbage or surplus material generated by the permitted development may be deposited or stored outside the site and bins must be returned to the waste room as soon as practicable after waste collection to the satisfaction of the Group Manager, Engineering Services, City of Melbourne.
15. This permit will expire if one of the following circumstances applies:
 - a) The development is not started within two years of the date of this permit.
 - b) The development is not completed within four years of the date of this permit.

The Responsible Authority may extend the permit if a request is made in writing before the permit expires, or within six months afterwards. The Responsible Authority may extend the time for completion of the development if a request is made in writing within 12 months after the permit expires and the development started lawfully before the permit expired.

Notes

16. All necessary approvals and permits are to be first obtained from the City of Melbourne and the works performed to the satisfaction of the Responsible Authority — Manager Engineering Services Branch.

Signature:
Blair Mather
Planning Officer

Date affirmed: