

## Report to the Future Melbourne Committee

Agenda item 6.1

**Ministerial Planning Referral: TPMR-2020-53**  
**540-550 Lonsdale Street, Melbourne**

**16 February 2021**

**Presenter:** Larry Parsons, Practice Leader Land Use and Development

### **Purpose and background**

1. The purpose of this report is to advise the Future Melbourne Committee of a Ministerial Planning Application seeking approval for demolition and buildings and works to construct a multi-level building (Office and Retail Premises) for the land at 540-550 Lonsdale Street, Melbourne (refer Attachment 2 – Locality Plan).
2. The applicant is Lonsdale Investment CD Pty Ltd ATF Lonsdale Unit Trust who is represented by Urbis Pty Ltd, the owner is Lonsdale Investment CD Pty Ltd ATF Lonsdale Unit Trust, and the architect is Bates Smart Architecture.
3. The land is located within the Capital City Zone Schedule 1 (CCZ1). The land is affected by the Design and Development Overlay Schedules 10 (DDO10) and Parking Overlay Schedule 1 (PO1).
4. The proposed building is 23 storeys high and features two distinct elements; a podium and tower with setbacks above the podium. The building would comprise 39,261 square metres of office space and 903 square metres of retail space.
5. The Minister for Planning has formally referred the application to Melbourne City Council as a recommending referral authority under section 55 of the *Planning and Environment Act 1987* (Act).

### **Key issues**

6. The key issues relevant to this proposal relate to urban design and built form, including an assessment against the design objectives and built form controls in DDO10, provision of an agreed public benefit due to the development exceeding a Floor Area Ratio of 18:1, loading, traffic and waste.
7. The proposed development complies with the modified built form requirements set out in DDO10. The podium and tower format and setbacks successfully minimise public realm amenity impacts and unreasonable impacts to the adjoining properties.
8. There are key issues relating to the east and west side boundaries of the site that have interfaces with lanes. These issues include the removal of a pedestrian footpath on private land in Healeys Lane to the west and traffic conflicts between pedestrians, waste collection, loading, and car parking access in Chisholm Place to the east.
9. These issues are recommended to be resolved through permit conditions that would maintain the footpath on the private land alongside Healeys Lane, and by widening Chisholm Place where vehicles are proposed to enter. Refer to Delegate's Report for detailed responses.
10. Aside from the above items, the proposed built form is considered acceptable as it introduces a high quality design response, increases ground level activation, and removes an above ground public car park.

### **Recommendation from management**

11. That the Future Melbourne Committee resolves to advise the Department of Environment, Land, Water and Planning that the Melbourne City Council does not object to the proposal subject to the conditions outlined in the delegate report (refer to Attachment 4 of the report from management).

### **Attachments:**

1. Supporting Attachment (Page 2 of 76)
2. Locality Plan (Page 3 of 76)
3. Selected Plans (Page 4 of 76)
4. Delegate Report (Page 33 of 76)

## Supporting Attachment

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### Legal

1. The Minister for Planning is the Responsible Authority for determining this application.
2. The Minister for Planning has formally referred the application to Council under Section 55 of the Act.

### Finance

3. There are no direct financial issues arising from the recommendations contained within this report.

### Conflict of interest

4. No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a material or general conflict of interest in relation to the matter of the report.

### Health and Safety

5. Relevant planning considerations such as traffic and waste management, potential amenity impacts and potentially contaminated land that could impact on health and safety have been considered within the planning permit application and assessment process.

### Stakeholder consultation

6. The application is exempt from the notice requirements of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3), and the review rights of section 82(1) of the Act.
7. It is the responsibility of the Department of Environment, Land, Water and Planning on behalf of the Minister for Planning to administer public notice where required.

### Relation to Council policy

8. Relevant Council policies are discussed in the attached delegate report (refer to Attachment 4).

### Environmental sustainability

9. The Environmentally Sustainable Design report submitted with the application confirms the development will achieve the relevant performance measures set out in Clauses 22.19 (Energy, Water and Waste Efficiency) and 22.23 (Stormwater Management) of the Melbourne Planning Scheme.

# Locality Plan

540-550 Lonsdale Street, Melbourne



# 550 LONSDALE STREET, MELBOURNE TOWN PLANNING SUBMISSION

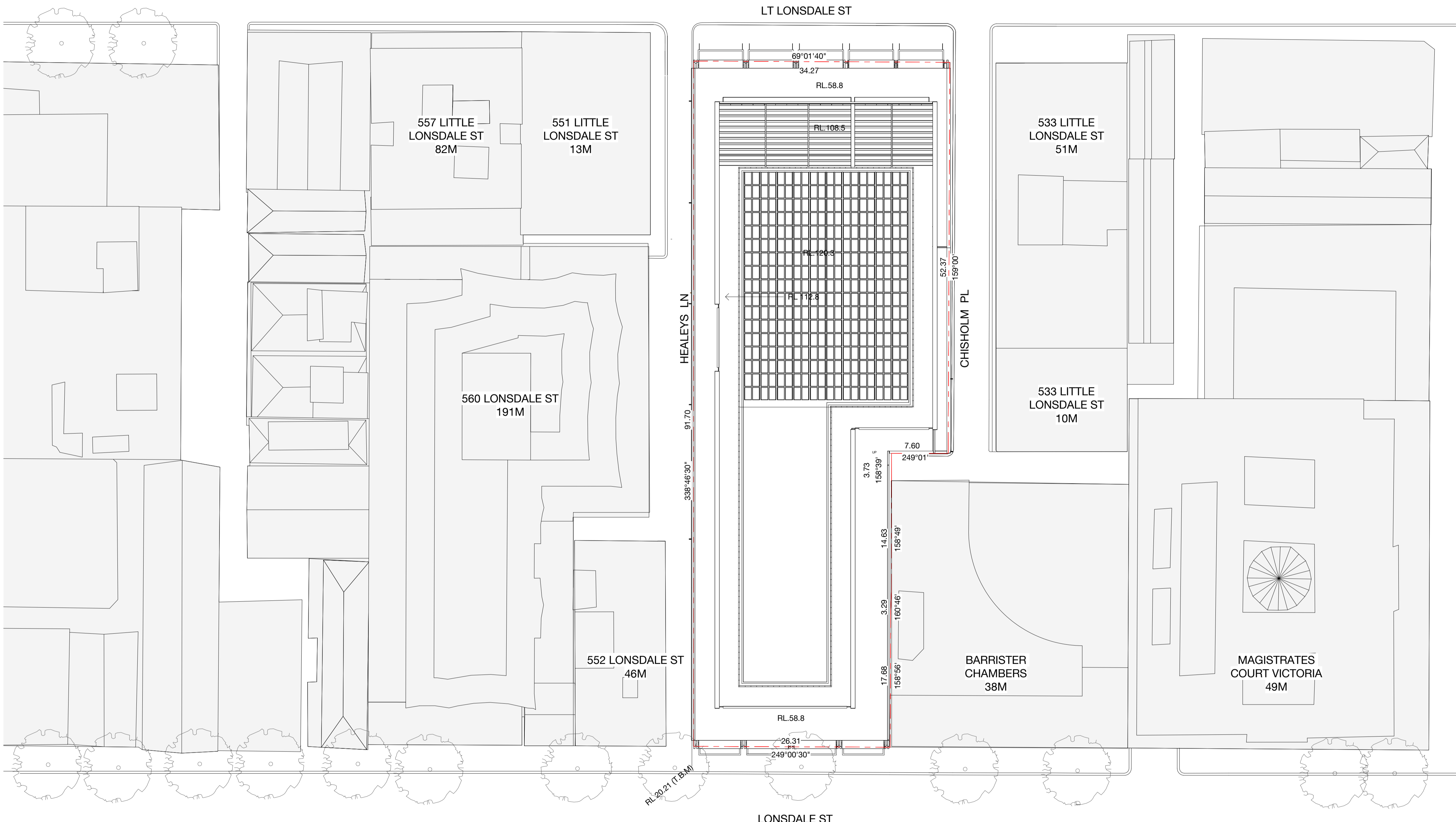
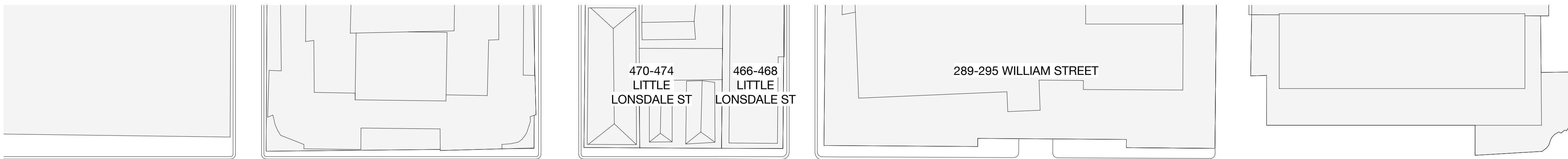
## DRAWING SCHEDULE

DWG NO.	REV	DRAWING SERIES	DRAWING TITLE
TP01.00	A	Site & Existing Conditions	Location & Site Plan
TP01.01	A	Site & Existing Conditions	Existing Site Survey Plan
TP01.02	A	Site & Existing Conditions	Title Boundary Plan
TP01.03	A	Site & Existing Conditions	Site Demolition Plan
TP01.04	A		Ground Plane Activation Comparison
TP03.0GF	C	General Arrangement Plan	Ground Floor
TP03.01	C	General Arrangement Plan	Level 01
TP03.02	B	General Arrangement Plan	Level 02 (Podium)
TP03.03	B	General Arrangement Plan	Level 03
TP03.04	B	General Arrangement Plan	Level 04 (Typical Levels 04-08)
TP03.09	B	General Arrangement Plan	Level 09 (Terrace)
TP03.10	B	General Arrangement Plan	Level 10 (Typical Levels 10-21)
TP03.22	B	General Arrangement Plan	Level 22 (Terrace)
TP03.23	B	General Arrangement Plan	Level 23 (Roof Plant)
TP03.24	B	General Arrangement Plan	Roof Level
TP03.B01	C	General Arrangement Plan	Basement Level 01
TP03.B02	C	General Arrangement Plan	Basement Level 02
TP03.B03	C	General Arrangement Plan	Basement Level 03
TP09.00	B	External Elevations	West Elevation
TP09.01	B	External Elevations	East Elevation
TP09.02	B	External Elevations	North Elevation
TP09.03	B	External Elevations	South Elevation
TP09.10	A		Ground Plane Elevations
TP09.11	A	External Elevations	Detailed South Elevation
TP09.12	A	External Elevations	Detailed West Elevations
TP09.13	A	External Elevations	Detailed North Elevation
TP10.00	B	Building Sections	Section AA
TP10.01	B	Building Sections	Section BB & Section CC

# BATES SMART

M12391 550 Lonsdale Street

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Rev	Date	Description	CP Initial	AA Checked
A	12.06.20	Town Planning		

### 550 Lonsdale Street Melbourne

Site & Existing Conditions Location & Site Plan



Status	TOWN PLANNING		
Scale	1 : 300	@	A1
Drawn	NJ	Checked	AA
Project No.	M12391		
Plot Date	9/11/2020 4:59:07 PM		

Drawing no.	Revision
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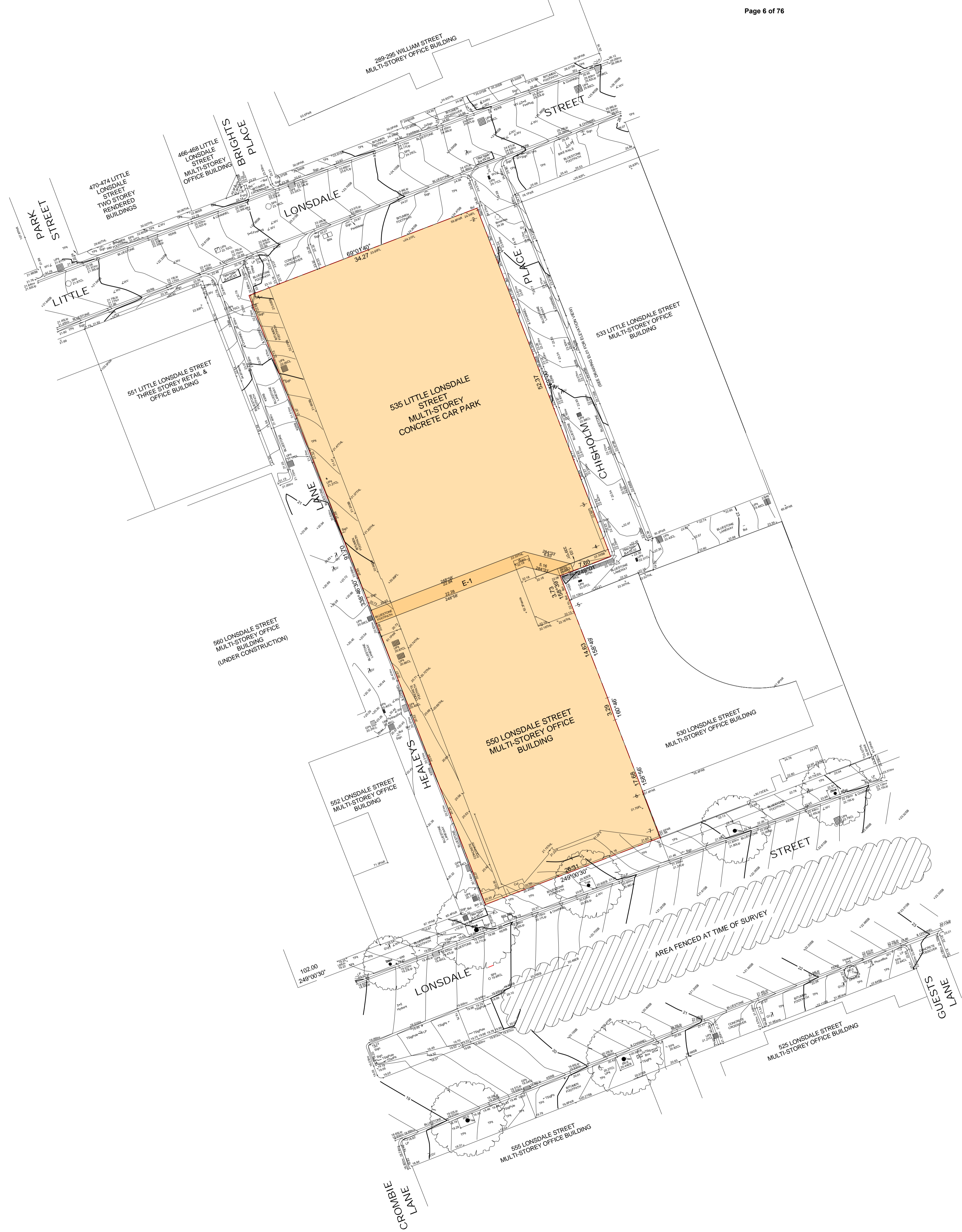
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**DRAWING NOTES:**  
 Title re-establishment, feature and level survey drawn and prepared by Reeds Consulting. Drawing included for reference only. Please refer to original surveyors drawing for full details.  
 Drawing Title: Plan of Relocation Features and Levels  
 Certified: Tom Champion  
 Job Number: 23607/RFL  
 CAD Drawing No.: 23607-0-M-RFL-B.dwg  
 Survey Datum: MGA2020 Zone 55  
 Date of Survey: 27.11.2019  
 Issue Date: 05.03.2020

Rev	Date	Description	CP	AA
	12.06.20	Town Planning		

## 550 Lonsdale Street Melbourne

### Site & Existing Conditions Existing Site Survey Plan



Status	TOWN PLANNING		
Scale	1 : 300	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 4:59:19 PM		
BIM			

Drawing no.	Revision
TP01.01	A

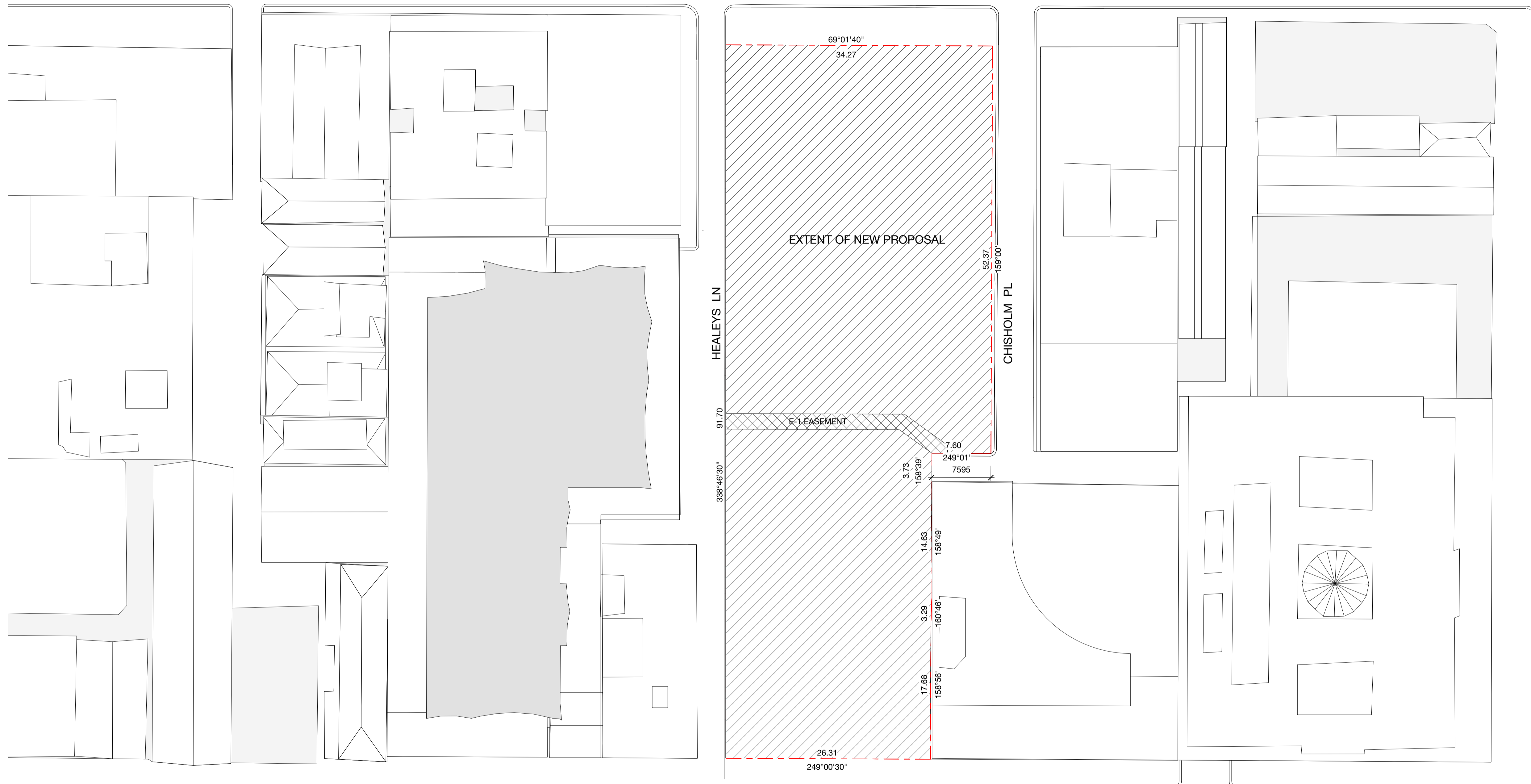
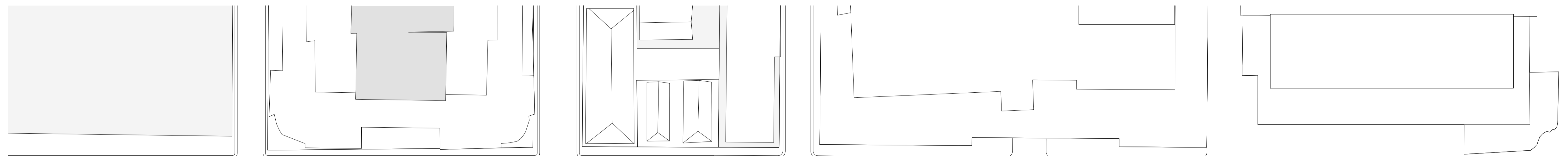
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**550 Lonsdale Street  
Melbourne**

**Site & Existing Conditions  
Title Boundary Plan**



Status	TOWN PLANNING		
Scale	1 : 300	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 4:59:28 PM		
BIM			

Drawing no.	Revision
<b>TP01.02</b>	<b>A</b>

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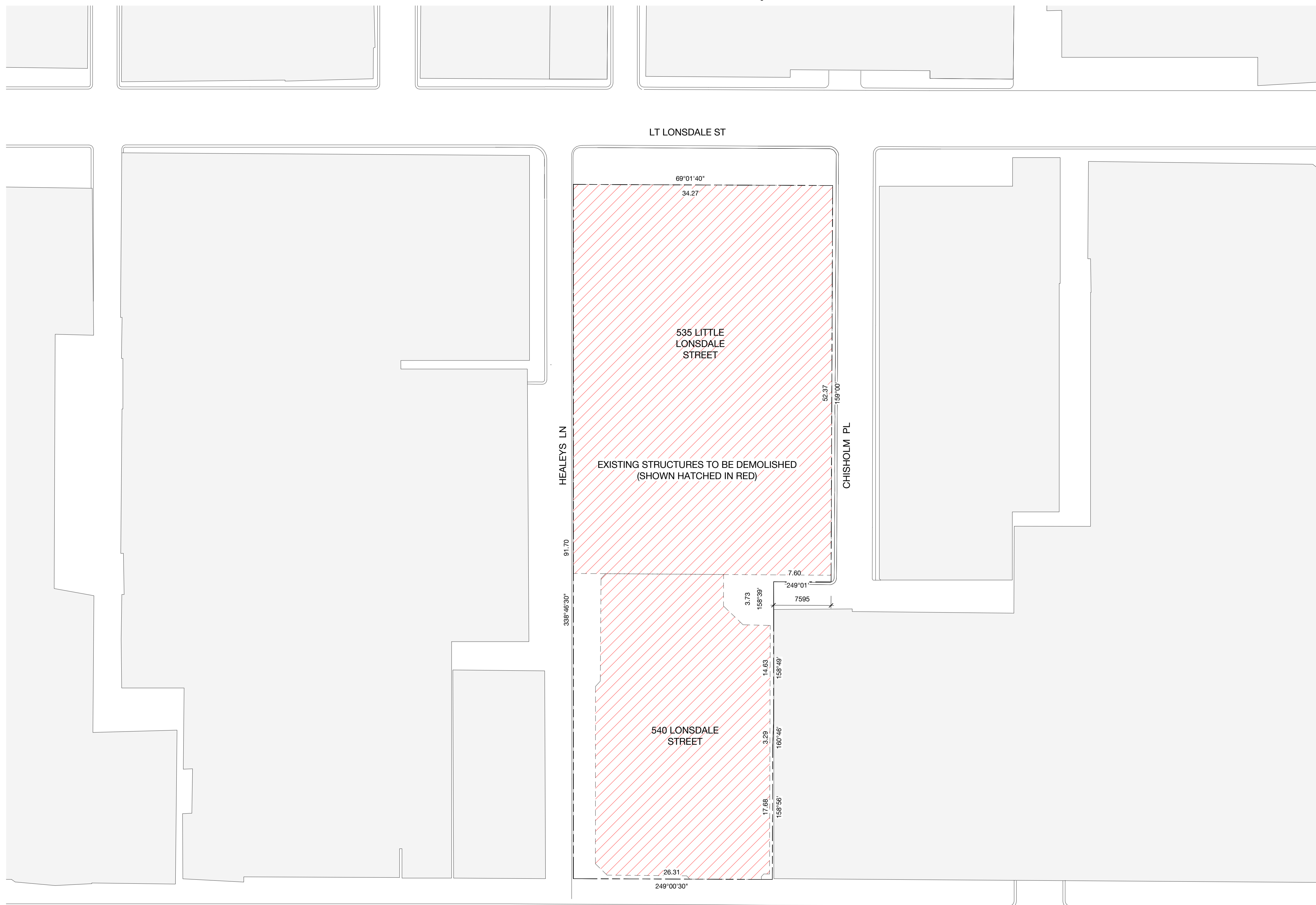


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**LEGEND - DEMOLITION PLAN**

 Demolition works



Rev	Date	Description	CP Initial	AA Checked
A	12.06.20	Town Planning		

**550 Lonsdale Street Melbourne**

Site & Existing Conditions  
Site Demolition Plan



Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 4:59:36 PM		

Drawing no.	Revision
TP01.03	A

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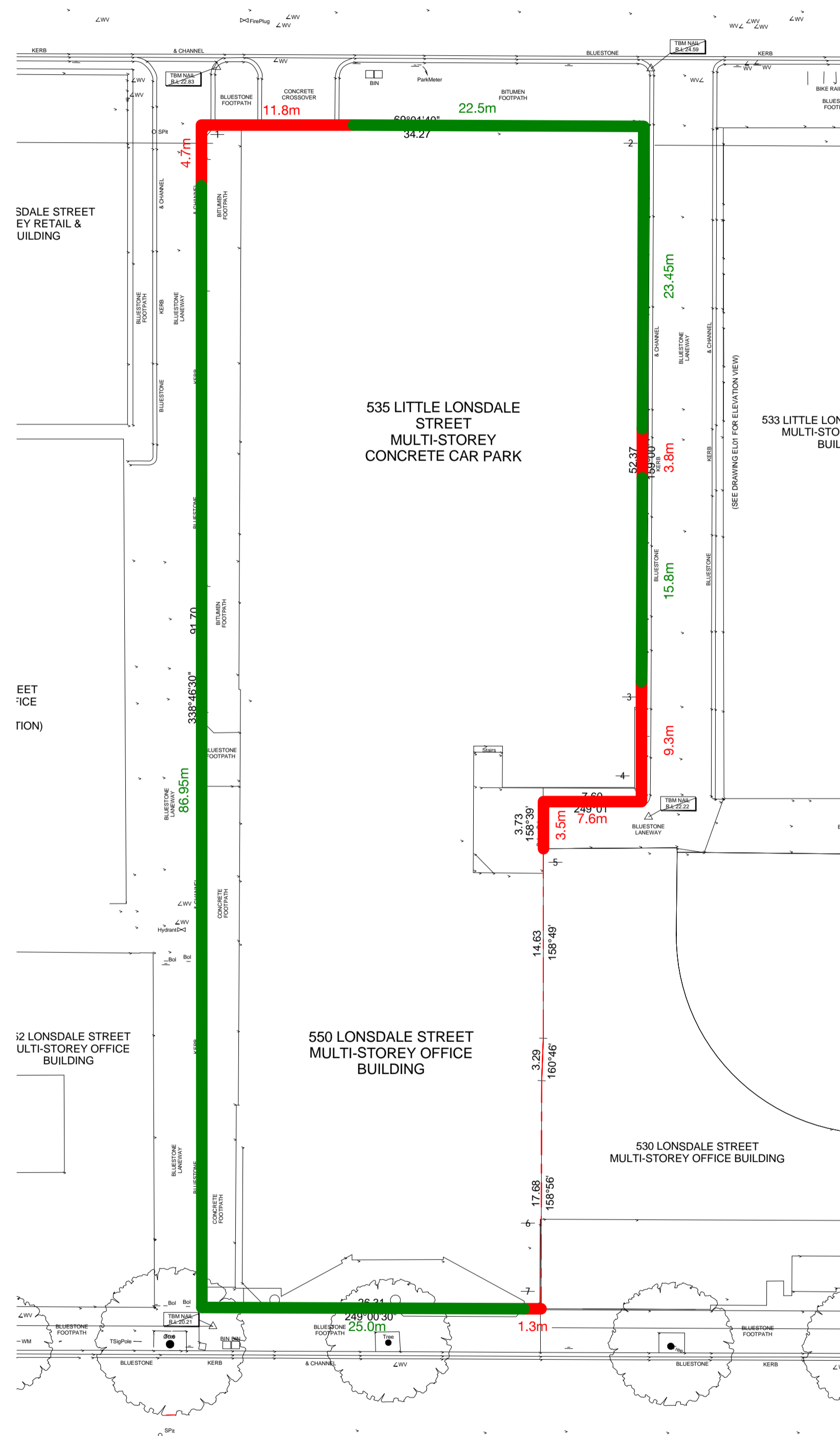
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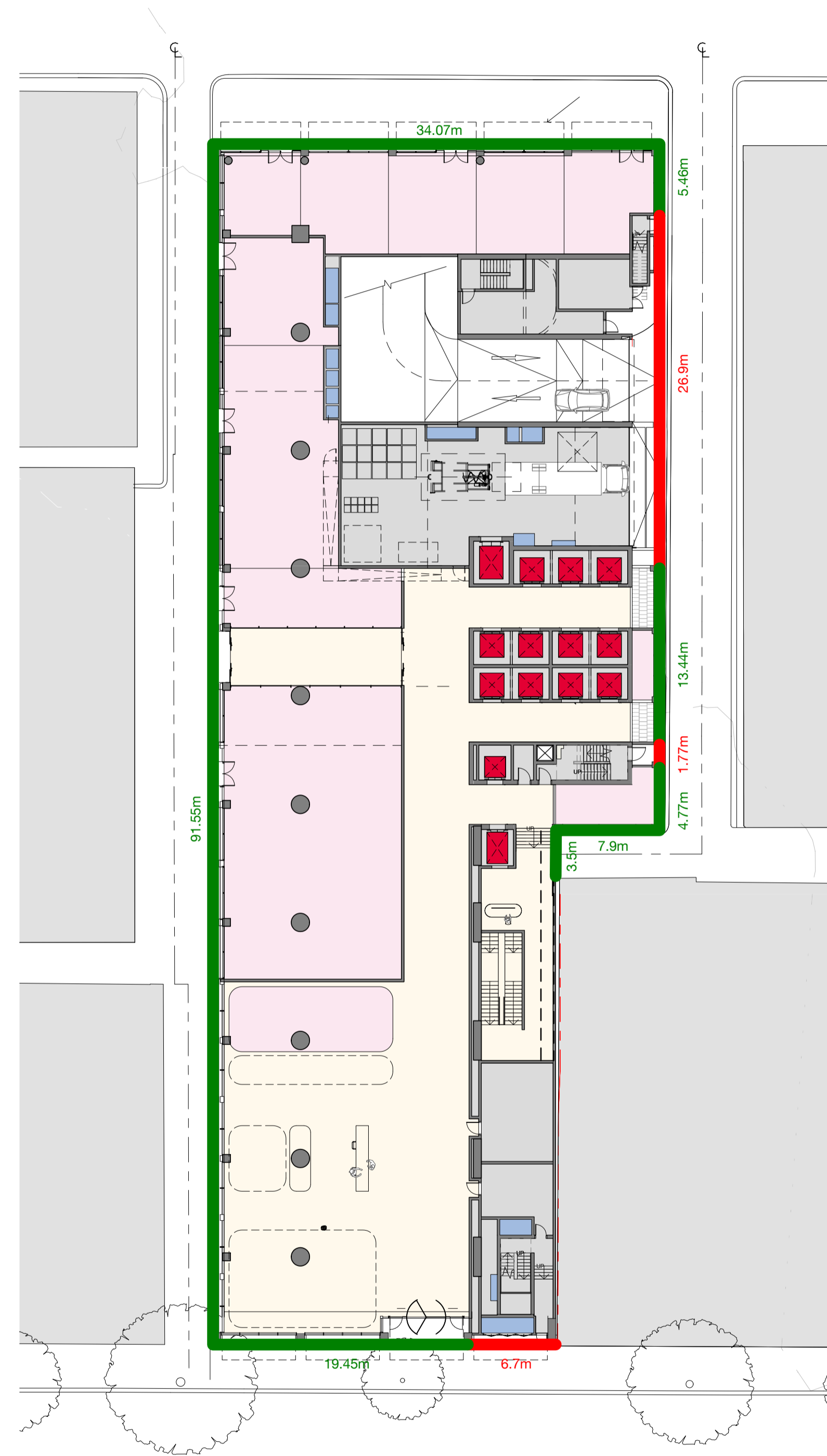
LEGEND

- █ ACTIVE FRONTAGE
- █ NON - ACTIVE FRONTAGE



EXISTING GROUND PLANE ACTIVATION  
1:300

EXTENT OF ACTIVE FRONTAGE: 173.7m 80.5%  
EXTENT OF NON-ACTIVE FRONTAGE: 42m 19.5%



PROPOSED GROUND PLANE ACTIVATION  
1:300

EXTENT OF ACTIVE FRONTAGE: 180.14m 83.6%  
EXTENT OF NON-ACTIVE FRONTAGE: 35.37m 16.4%

Rev	Date	Description	Initial	Checked
A	09.11.20	DELWP RFI Responses	AA	

550 Lonsdale Street  
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Ground Plane Activation  
Comparison



Status	TOWN PLANNING		
Scale	1 : 300	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 4:59:45 PM		
BIM			

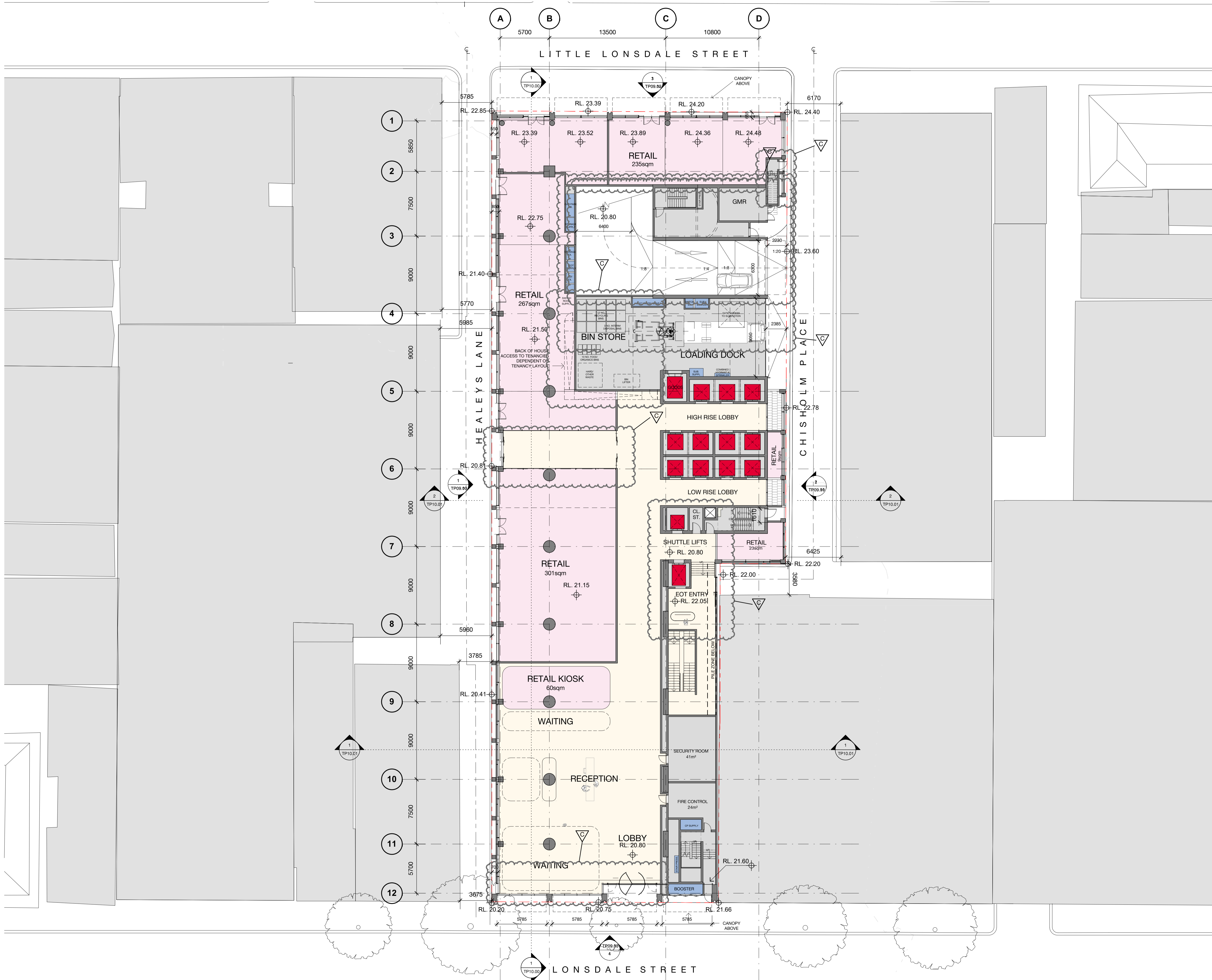
Drawing no.	Revision
TP01.04	A

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Rev	Date	Description	Initial	Checked
D	09.11.20	DELWP RFI Responses	AA	
C	06.11.20	Draft DELWP RFI Response	CP	
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

### 550 Lonsdale Street Melbourne

#### General Arrangement Plan Ground Floor



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:12:53 PM		

Drawing no.	Revision
TP03.0GF	D

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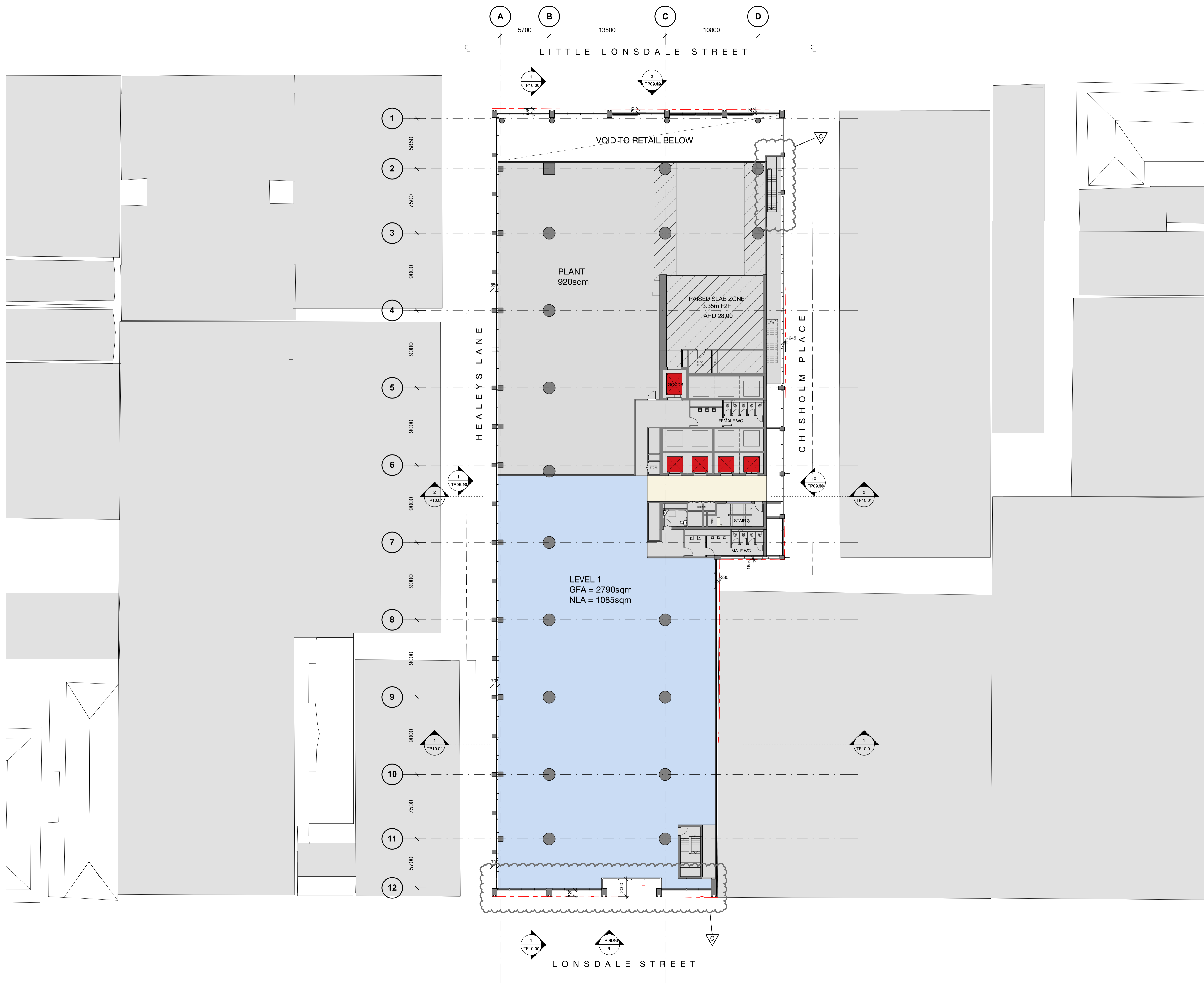
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C	06.11.20	Draft DELWP RFI Response	CP	
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA
Rev	Date	Description	Initial	Checked

**550 Lonsdale Street  
Melbourne**

General Arrangement Plan  
Level 01



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:21:18 PM		

Drawing no.	Revision
<b>TP03.01</b>	<b>C</b>

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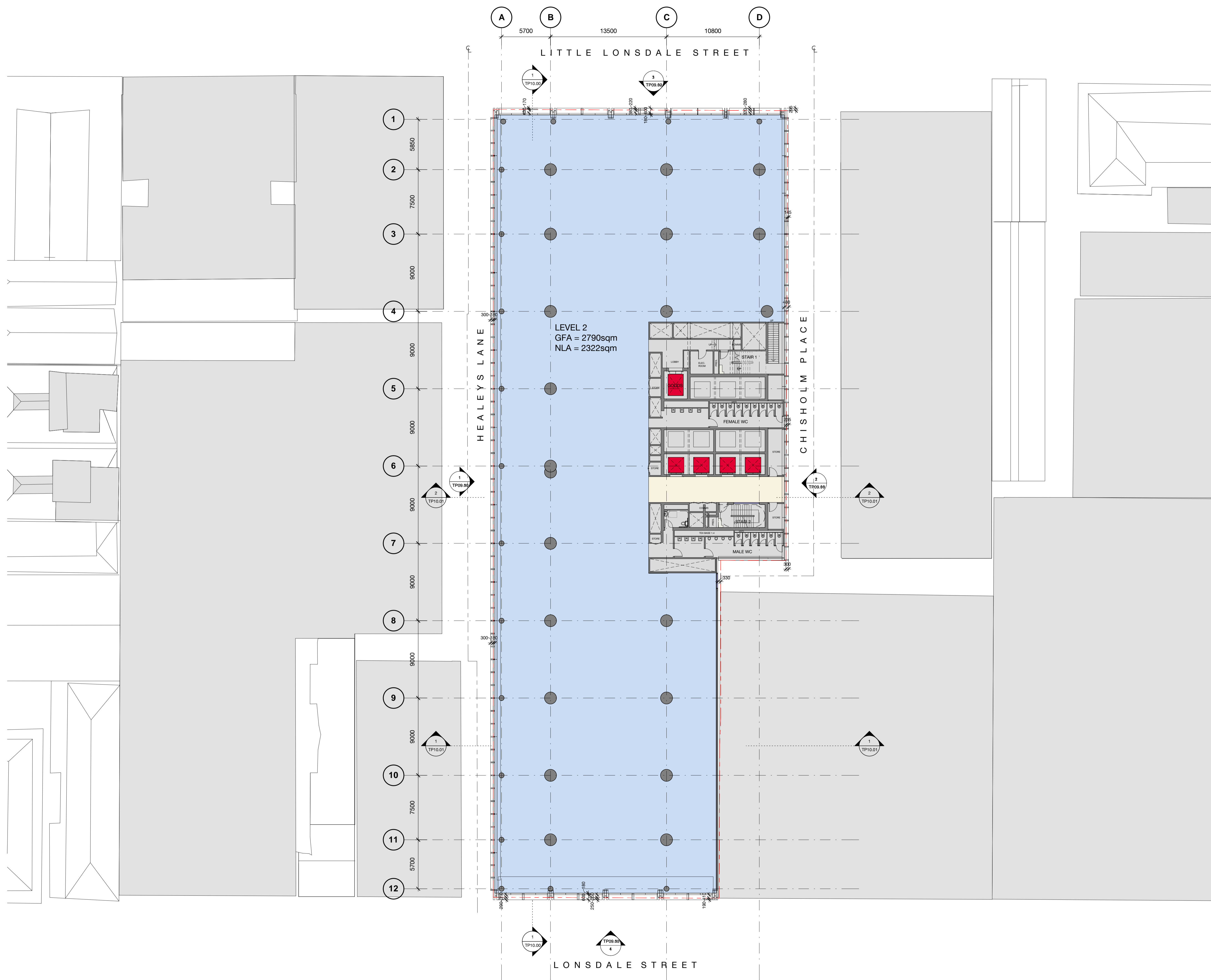
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LITTLE LONSDALE STREET

HEALEYS LANE

CHISHOLM PLACE

LONSDALE STREET

LEVEL 2  
 GFA = 2790sqm  
 NLA = 2322sqm

Rev	Date	Description	Initial	Checked
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

550 Lonsdale Street  
 Melbourne

General Arrangement Plan  
 Level 02 (Podium)



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:00:05 PM		

Drawing no.	Revision
TP03.02	B

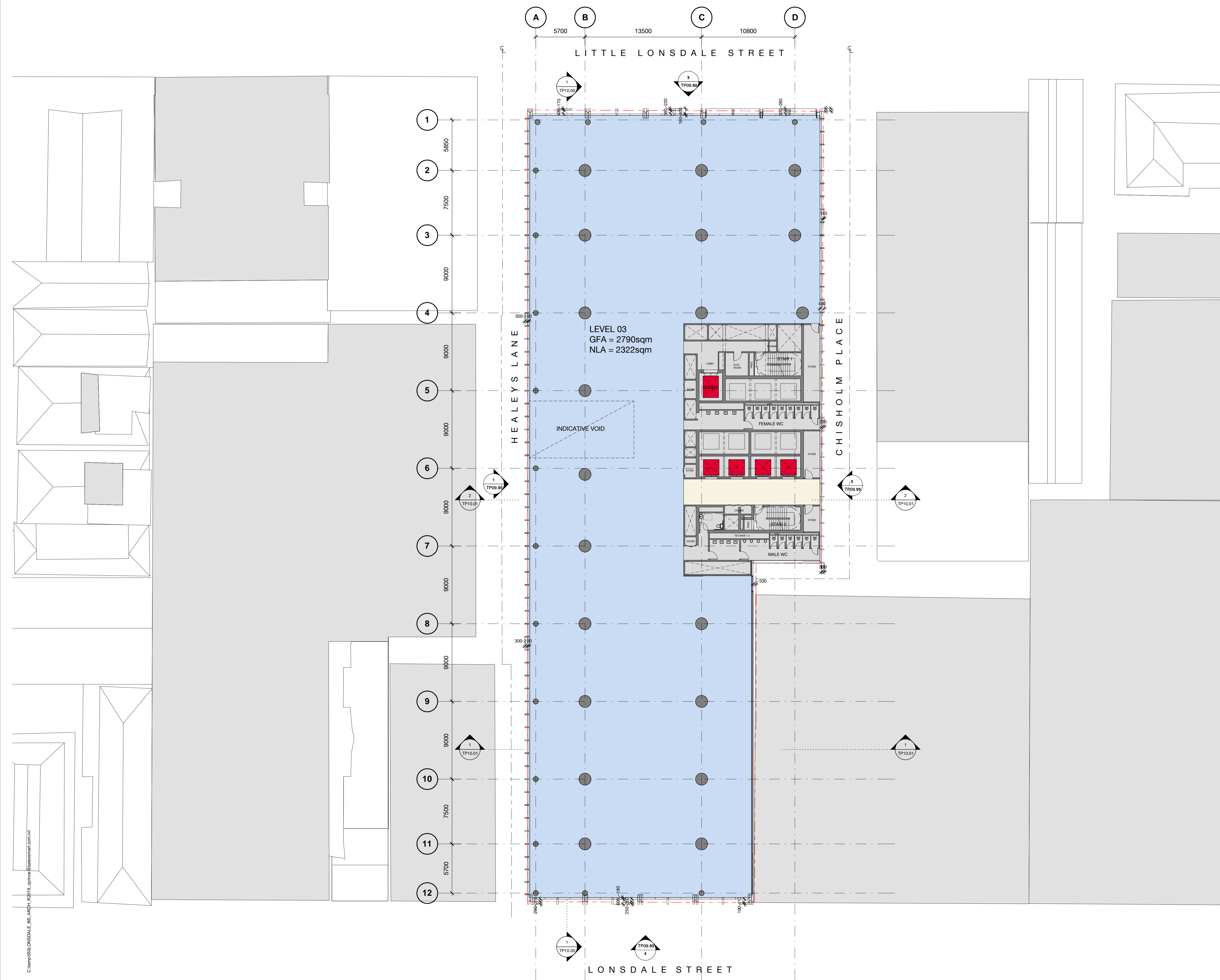
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B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

General Arrangement Plan  
Level 03



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:00:14 PM		

Drawing no.	Revision
<b>TP03.03</b>	<b>B</b>

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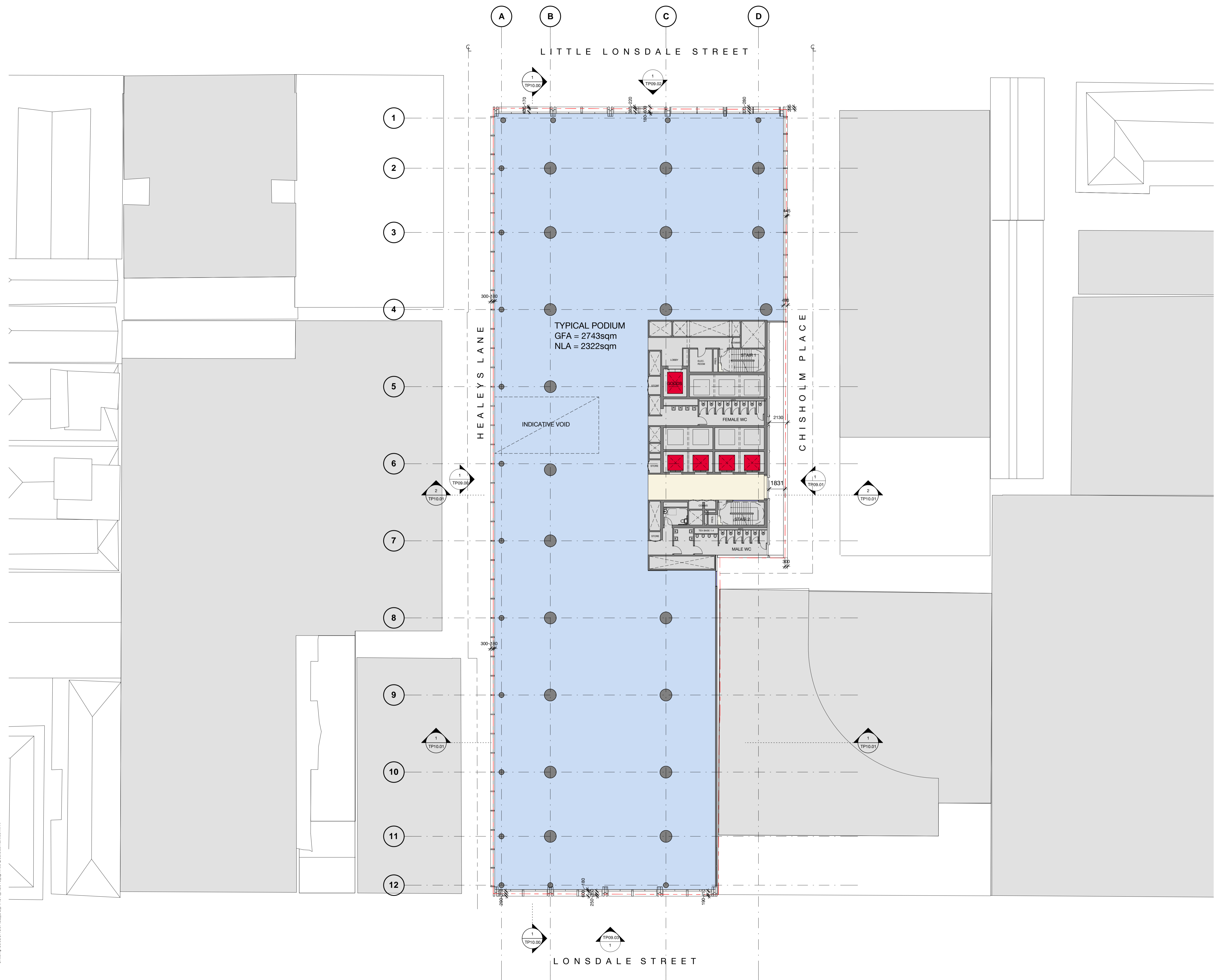
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B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Level 04 (Typical Levels 04-08)**



Status	TOWN PLANNING		
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Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:00:24 PM		
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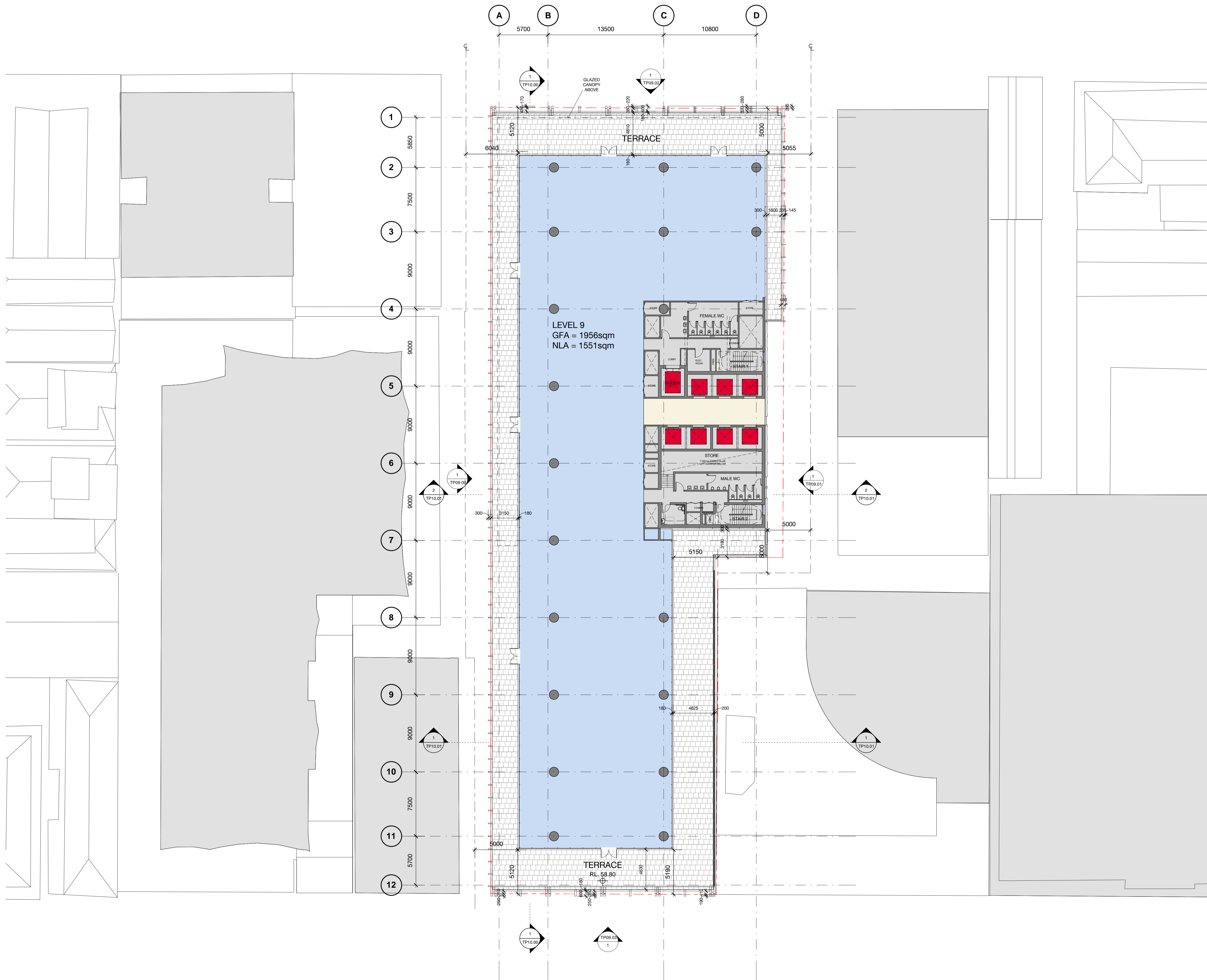
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<b>TP03.04</b>	<b>B</b>

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B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Level 09 (Terrace)**



Status	TOWN PLANNING		
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Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:00:35 PM		
BIM			

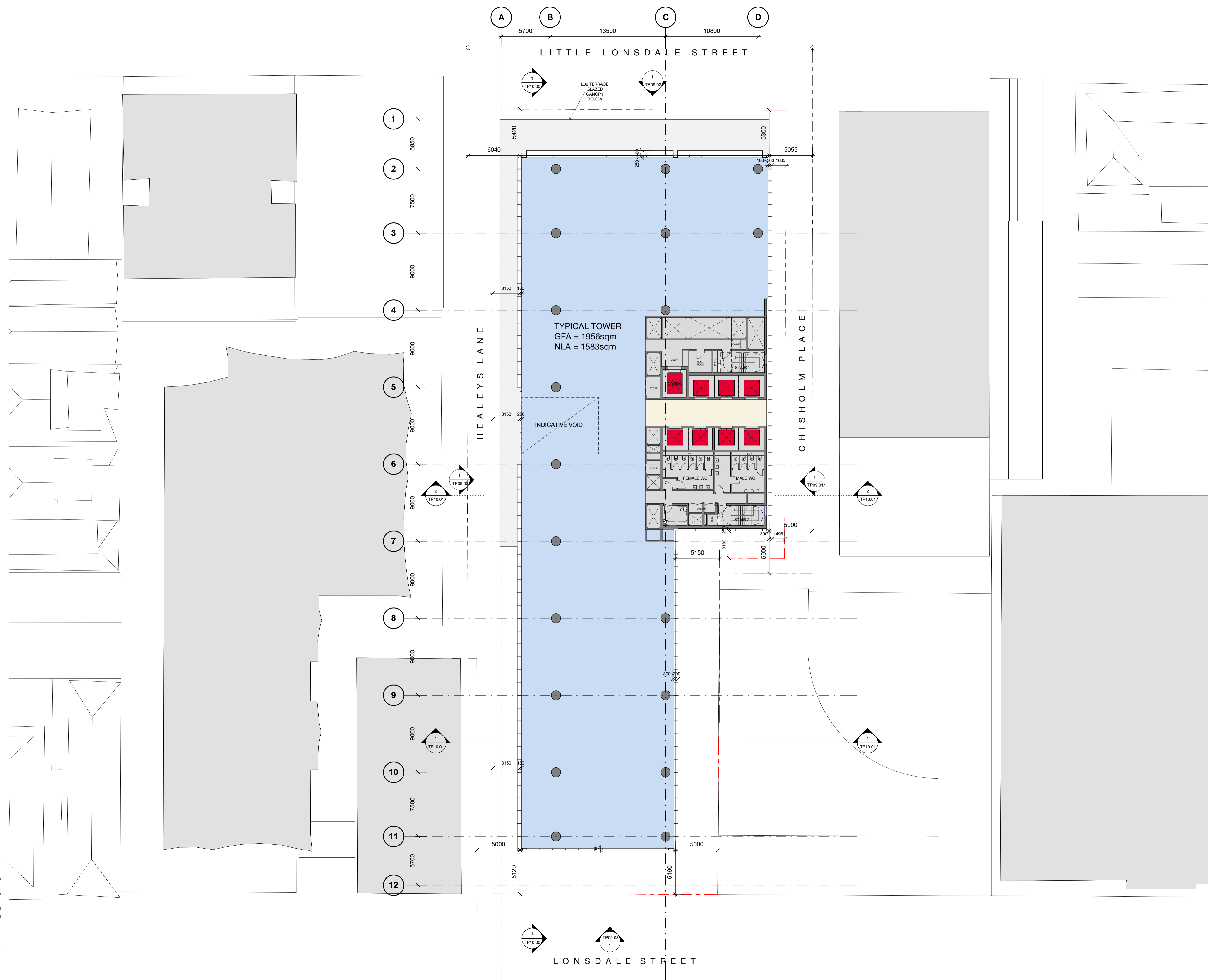
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<b>TP03.09</b>	<b>B</b>

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**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Level 10 (Typical Levels 10-21)**



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:00:56 PM		

Drawing no.	Revision
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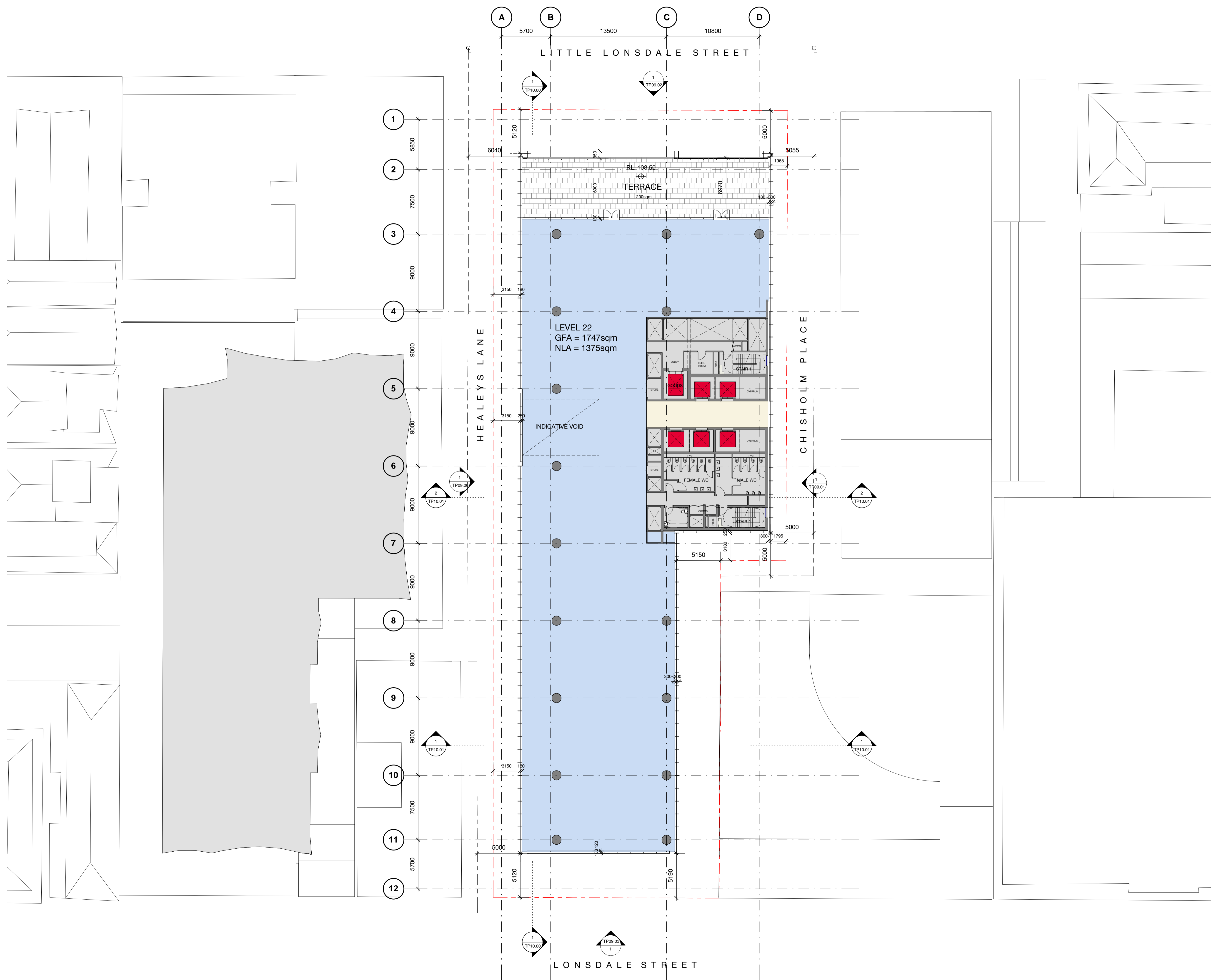
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Rev	Date	Description	Initial	Checked
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Level 22 (Terrace)**



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:01:06 PM		

Drawing no.	Revision
<b>TP03.22</b>	<b>B</b>

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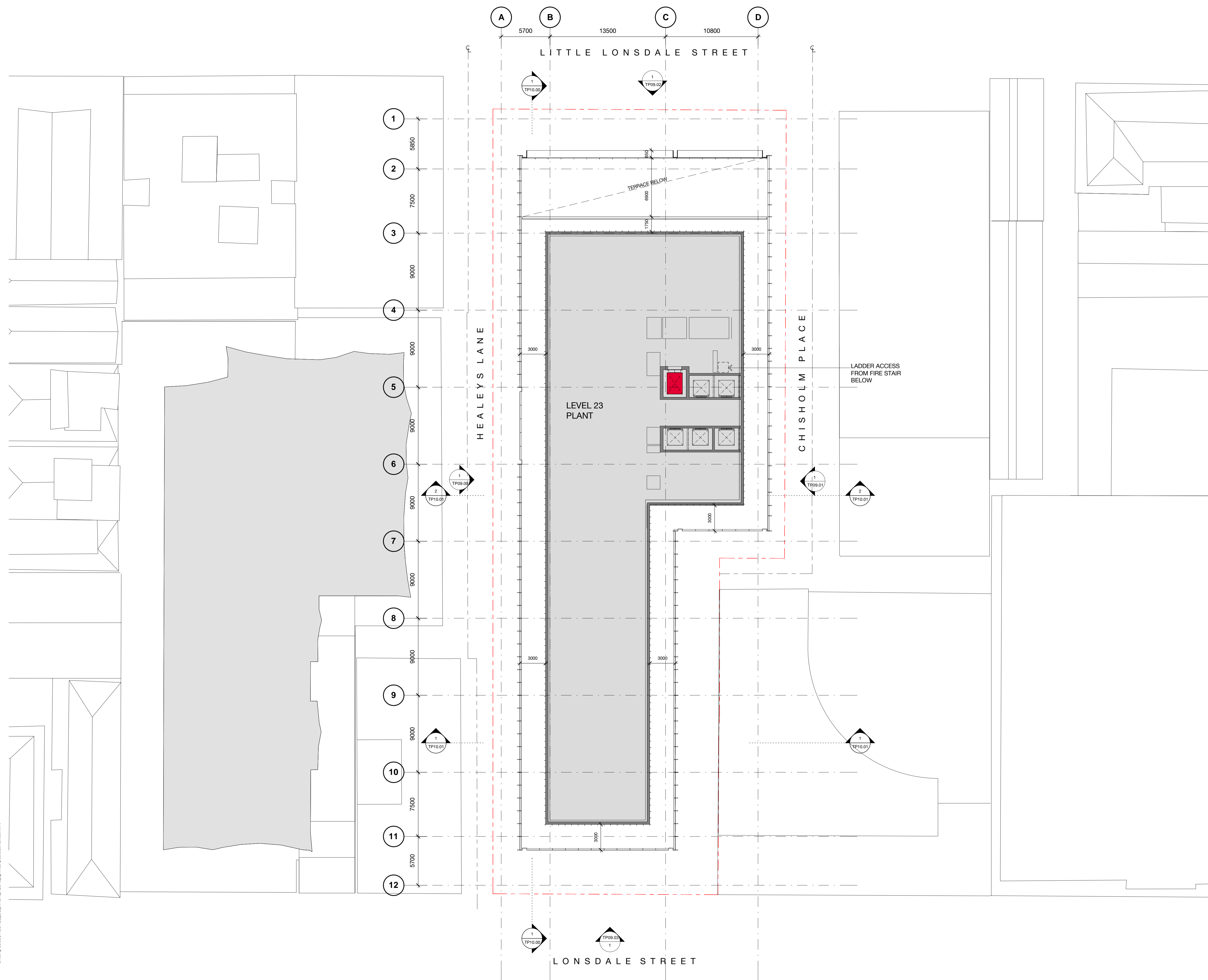
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B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Level 23 (Roof Plant)**



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:01:16 PM		
BIM			

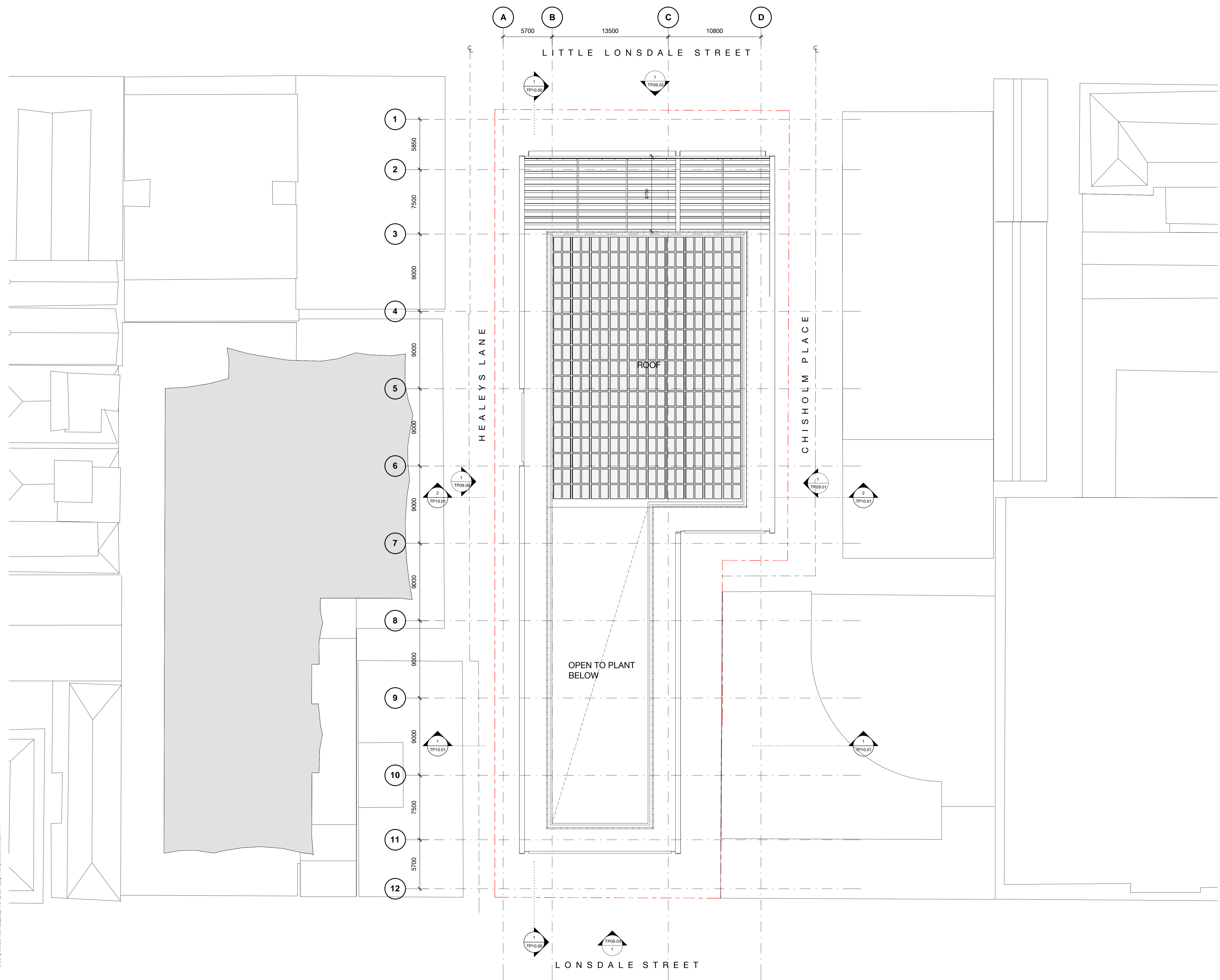
Drawing no.	Revision
<b>TP03.23</b>	<b>B</b>

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Rev	Date	Description	Initial	Checked
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

General Arrangement Plan  
Roof Level



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:01:32 PM		

Drawing no.	Revision
<b>TP03.24</b>	<b>B</b>

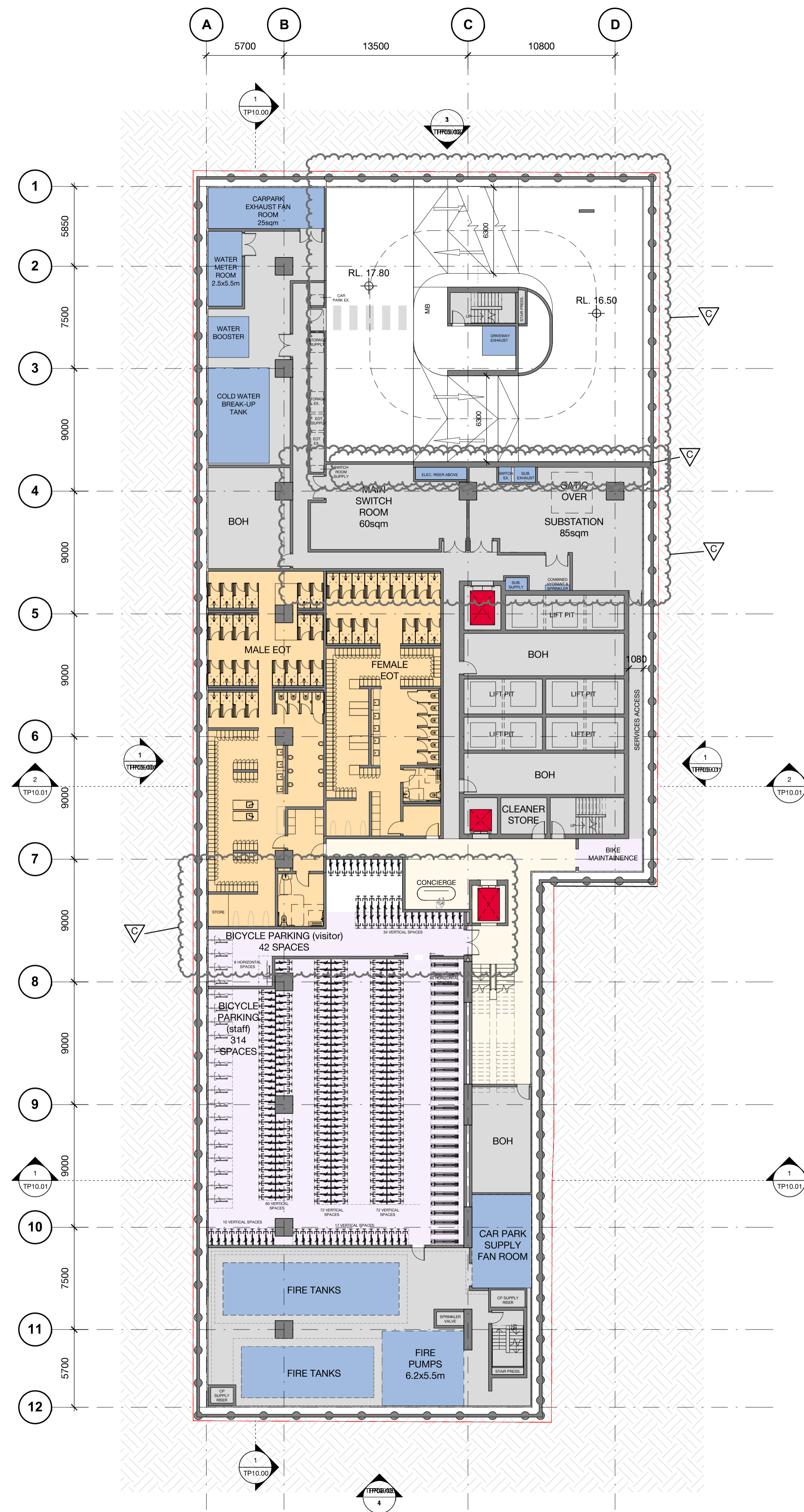
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Rev	Date	Description	Initial	Checked
C	06.11.20	Draft DELWP RFI Response	CP	CP
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

### 550 Lonsdale Street Melbourne

#### General Arrangement Plan Basement Level 01



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:22:40 PM		
BIM			

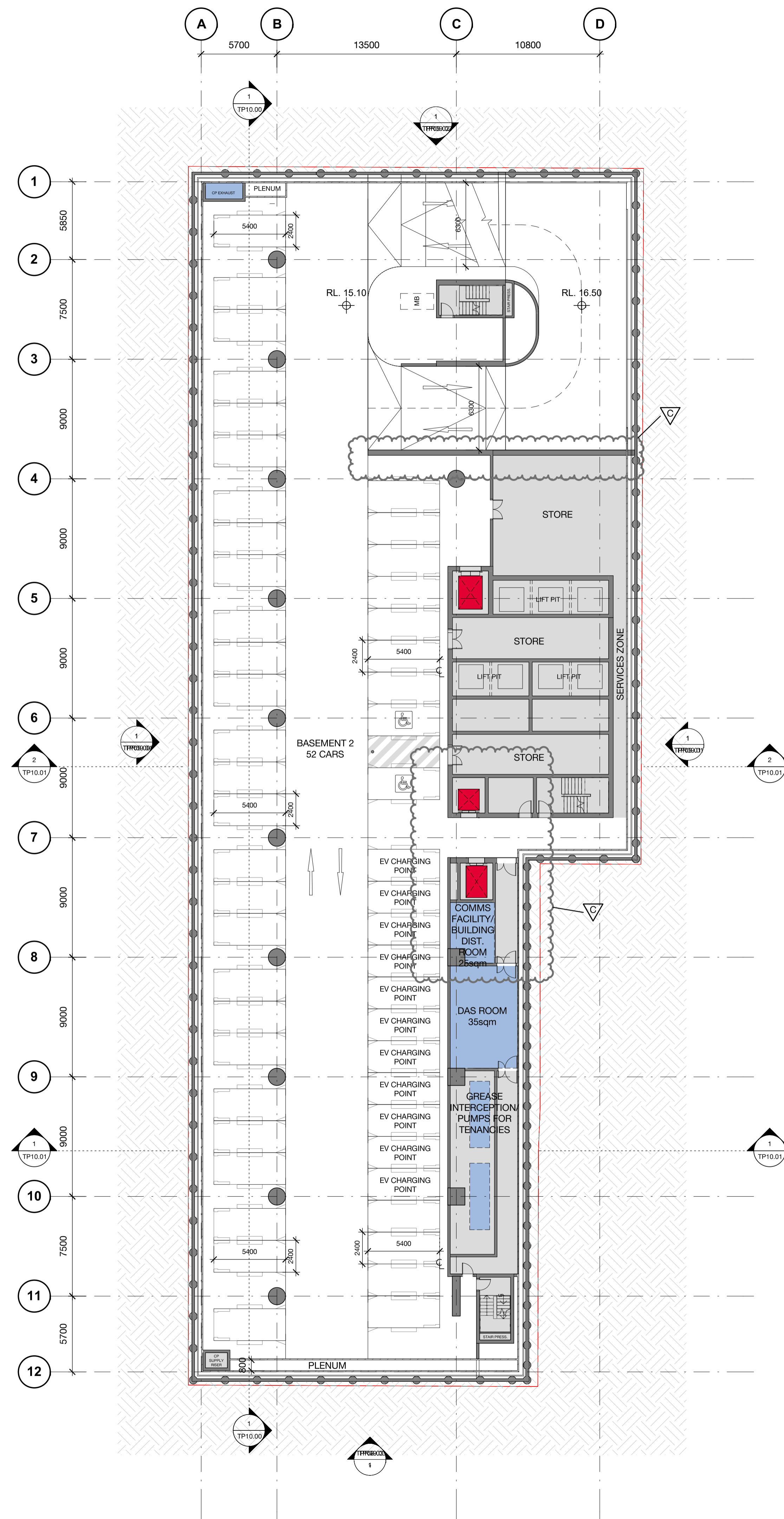
Drawing no.	Revision
<b>TP03.B01</b>	<b>C</b>

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Rev	Date	Description	Initial	Checked
C	06.11.20	Draft DELWP RFI Response	CP	CP
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**General Arrangement Plan  
Basement Level 02**



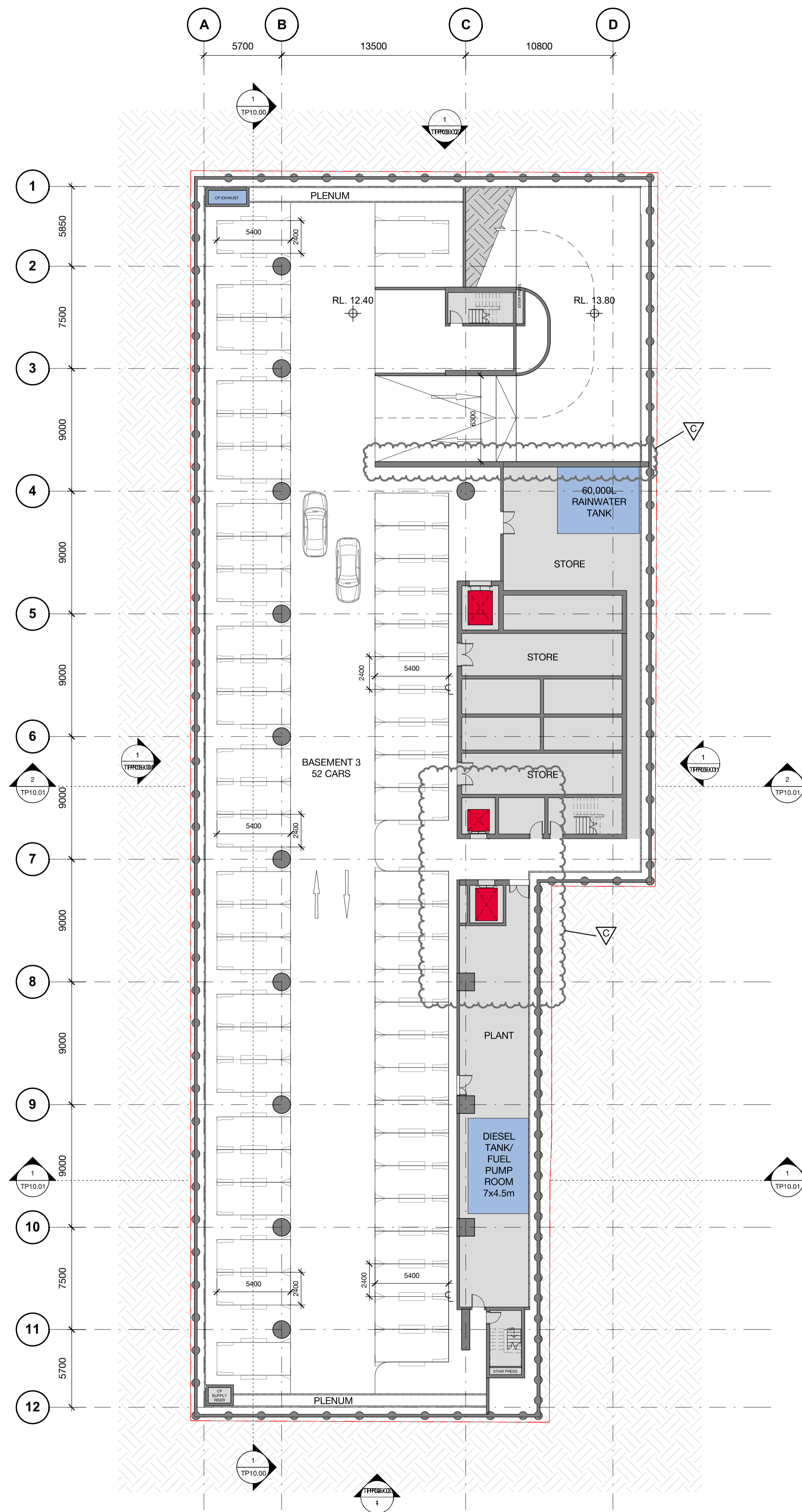
Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:24:04 PM		
BIM			

Drawing no.	Revision
<b>TP03.B02</b>	<b>C</b>

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Rev	Date	Description	Initial	Checked
C	06.11.20	Draft DELWP RFI Response	CP	CP
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

General Arrangement Plan  
Basement Level 03



Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:24:43 PM		
BIM			

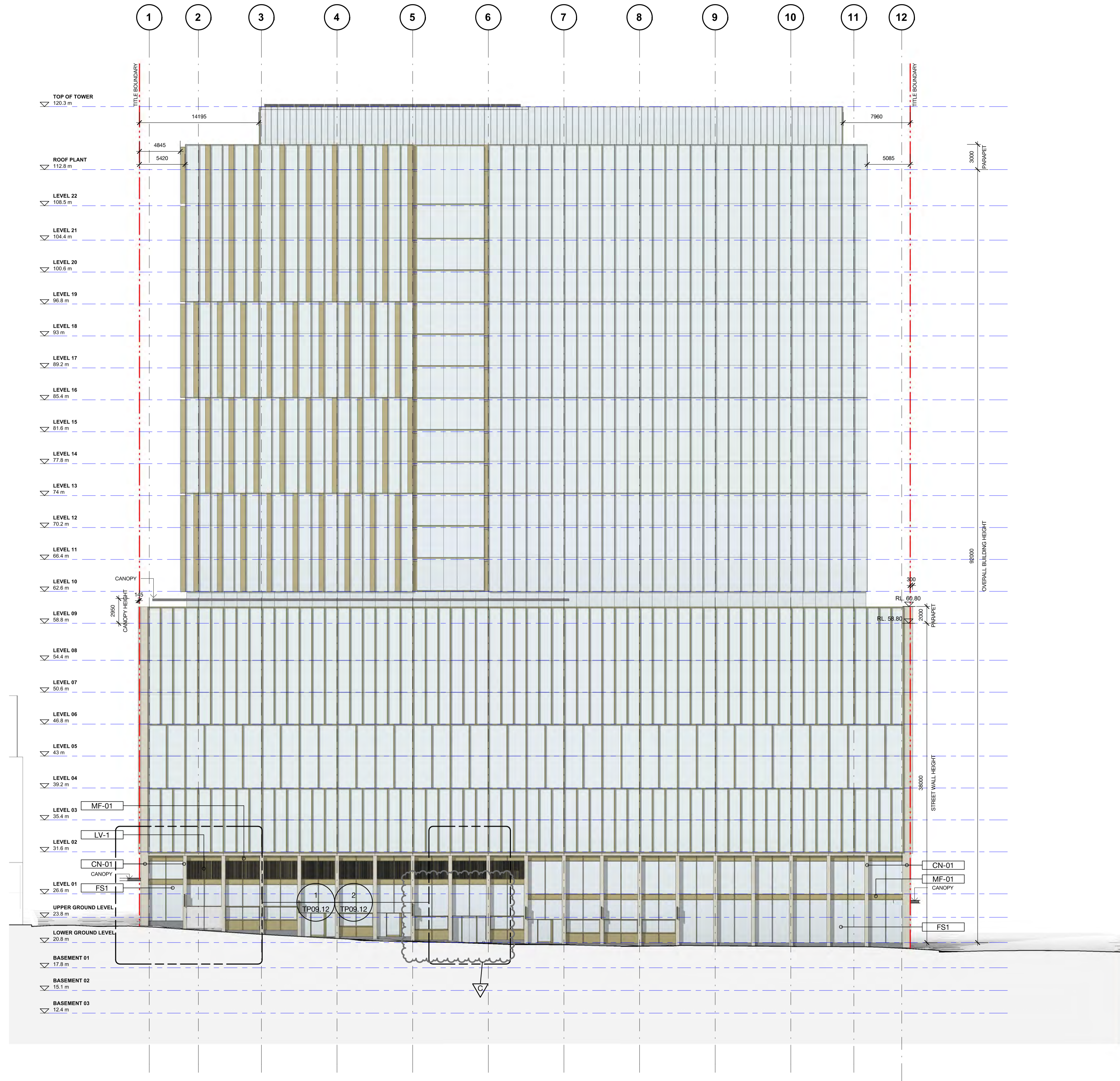
Drawing no. **TP03.B03** Revision **C**

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**FACADE SYSTEMS**

- FS1 Clear vision shop front IGU glazing
- FS2 Clear vision high performance double glazed IGU with expressed masonry fin
- FS3 Clear vision high performance double glazed IGU with deep expressed metallic fin
- FS4 Clear vision high performance double glazed IGU with vertical expressed metallic fin
- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawm cut basalt

Rev	Date	Description	Initial	Checked
D	09.11.20	DELWP RFI Responses	AA	
C	06.11.20	Draft DELWP RFI Response	CP	
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**External Elevations  
West Elevation**



Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:26:56 PM		

BIM	
Drawing no.	Revision
<b>TP09.00</b>	<b>D</b>

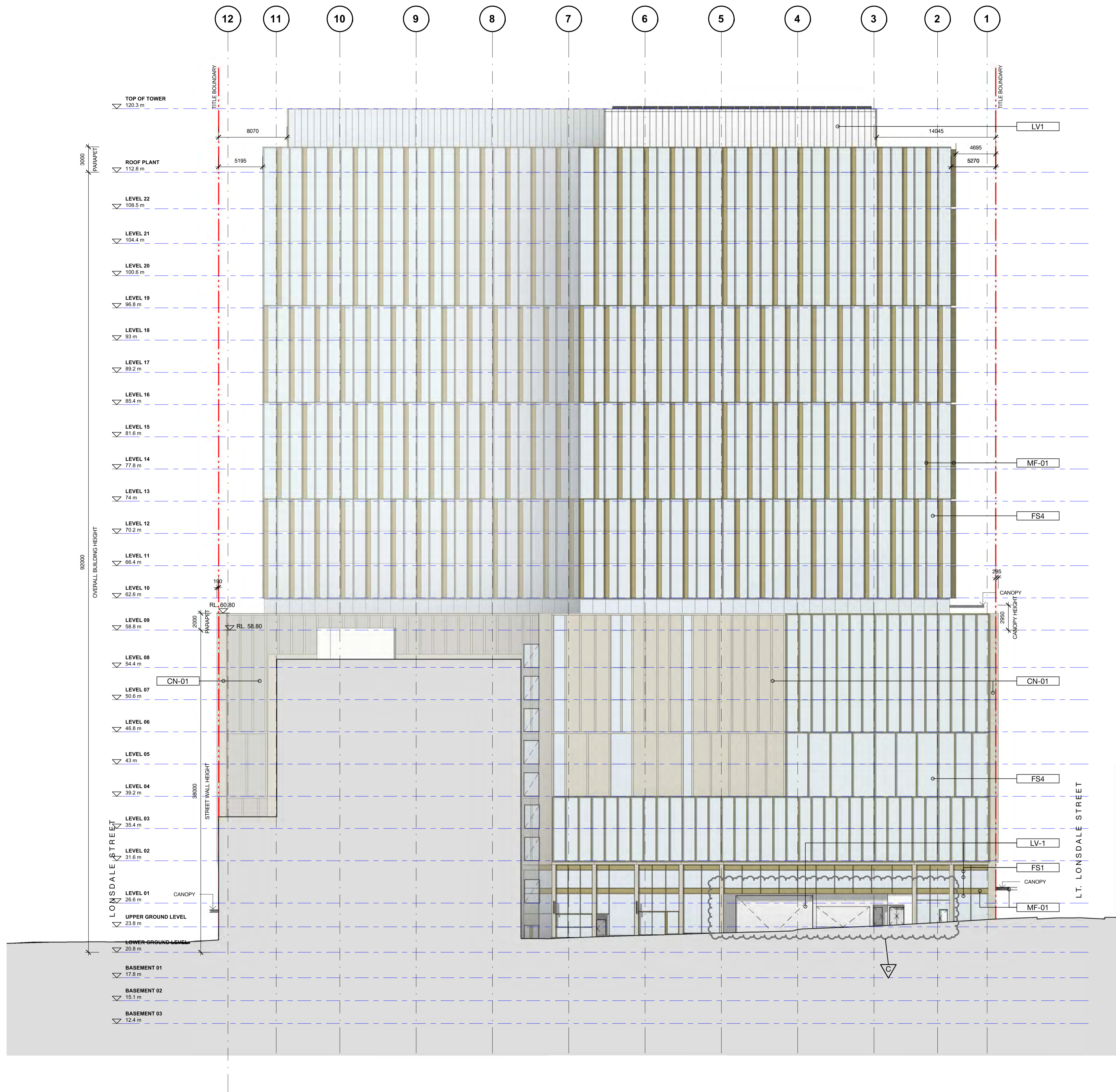
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1 West Elevation  
1 : 250

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**FACADE SYSTEMS**

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- FS2 Clear vision high performance double glazed IGU with expressed masonry fin
- FS3 Clear vision high performance double glazed IGU with deep expressed metallic fin
- FS4 Clear vision high performance double glazed IGU with vertical expressed metallic fin
- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawn cut basalt

Rev	Date	Description	Initial	Checked
D	09.11.20	DELWP RFI Responses	AA	
C	06.11.20	Draft DELWP RFI Response	CP	
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**External Elevations  
East Elevation**



Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:30:15 PM		

BIM	
Drawing no.	Revision
<b>TP09.01</b>	<b>D</b>

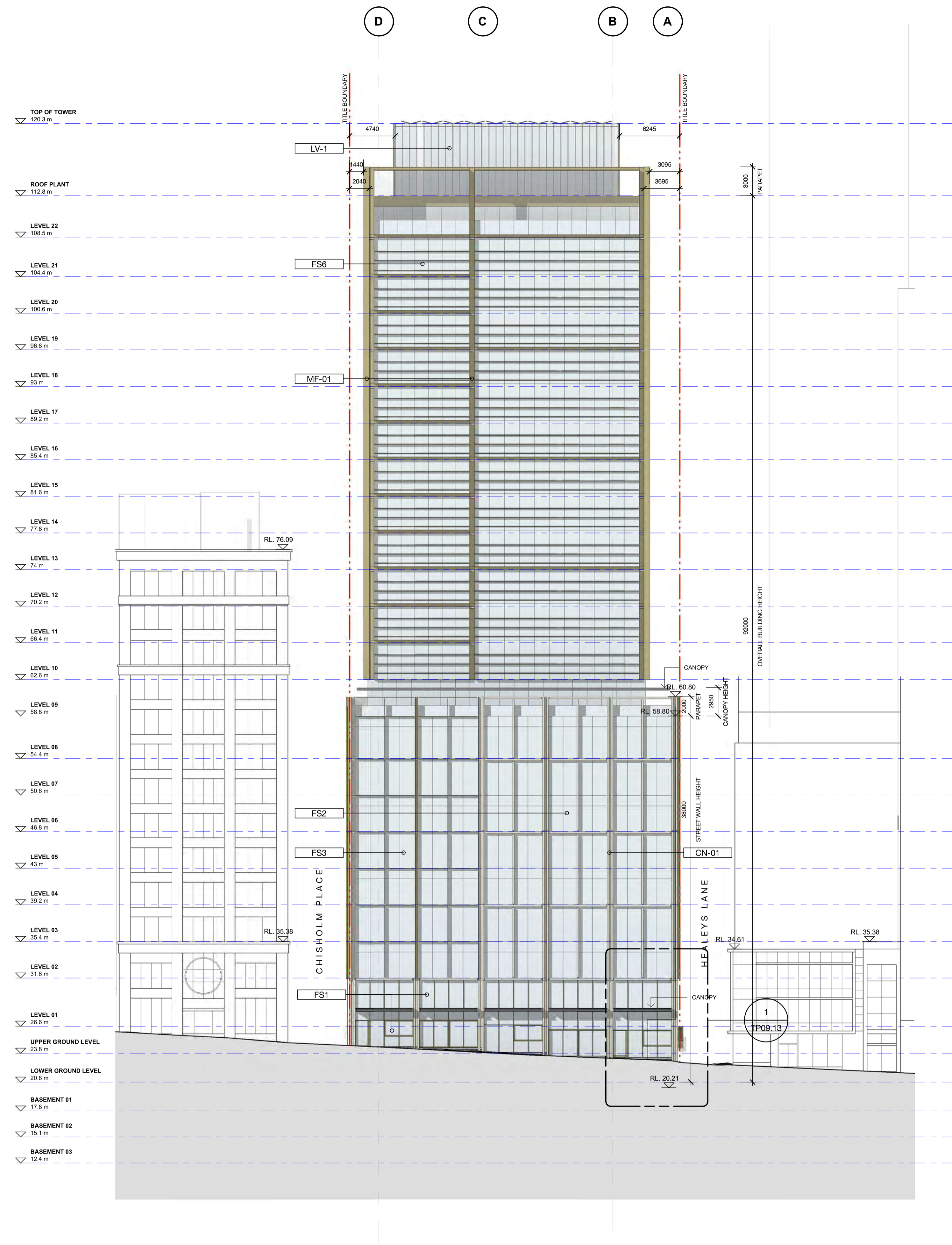
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1 East Elevation  
1 : 250





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**FACADE SYSTEMS**

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- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawn cut basalt

B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA
Rev	Date	Description	Initial	Checked

**550 Lonsdale Street  
Melbourne**

**External Elevations  
North Elevation**



Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:31:15 PM		
BIM			

Drawing no.	Revision
<b>TP09.02</b>	<b>B</b>

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1 North Elevation  
1 : 250



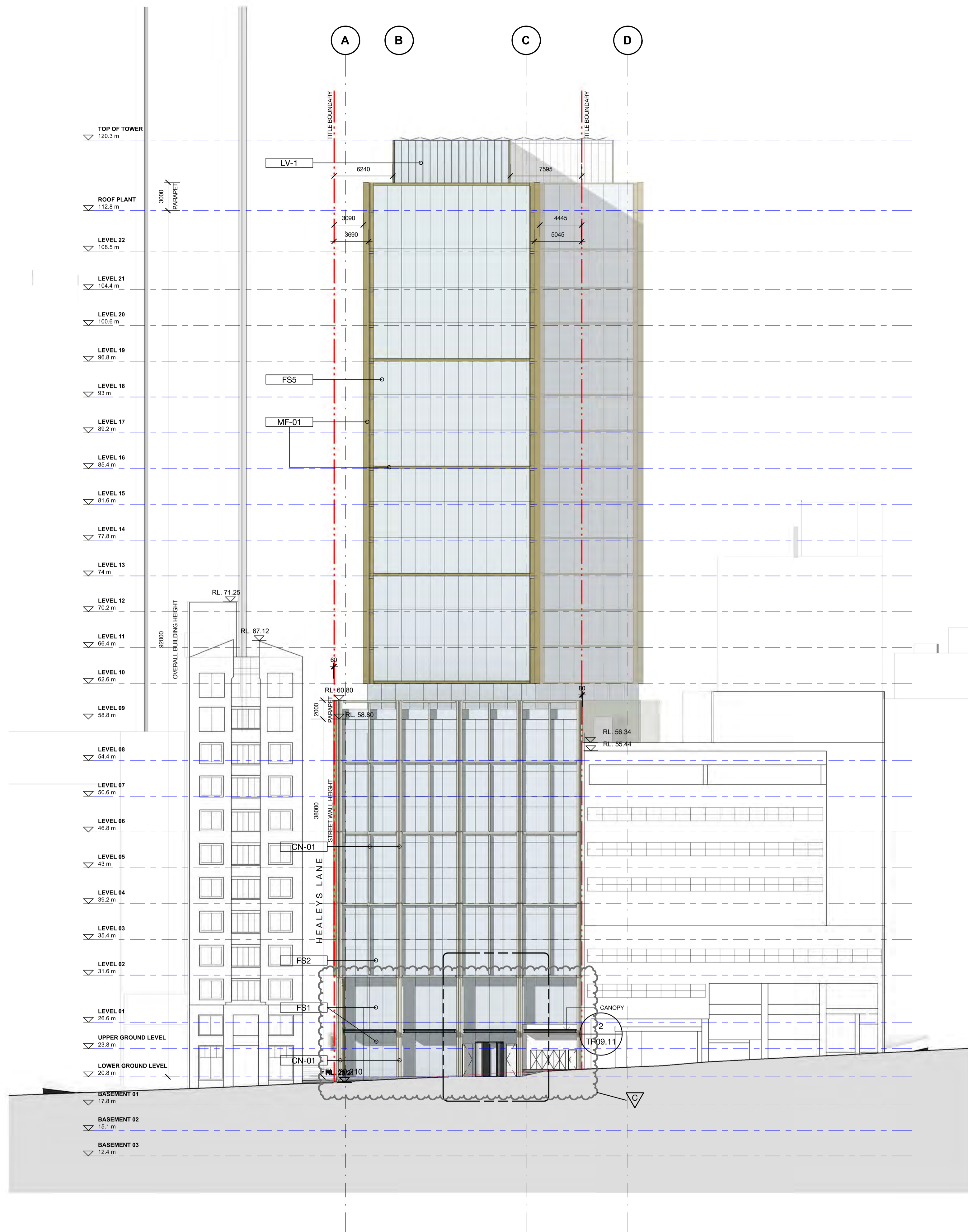
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- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawm cut basalt



Rev	Date	Description	Initial	Checked
D	09.11.20	DELWP RFI Responses	AA	
C	06.11.20	Draft DELWP RFI Response	CP	
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

**550 Lonsdale Street  
Melbourne**

**External Elevations  
South Elevation**



Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:32:23 PM		
BIM			

Drawing no.	Revision
<b>TP09.03</b>	<b>D</b>

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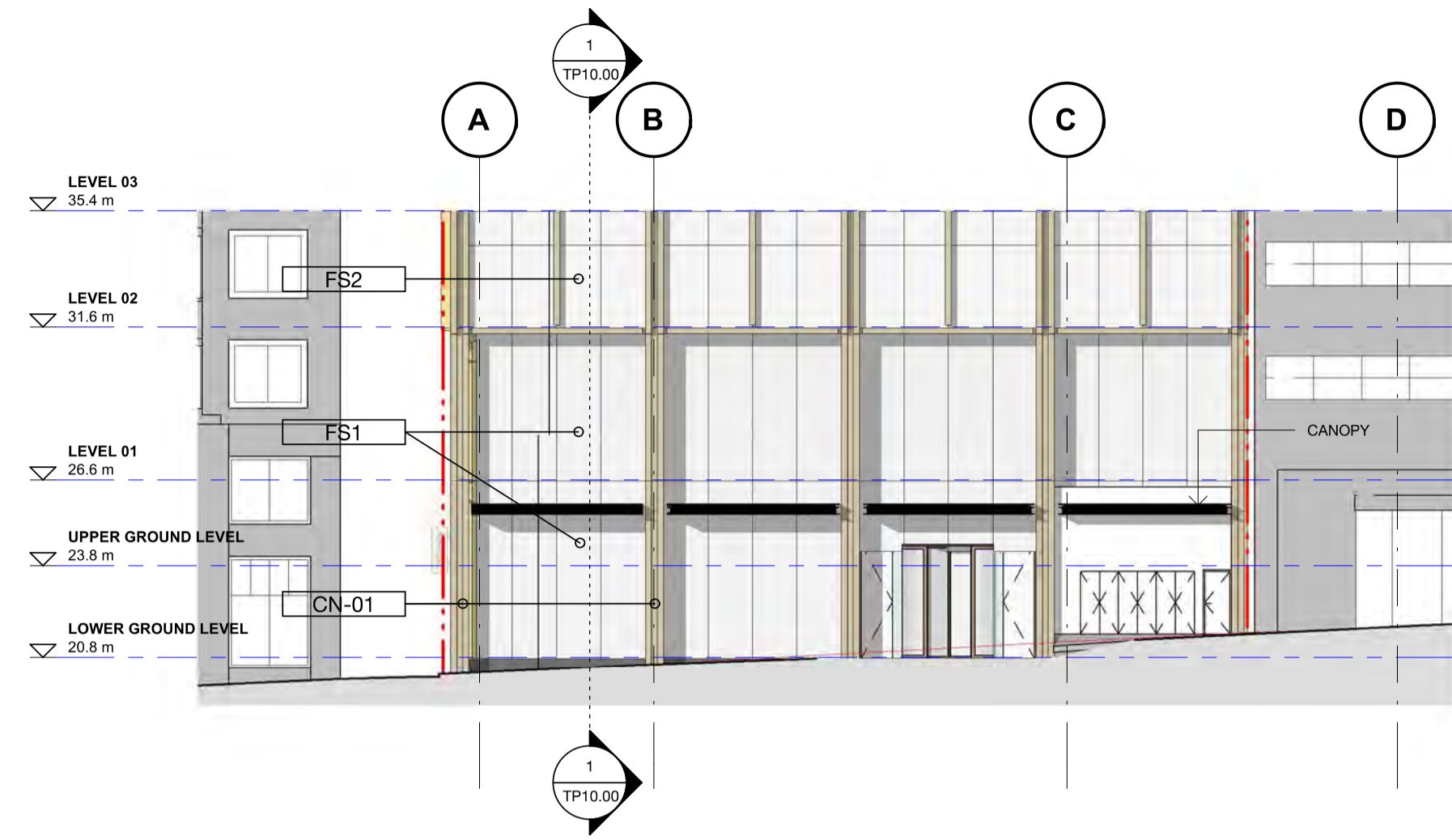
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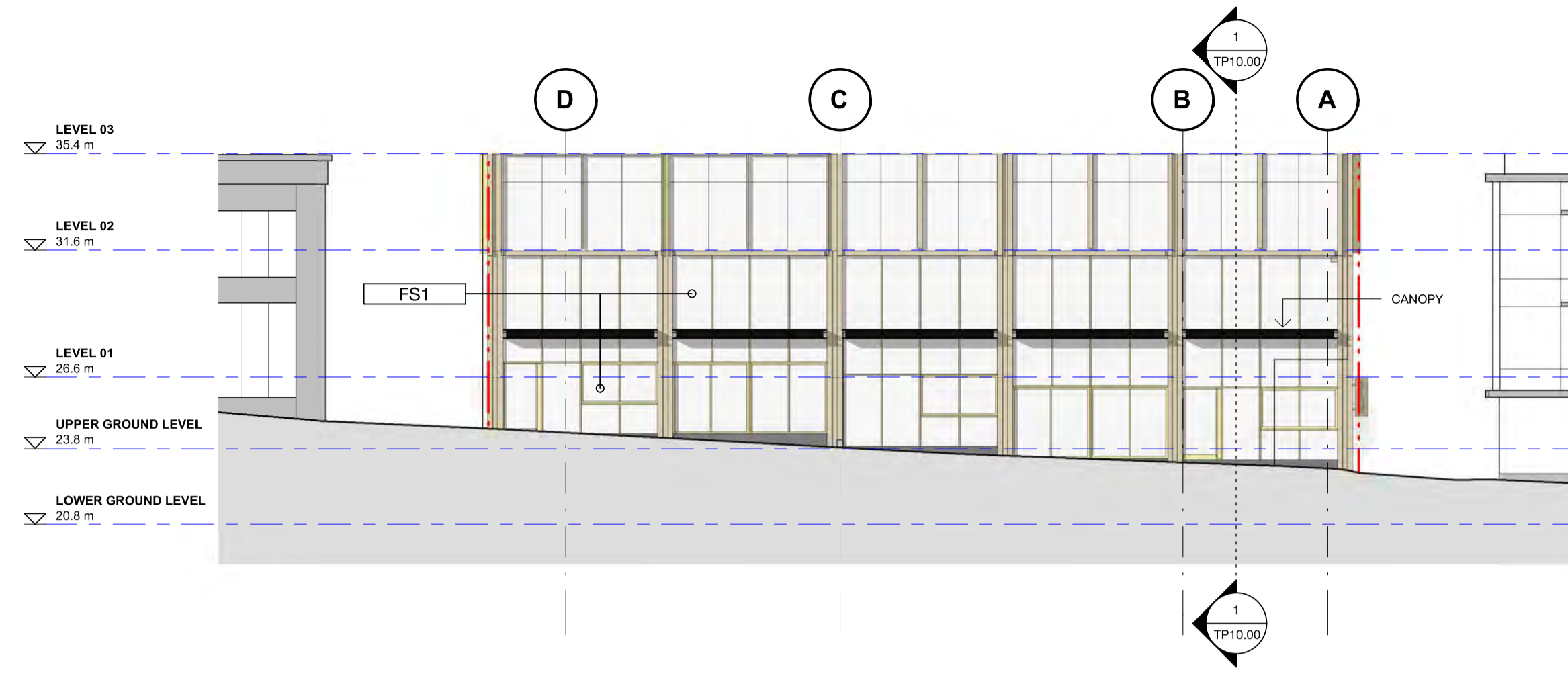
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1 : 250

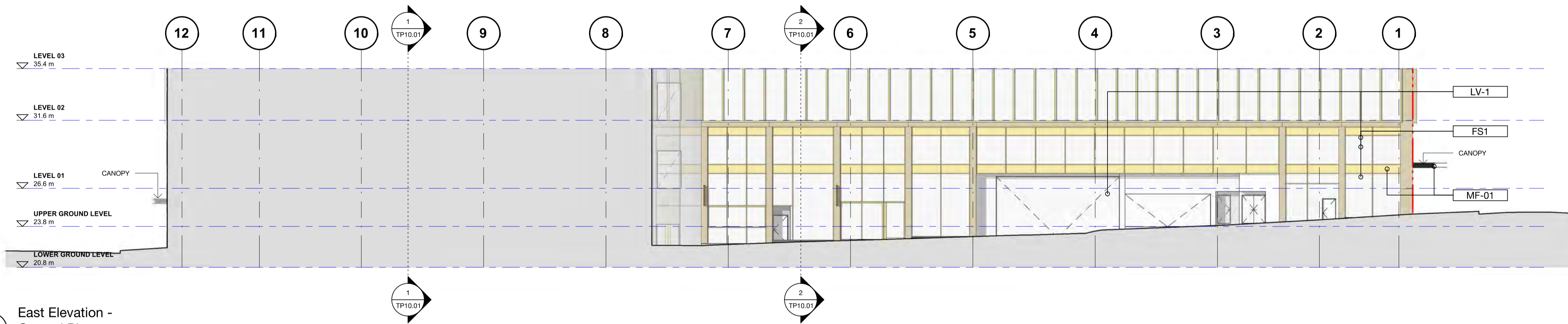
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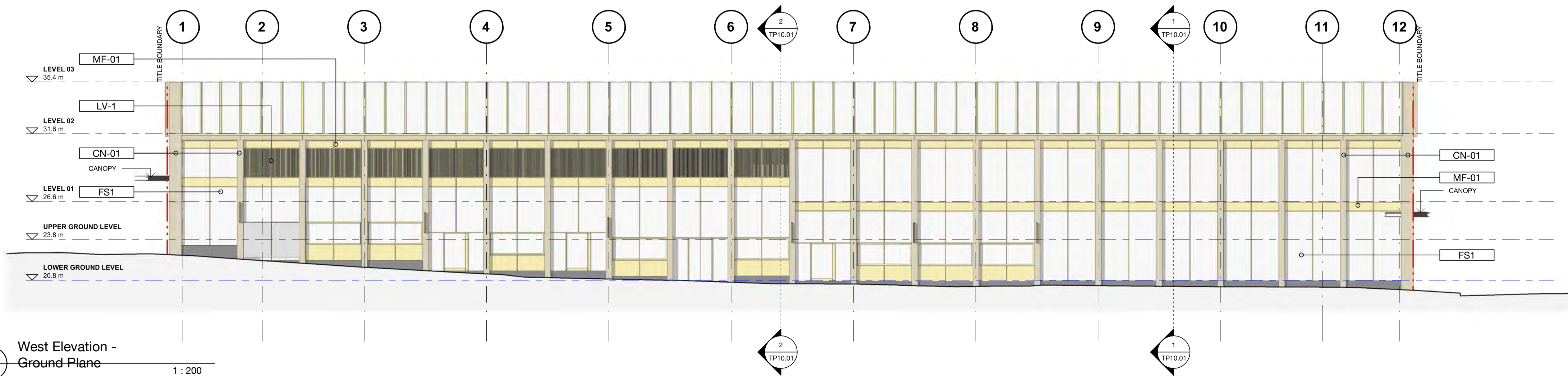
4 South Elevation - Ground Plane 1 : 200



3 North Elevation - Ground Plane 1 : 200



2 East Elevation - Ground Plane 1 : 200



1 West Elevation - Ground Plane 1 : 200

Rev	Date	Description	Initial	Checked
A	09.11.20	DELWP RFI Responses	AA	

550 Lonsdale Street  
Melbourne

Ground Plane Elevations

Status	TOWN PLANNING		
Scale	1 : 200	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:32:57 PM		
BIM			

Drawing no.	Revision
TP09.10	A

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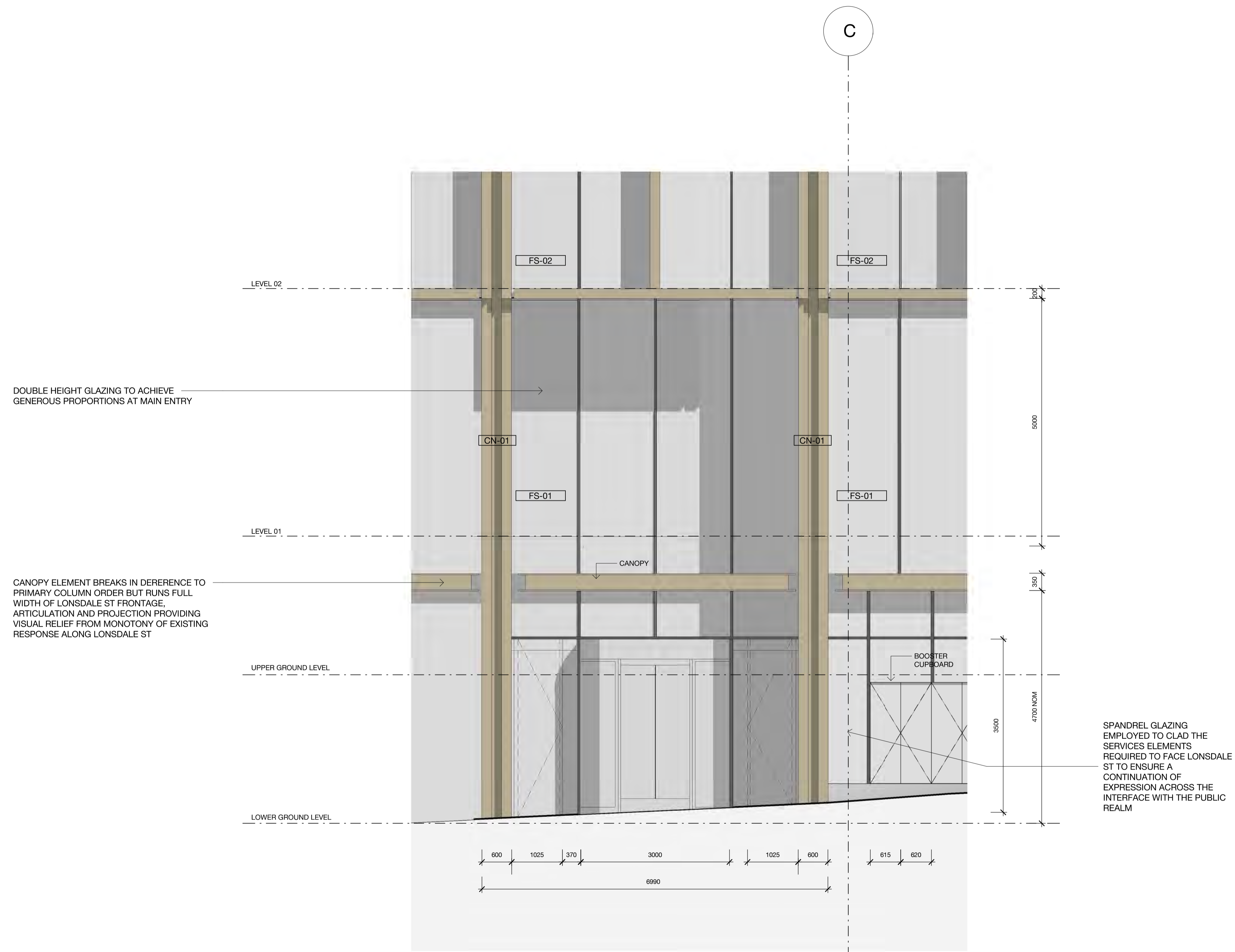
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- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawn cut basalt



Rev	Date	Description	Initial	Checked
A	09.11.20	DELWP RFI Responses	AA	

**550 Lonsdale Street  
Melbourne**

**External Elevations  
Detailed South Elevation**



Status	TOWN PLANNING		
Scale	As indicated	@ A1	
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:37:11 PM		

Drawing no.	Revision
TP09.11	A

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2 South Elevation - Detail  
1 : 50

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**FACADE SYSTEMS**

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- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawn cut basalt



Rev	Date	Description	Initial	Checked
A	09.11.20	DELWP RFI Responses	AA	

**550 Lonsdale Street  
Melbourne**  
 External Elevations  
 Detailed West Elevations

Status	TOWN PLANNING		
Scale	As indicated	@ A1	
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	10/11/2020 11:09:00 AM		
BIM			

Drawing no. **TP09.12** Revision **A**

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**FACADE SYSTEMS**

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- FS5 Clear vision high performance double glazed IGU
- FS6 Clear vision high performance double glazed IGU with deep expressed horizontal fin
- CN-01 Feature masonry
- MF-01 Light gold metal frame or panel
- LV-1 Light metal vertical louvre cladding to plant areas
- ST-01 Sawm cut basalt



Rev	Date	Description	Initial	Checked
A	09.11.20	DELWP RFI Responses	AA	

550 Lonsdale Street  
Melbourne

External Elevations  
Detailed North Elevation



Status	TOWN PLANNING		
Scale	As indicated	@ A1	
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 9:33:50 PM		

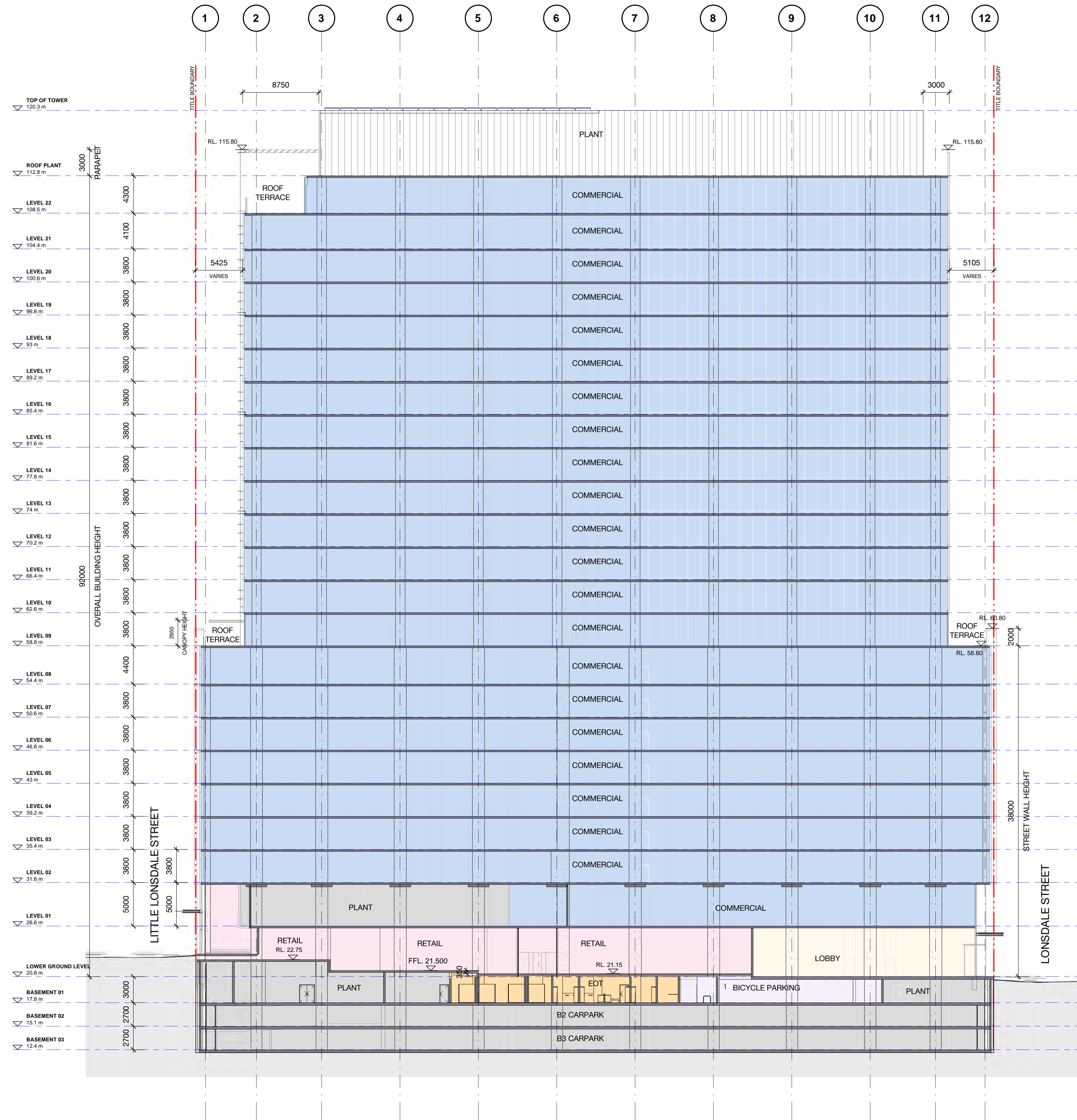
Drawing no.	Revision
TP09.13	A

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**BATESSMART**



1 Section AA  
Section  
1 : 250

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Do not scale drawings - refer to figured dimensions only. Any discrepancies shall immediately be referred to the architect for clarification.  
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Rev	Date	Description	Initial	Checked
B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA

### 550 Lonsdale Street Melbourne

#### Building Sections Section AA

Status	TOWN PLANNING		
Scale	1 : 250	@	A1
Drawn	Author	Checked	Checker
Project No.	M12391		
Plot Date	9/11/2020 5:13:35 PM		
BIM			
Drawing no.	TP10.00	Revision	B

Melbourne 1 Nicholson Street  
Melbourne VIC 3000 Australia  
T 03 8664 6200 F 03 8664 6300  
email mel@batesmart.com.au  
http://www.batesmart.com.au

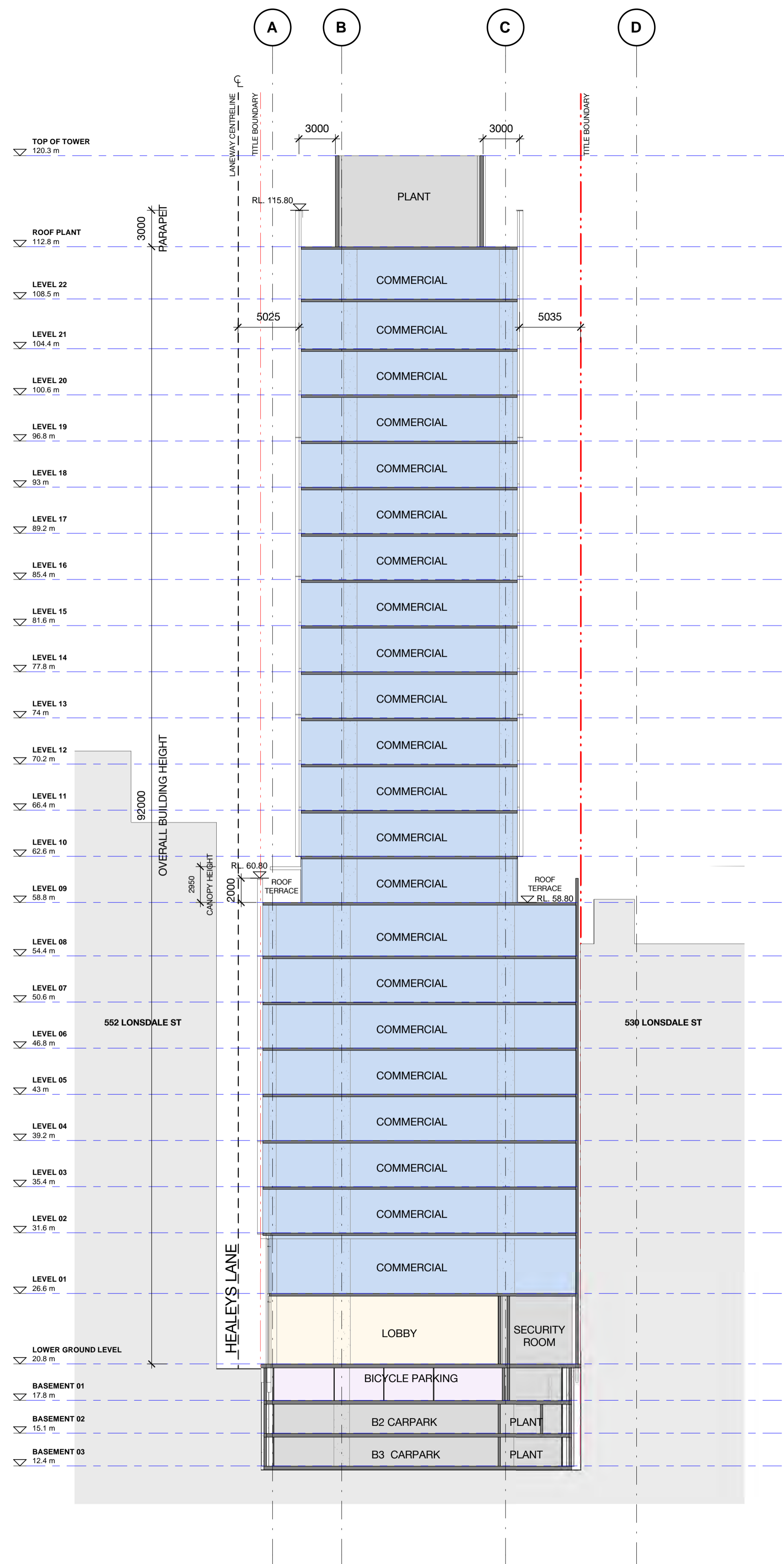
Sydney 43 Brisbane Street  
Surry Hills NSW 2010 Australia  
T 02 8354 5100 F 02 8354 5199  
email syd@batesmart.com.au  
http://www.batesmart.com.au

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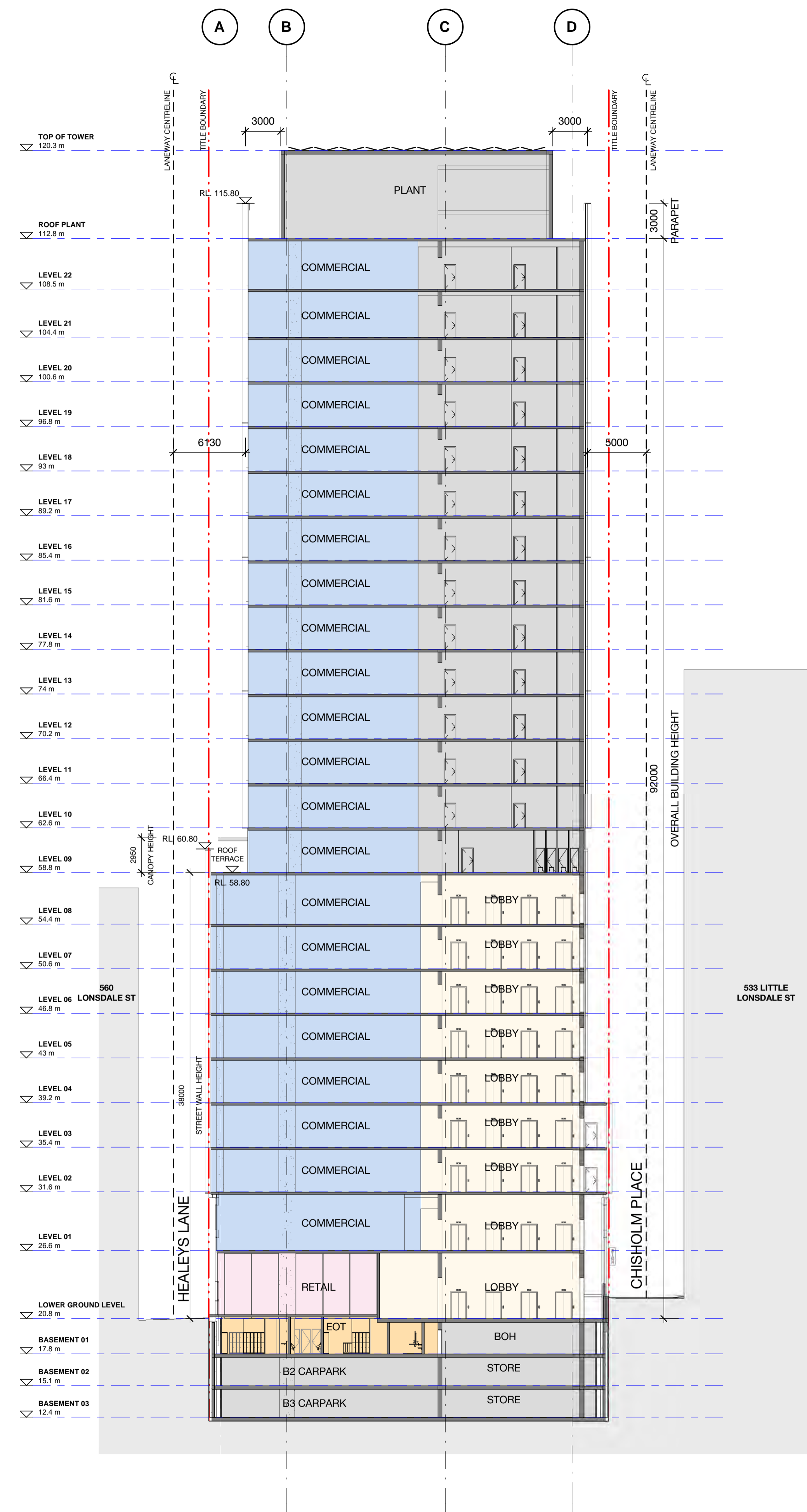
Check all dimensions and site conditions prior to commencement of any work, the purchase or ordering of any materials, fittings, plant, services or equipment and the preparation of shop drawings and or the fabrication of any components.

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1 Section BB  
Section  
1 : 250



2 Section CC  
Section  
1 : 250

B	28.07.20	DELWP RFI Response	MM	CP
A	12.06.20	Town Planning	CP	AA
Rev	Date	Description	Initial	Checked

### 550 Lonsdale Street Melbourne

#### Building Sections Section BB & Section CC

Status	TOWN PLANNING			
Scale	1 : 250	@	A1	
Drawn	Author	Checked	Checker	
Project No.	M12391			
Plot Date	9/11/2020 5:14:04 PM			

BIM

Drawing no.	Revision
<b>TP10.01</b>	<b>B</b>

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Bates Smart Pty Ltd ABN 70 004 999 400

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**PLANNING REPORT MINISTERIAL REFERRAL**

<b>Application number:</b>	<b>TPMR-2020-53</b>
<b>DTPLI Application number:</b>	PA2000895
<b>Applicant:</b>	Lonsdale Investment CD Pty Ltd ATF Lonsdale Unit Trust C/ Urbis Pty Ltd
<b>Owner:</b>	Lonsdale Investment CD Pty Ltd ATF Lonsdale Unit Trust
<b>Architect:</b>	Bates Smart Architecture
<b>Address:</b>	550 Lonsdale Street, Melbourne VIC 3000
<b>Proposal:</b>	Demolition and buildings and works to construct a multi-level building (Office and Retail Premises)
<b>Cost of works:</b>	\$183,250,000.00
<b>Date received by CoM:</b>	26 June 2020
<b>Responsible officer:</b>	Ryan Cottrell, Senior Urban Planner

**1 SITE AND SURROUNDS****1.1 Subject Site**

The subject site (the Site) is located on the north side of Lonsdale Street, between King Street and William Street, Melbourne (Figure 1).

The Site has a Lonsdale Street frontage that measures 26.305 metres, a secondary frontage to Little Lonsdale Street that measures 34.27 metres, a maximum depth of 91.70 metres, and a total area of 2,828 square metres. The Site also contains frontages to Healeys Lane and Chisholm Place. The southern third of Healeys Lane is pedestrianised and not accessible to cars.

The Site's plan of subdivision (PS 427313 X) contains an easement referred to as 'E-1' that relates to drainage which runs east to west through the site that is 2 metres in width.

The southern portion of the Site is currently developed with a nine storey concrete and glass building with ground floor retail. The Lonsdale Street frontage has a two storey undercroft with recessed retail at ground level (Figure 2).

The building at the southern portion of the Site is set back approximately 3 metres from Healeys Lane for approximately 40 metres in length (from Lonsdale Street towards the centre of the Site).

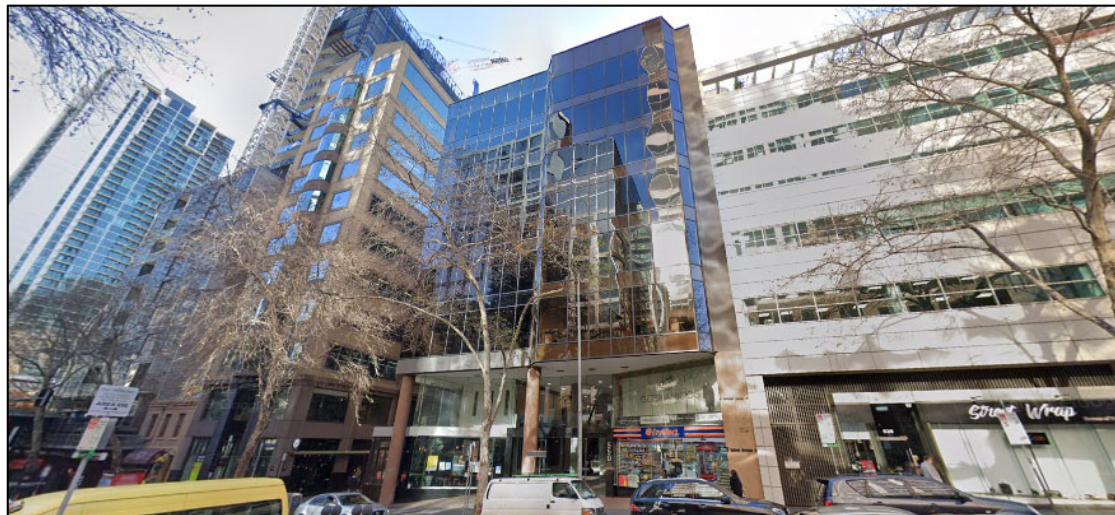
The rear of the Site fronts Little Lonsdale Street and contains a thirteen storey concrete and glass building. This section of the Site has ground level retail and car parking access (Figure 3). This building is constructed to the north, east, and west boundaries.

The site is not included in the City of Melbourne's Heritage Places Inventory February 2020 Part A (Amended July 2020) or Part B.

Figure 1 – Locality map



Figure 2 – The Site, street view (Lonsdale Street)



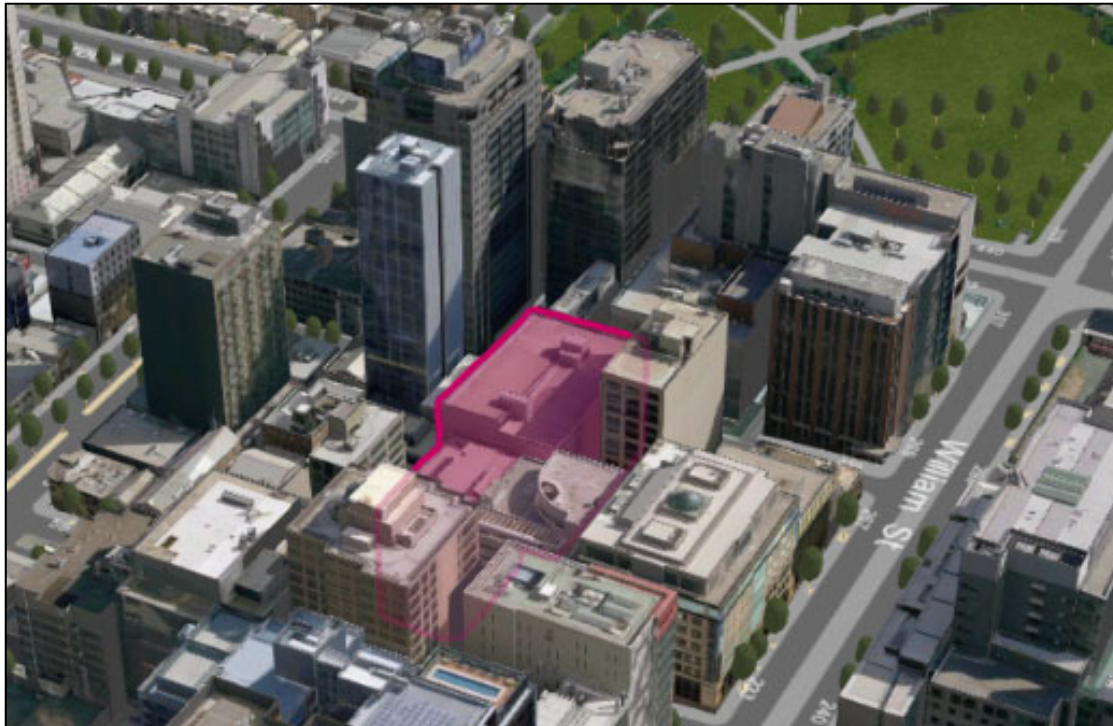
**Figure 3 – The site, street view (Little Lonsdale Street)**



## 1.2 Site Surrounds

The immediate surrounds contain a broad range of uses and built form typologies. Generally, the built form surrounding the Site is developed with several larger scale contemporary buildings used for office, residential and retail purposes (Figure 4).

**Figure 4 – 3D view of the surrounds (Looking north-west)**



The surrounding area has been summarised as follows:

North

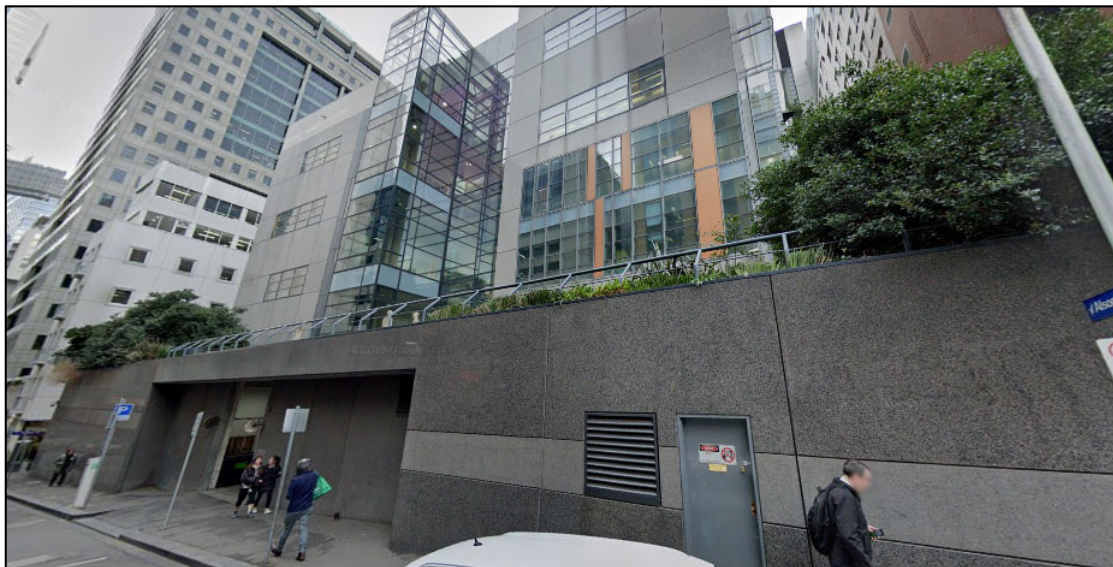
Across Little Lonsdale Street to the north, at 305 William Street, is the Commonwealth Law Courts building. The court is contained within an eighteen storey building that was built in 1999. Of relevance is the Little Lonsdale frontage of the site which has a single storey access and services frontage with a landscaped terrace above (Figure 5). The main part of the building at this side of the site is setback approximately seven metres from Little Lonsdale Street.

To the north, at 466-468 Little Lonsdale Street, is a seven storey concrete office building with a basement and ground level retail that was built in 2004 (Figure 6).

To the north-west of the Site, at 470-472 Little Lonsdale Street, is a two storey brick building that was built in 1873. The building is constructed to its site boundaries and presently used for retail (Figure 6). This site has been afforded a 'C' grading in the Central City Heritage Review Study 1994.

To the north-west of the Site, at 474 Little Lonsdale Street, is a two storey brick building that was built in 1920 (Figure 6). The building is constructed to its site boundaries and presently used for retail. This site has been afforded a 'C' grading in the Central City Heritage Review Study 1994.

**Figure 5 - Rear of the Commonwealth Law Courts building**



**Figure 6 - 466-474 Little Lonsdale Street (Right to left)**



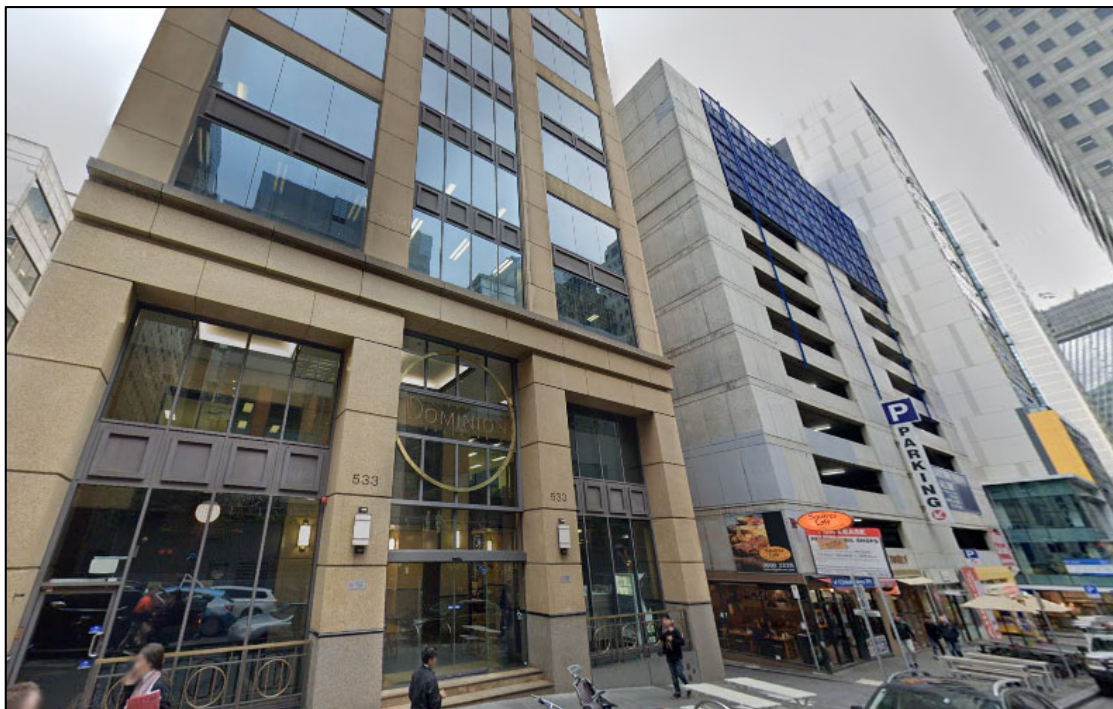
East

Adjoining the Site to the east, at 530 Lonsdale Street is a nine storey concrete office building that was constructed in 1957. This building is built to its site boundaries up to the third level where it is then setback approximately six metres from Lonsdale Street. Above this the building is constructed to its side and rear boundaries.

Further east at 233 Lonsdale Street is the Melbourne Magistrates Court. This site is developed with nine store concrete building.

Across Chisholm Place to the east of the Site, at 533 Little Lonsdale Street, is thirteen storey concrete office building that is known as the Dominion Building (Figure 7). This building is constructed to all boundaries all the way up.

**Figure 7 – 533 Little Lonsdale Street (left) and the Site**



### South

Across Lonsdale Street to the south, at 525 Lonsdale Street, is a nineteen storey concrete office building that was constructed in 1987. The podium of the building is built to the Lonsdale Street and side boundaries up to the fourth level where the tower is then setback approximately 4 metres from Lonsdale Street (Figure 8).

Across Lonsdale Street to the south, at 555 Lonsdale Street, is a thirteen storey concrete office building that was constructed in 1989. The building is predominately built to all of its site boundaries (Figure 8).

**Figure 8 – 555 Lonsdale Street (right) and 525 Lonsdale Street (left)**



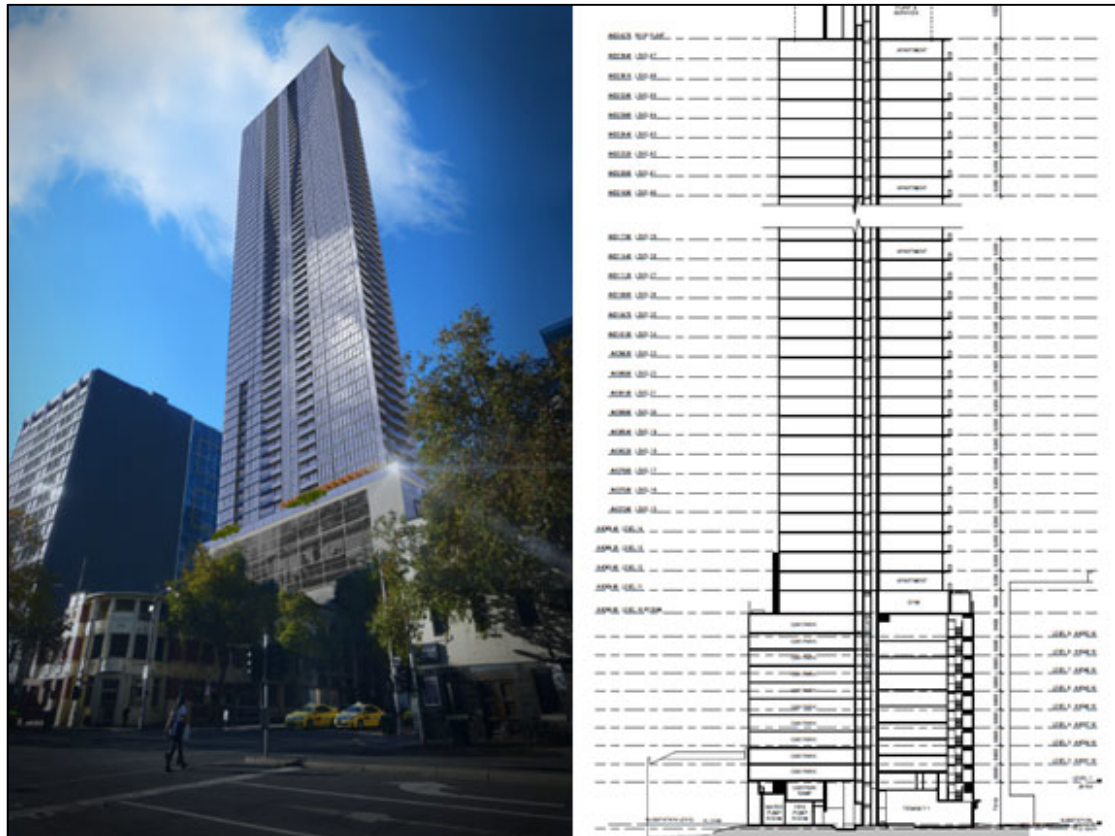
### West

Across Healeys Lane to the west of the Site, at 551-555 Little Lonsdale Street, is a three storey glass office building with ground level retail that was built in 2009.

Across Healeys Lane to the west of the Site, at 556-560 Lonsdale Street, is construction site that is currently erecting a 57 level residential building that was approved by the Minister for Planning (Figure 9). This development contains a 34.75 metre high podium that is built to the site boundaries with a tower that is set back 5.05 metres from Lonsdale Street, 3.83 metres from Healeys Lane, and 5.02 metres from Little Lonsdale Street.

Across Healeys Lane to the west, at 552-554 Lonsdale Street, is a twelve storey concrete office building with ground level retail that was built in 1991. This building is constructed to all boundaries all the way up.

Figure 9 – 556-560 Lonsdale Street, Melbourne



## 2 BACKGROUND AND HISTORY

### 2.1 Planning Application History

The following applications, listed as considered relevant to the current proposal, have previously been considered for the Site and surrounding sites (Table 1):

Table 1: Planning Application History			
Reference	Property Address	Description of Proposal	Decision & Date
TP-2006-1212	540-550 Lonsdale Street, Melbourne	Construct an additional floor over existing 12 storey building	Permit: 25/01/2007
TP-2009-520	540-550 Lonsdale Street, Melbourne	Use of the land as a childcare centre and buildings and works (including additional levels) to the existing building	Permit: 27/11/2009
TPM-2015-15	556-560 Lonsdale Street, Melbourne	Demolition of the existing building and construction of a multi-level residential building (57 levels).	Completed: 12/12/2016
PAM-2020-208	540-550 Lonsdale Street, Melbourne	Pre-Application with DELWP Discussions on preferred urban design outcomes	Permit issued: 17/12/2019

Pre-application PAM-2020-208 is of relevance as it relates to advice sought by the applicant prior to the lodging of this permit application. The following comments were made (summarised):

- In principal support was given for the Site's redevelopment.
- The podium / tower format and height were supported.
- The importance of retaining activation at Healeys Lane and Chisholm Place was raised.

## 2.2 Amendments throughout the process

The applicant submitted discussion drawings to the Department of Environment, Land, Water, and Planning (DELWP) in response to various issues raised by the City of Melbourne's Urban Design and Traffic departments. These discussion drawings were received by the City of Melbourne on 26 November 2020. A summary of the changes includes:

- Reducing the size of the loading bays at Chisholm Place.
- Increase pedestrian access to the building via Chisholm Place.
- An increased thoroughfare for pedestrians along Healeys Lane.
- Removing entrapment areas at the Lonsdale Street interface by bringing the building to the boundary.
- Revising the waste management plan to accommodate the above.

## 2.3 Planning Scheme Amendments

Upcoming Melbourne Planning Scheme Amendment C308 is of relevance as it seeks to refresh Design and Development Overlay Schedule 1 (DDO1) through consolidating several Design and Development Overlays and bringing them into line with best practice.

Amendment C308 is of relevance to this application as it inserts stronger policy relating to high quality, pedestrian oriented built form with activated street frontages.

The Site is included in the Central City area affected by the proposed amendment and is not included in a Special Character Area which means there are no mandatory public interface requirements that would impact on the decision making process for this application.

## 3 PROPOSAL

### 3.1 Plans / Reports considered in assessment

The plans which have been considered in this assessment are identified in Table 2 below:

Table 2: Plans / Reports considered in assessment		
Plan / Report Title	Drawing/ Report No.	Date Stamped / dated
Metropolitan Planning Levy (MPL)	MPLCERT16085	18/06//2020



Copy of Title	Volume 10572, Folio 221	18/06/2019
Planning Report	Urbis	04/06/2020, Revised 29/07/2020
Response to RFI Letter	Urbis	22/07/2020
Façade Strategy Report	Bates Smart	07/2020
Urban Context Report	Bates Smart	07/2020
Development Plans (Bates Smart Architecture)	Drawing No. TP01.00, TP01.01, TP01.02, TP01.03, TP03.0GF, TP03.01, TP03.02, TP03.013, TP03.04, TP03.09, TP03.10, TP03.22, TP03.23, TP03.24, TP03.B01, TP03.B02, TP03.B03, TP09.00, TP09.01, TP09.02, TP09.03, TP10.00, and TP10.01.	04/09/2020
Discussion Plans (Bates Smart)	Drawing No. TP01.00, TP01.01, TP01.02, TP01.03, <b>TP01.04</b> , TP03.0GF, TP03.01, TP03.02, TP03.013, TP03.04, TP03.09, TP03.10, TP03.22, TP03.23, TP03.24, TP03.B01, TP03.B02, TP03.B03, TP09.00, TP09.01, TP09.02, TP09.03, TP10.00, and TP10.01.	Dated 9/11/2020
Environmentally Sustainable Design (ESD) Statement and Water Sensitive Urban Design Response	Ark Resources	24/07/2020
Waste Management Plan	Leigh Design Pty Ltd	10/11/2020
Traffic Impact Assessment	GTA Consultants	10/06/2020
Wind Impact Assessment	Mel Consultants Pty Ltd	05/06/2020, amended 27/07/2020

### 3.2 Summary of proposed development

The proposal, as shown on the plans referenced in Table 2 above, seeks planning approval for demolition, buildings and works to construct a multi-storey mixed use building.

A summary of the key features of the development include:

- Complete demolition of the buildings on the Site.
- Removal of an above ground car park.
- Construction of a 23 storey building to include retail premises at ground level and offices above.
- Three basements containing car parking and end of trip facilities.
- The development proposes a podium / tower format.

- Access to the site is possible for pedestrians via Lonsdale Street, Healeys Lane, Chisholm Place and Little Lonsdale Street.
- Access to the site for cars and loading is via Chisholm Place which is accessed via Little Lonsdale Street.
- End of trip facilities are also accessed via Chisholm Place off Little Lonsdale Street.
- The proposal contains active uses (retail and office lobby) at the Lonsdale Street, Little Lonsdale Street, and Healeys Lane frontages as well as street awnings to Lonsdale Street and Little Lonsdale Street.
- The development proposes untinted glazing, sandstone coloured masonry fins, sandstone coloured metallic fins, light gold coloured louvres and light gold coloured frames and panels.

### 3.3 Detailed Information

<b>Table 3: Summary of proposed development</b>	
Site Area	2,828 square metres
Floor Area Uplift	2,138 square metres
Total Gross Floor Area	61,532 square metres
Total Gross Floor Area above ground	53,042 square metres
Floor Area Ratio	18.76:1 (53,042 (GFA above ground) / 2,828 (Site Area))
<b>Built Form</b>	
Number of storeys above ground level	23
Maximum Building Height	95 metres measured from the centre of the site frontage to the top of the parapet (excludes plant and lift overrun)
Number of basement levels	3
Street Wall Height	40 metres at Lonsdale Street, 37 metres at Little Lonsdale Street

Setbacks	<ul style="list-style-type: none"> <li>▪ North boundary (Little Lonsdale Street) - the podium is built to the Little Lonsdale Street boundary to a height of 37 metres with the tower set back 5.42 metres (4.845 metres from the architectural detail) from Little Lonsdale Street at the west edge of the tower and 5.27 metres (4.695 metres from the architectural detail) at the east edge of the tower</li> <li>▪ South boundary (Lonsdale Street) - the podium is built to the Lonsdale Street boundary to a height of 40 metres with the tower set back 5.085 metres from Lonsdale Street.</li> <li>▪ East boundary (Chisholm Place) - Above the podium street wall, the tower is set back 1.440 metres from the Chisholm Place boundary (5 metres from the centre on the lane (Chisholm Place).</li> <li>▪ East boundary (where the Site adjoins 530 Lonsdale Street) - above the street wall podium, the tower is set back 5.15 metres.</li> <li>▪ West boundary (Healeys Lane) - Above the podium street wall, the tower is set back 3.09 metres from the Healeys Lane boundary (5 metres from the centre on the lane (Healeys Lane).</li> </ul>
<b>Land Use</b>	
Retail (gross leasable area)	903 square metres located at ground level.
Office (Net Leasable Area)	39,261 square metres with 945 square metres of terrace.
<b>Traffic</b>	
Car parking spaces	106 at basement level 2 and 3.
Bicycle spaces	355 spaces and 40 showers at basement level 1.
Motorcycle spaces	2

### 3.4 Key Excerpts from Development Plans

Excerpts from the development plans are found at Figures 10, 11, 12, 13, 14, 15, and 16.

Figure 10 - Proposed demolition plan



Figure 11 - Proposed ground floor plan (3D view)

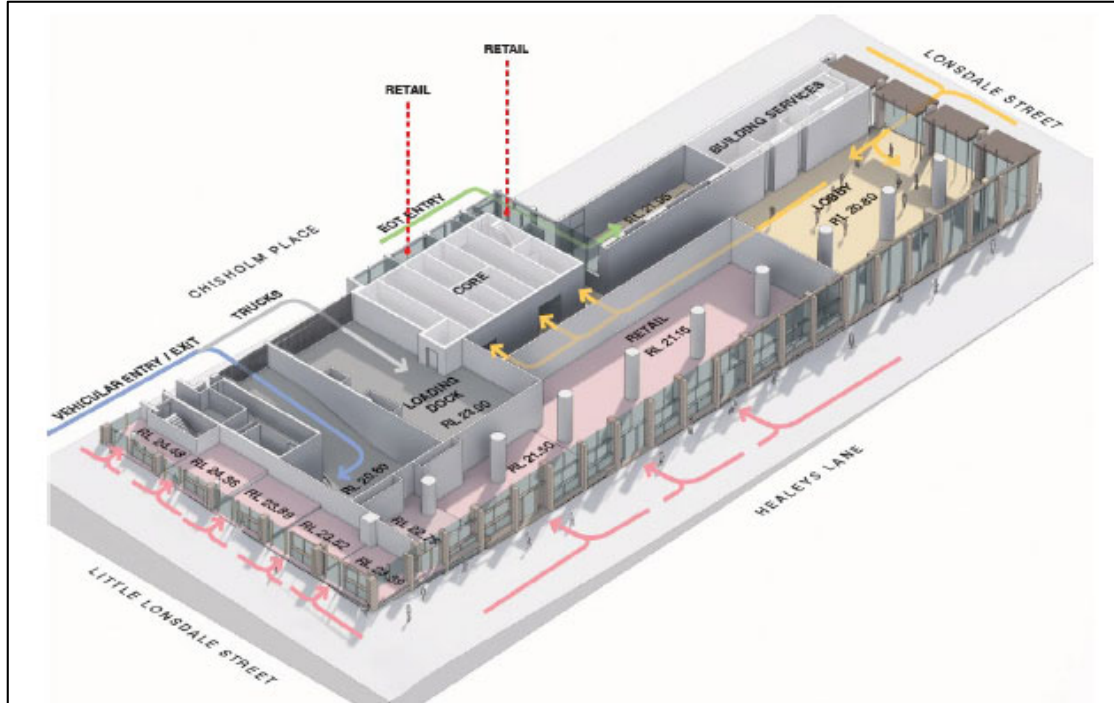


Figure 12 - Proposed Level 9 terrace and with landscaping

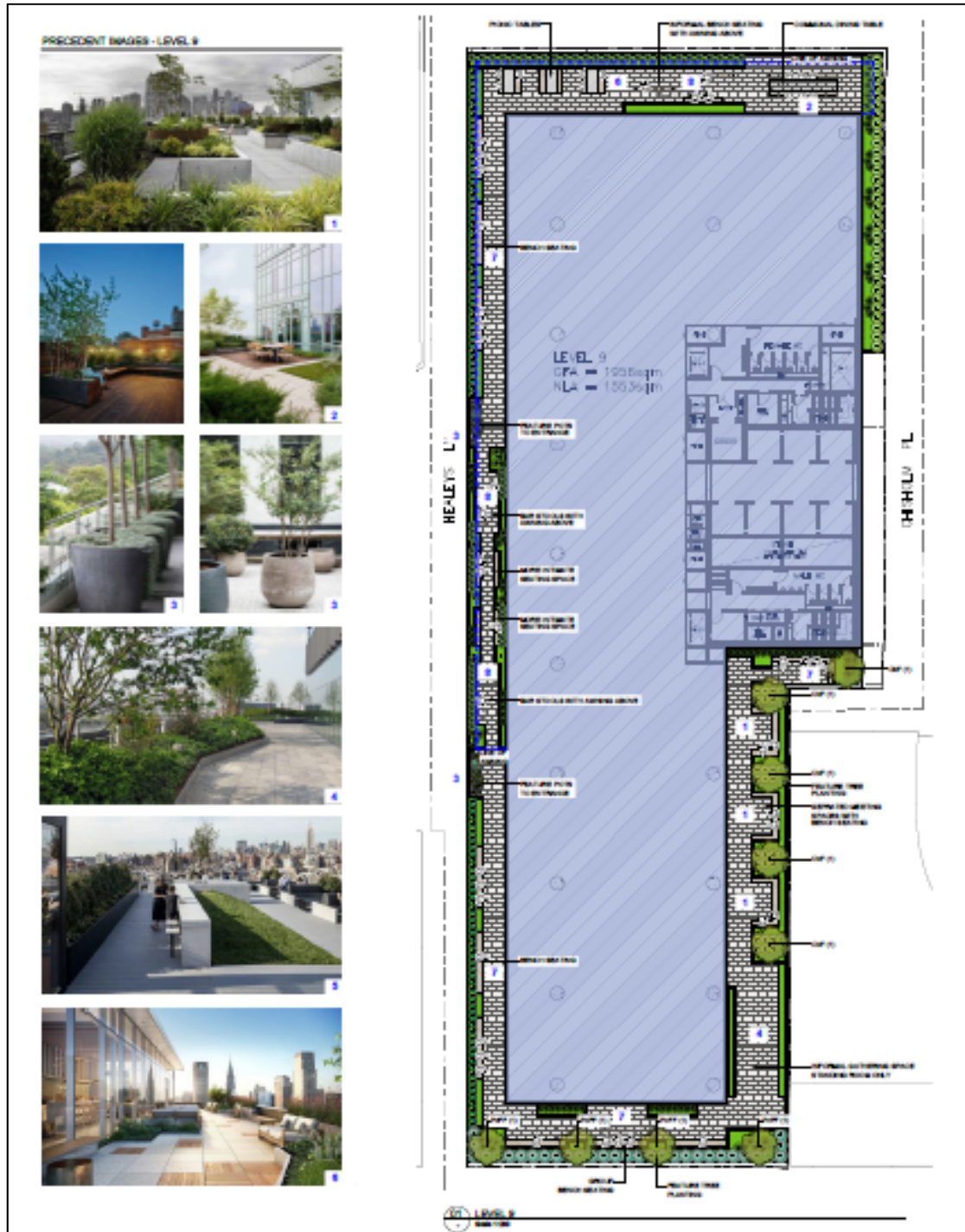


Figure 13 - Proposed materiality and colours

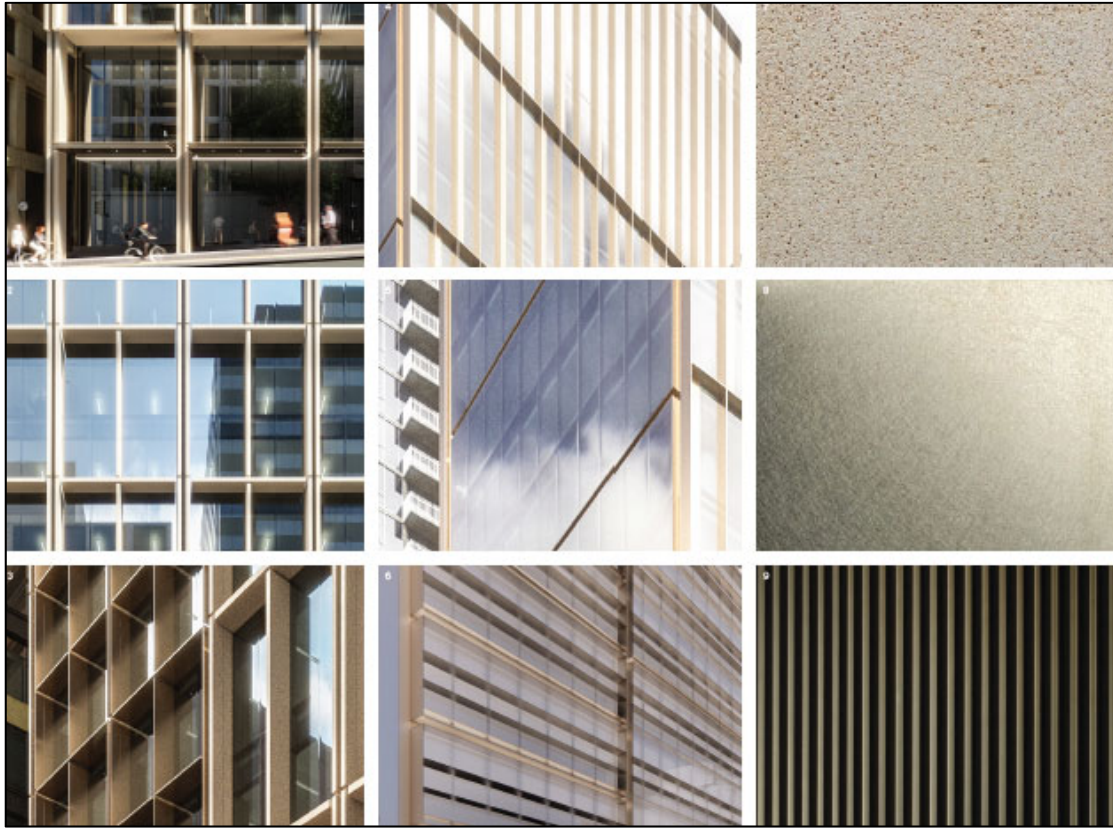


Figure 14 - Proposed development 3D renders (Lonsdale Street frontage in foreground)



Figure 15 – Artist impression of the proposed Lonsdale Street frontage



Figure 16 - Artist impression of the proposed Little Lonsdale Street frontage



## 4 PLANNING FRAMEWORK

The following provisions of the Melbourne Planning Scheme apply:

State Planning Policies	<ul style="list-style-type: none"> <li>• Clause 15 – Built Environment and Heritage <ul style="list-style-type: none"> <li>• Clause 15.01 – Built Environment <ul style="list-style-type: none"> <li>• Clause 15.01-1S – Urban Design</li> <li>• Clause 15.01-1R – Urban Design – Metropolitan Melbourne.</li> <li>• Clause 15.01-2S – Building Design</li> <li>• Clause 15.01-4R – Healthy Neighbourhoods – Metropolitan Melbourne</li> </ul> </li> <li>• Clause 15.02 – Sustainable Development <ul style="list-style-type: none"> <li>• Clause 15.02-1S Energy and Resource Efficiency</li> </ul> </li> </ul> </li> <li>• Clause 17 – Economic Development <ul style="list-style-type: none"> <li>• Clause 17.01 – Employment <ul style="list-style-type: none"> <li>• Clause 17.01-1S – Diversified Economy.</li> <li>• Clause 17.01-1R – Diversified Economy – Metropolitan Melbourne</li> </ul> </li> <li>• Clause 17.02 – Commercial <ul style="list-style-type: none"> <li>• Clause 17.02-1S – Business</li> </ul> </li> </ul> </li> <li>• Clause 18 – Transport <ul style="list-style-type: none"> <li>• Clause 18.01 – Integrated Transport <ul style="list-style-type: none"> <li>• Clause 18.01-1S – Land Use and Transport Planning</li> </ul> </li> <li>• Clause 18.02 – Movement Networks <ul style="list-style-type: none"> <li>• Clause 18.02-1S – Sustainable Personal Transport</li> <li>• Clause 18.02-2S – Public Transport</li> </ul> </li> </ul> </li> <li>• Clause 19 - Infrastructure <ul style="list-style-type: none"> <li>• Clause 19.03 – Development Infrastructure <ul style="list-style-type: none"> <li>• Clause 19.03-3S Integrated Water Management</li> </ul> </li> </ul> </li> </ul>
Municipal Strategic Statement	<ul style="list-style-type: none"> <li>• Clause 21.02 – Municipal Profile</li> <li>• Clause 21.03 – Vision</li> <li>• Clause 21.06 – Built Environment and Heritage</li> <li>• Clause 21.08 – Economic Development</li> <li>• Clause 21.09 – Transport</li> <li>• Clause 21.10 – Infrastructure</li> <li>• Clause 21.12 – Hoddle Grid</li> </ul>



Local Planning Policies	<ul style="list-style-type: none"> <li>• Clause 22.01 – Urban Design within the Capital City Zone</li> <li>• Clause 22.02 – Sunlight to Public Spaces</li> <li>• Clause 22.19 – Energy, Water and Waste Efficiency</li> <li>• Clause 22.20 – CBD Lanes</li> <li>• Clause 22.23 – Stormwater Management</li> </ul>
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Statutory Controls	
Clause	Permit Trigger
Clause 37.04 Capital City Zone Schedule 1	<p>Pursuant to Clause 37.04-4, a planning permit is required to:</p> <ul style="list-style-type: none"> <li>- Construct a building or construct or carry out works unless the schedule to this zone specifies otherwise.</li> <li>- Demolish or remove a building or works if specified in the schedule to this zone.</li> </ul> <p>Pursuant to Clause 3.0 of Clause 37.04, Schedule 1, a <b><u>permit is required</u></b> to construct a building or construct or carry out works.</p> <p>A permit must not be granted or amended (unless the amendment does not increase the extent of non-compliance) to construct a building or construct or carry out works with a floor area ratio in excess of 18:1 on land to which schedule 10 to the Design and Development Overlay unless:</p> <ul style="list-style-type: none"> <li>▪ A public benefit as calculated and specified in a manner agreed to by the responsible authority is provided; and</li> <li>▪ The permit includes a condition (or conditions) which requires the provision of a public benefit to be secured via an agreement made under section 173 of the Planning and Environment Act 1987.</li> </ul> <p>Floor Area Ratio (FAR) = The total gross floor area (53,042m<sup>2</sup>) / site area (2,828m<sup>2</sup>)</p> <p><b><u>FAR = 18.76:1</u></b></p> <p>The works exceed a FAR of 18:1; therefore, a public benefit is required.</p> <p>Pursuant to Clause 4.0 of Clause 37.04, Schedule 1, a <b><u>permit and prior approval for the redevelopment of the site are required</u></b> to demolish or remove a building or works.</p>
Clause 43.02 Design and Development Overlay Schedule 10	<p>Pursuant to Clause 43.02-2, a planning permit is required to construct a building or construct or carry out works. This does not apply if a schedule to this overlay specifically states that a permit is not required.</p> <p>The application does not meet any of the permit exemptions listed at Clause 2.2 of Clause 43.02, Schedule 10; therefore, a <b><u>planning permit is required</u></b>.</p>
Clause 45.09 Parking Overlay Schedule 1	<p>Clause 45.09, Schedule 1 provides maximum rates for land uses other than dwellings.</p> <p>Pursuant to Clause 3.0 of Clause 45.09, Schedule 1, the maximum car parking rate for a use other than dwellings is:</p>

	<p><b>Maximum spaces =</b>  <u>5 x net floor area of buildings on the site in sq m</u>  1000 sq m</p> <p>or</p> <p><u>12 x site area in sq m</u>  1000 sq m</p> <p>The proposal contains a net floor area of 40,164 square metres, imposing a maximum car parking rate of 201 car parking spaces (5x40,164/1000=200.83).</p> <p>The proposal contains 106 car parking spaces; therefore, a permit is not required.</p> <p>Pursuant to Clause 3.0 of Clause 45.09, Schedule 1, all buildings that provide on-site car parking must provide motorcycle parking for the use of occupants and visitors, at a minimum rate of one motorcycle parking space for every 100 car parking spaces, unless the responsible authority is satisfied that a lesser number is sufficient.</p> <p>The proposal contains two motorcycle parking spaces, satisfying the requirement; therefore, a permit is not required.</p>
<p>Clause 52.34 Bicycle Facilities</p>	<p><u>Bicycle Spaces</u></p> <p>Pursuant to Table 1 of Clause 52.34-5, an office requires 1 employee bicycle space to each 300sqm of net floor area if the net floor area exceeds 1000sqm, and 1 visitor bicycle space to each 1000sqm of net floor area if the net floor area exceeds 1000sqm.</p> <p>The proposal contains 39,261 square metres of office floor area which requires 131 employee bicycle spaces (39,261/300=310.87) and 39 visitor bicycle spaces (39,261/1000=39.261).</p> <p>Pursuant to Table 1 of Clause 52.34-5, a retail premises requires 1 employee bicycle space to each 300sqm of net floor area, and 1 visitor/shopper bicycle space to each 500sqm of net floor area.</p> <p>The proposal contains 903 square metres of retail floor area which requires 3 employee bicycle spaces (903/300=3.01) and 2 visitor bicycle spaces (903/500=1.806).</p> <p>The proposal requires a total of 134 employee bicycle spaces and 41 visitor bicycle spaces.</p> <p>The proposal contains 355 bicycle spaces at basement level 1.</p> <p>The discussion drawings dated 9/11/2020 show 42 of these spaces in a separate room, satisfying the requirement for visitors. As such, a permit is not required.</p> <p><u>Facilities</u></p> <p>Pursuant to Table 2 of Clause 52.34-5, if 5 or more employee bicycle spaces are required, 1 shower for the first 5 employee bicycle spaces, plus 1 to each 10 employee bicycle spaces thereafter.</p> <p>The proposal requires 134 employee bicycle spaces which requires 14 showers (1 shower for the first 5 spaces + 13 showers for the remaining 129 required spaces (129/10=12.9).</p> <p>The proposal contains 40 showers, exceeding the requirement; therefore, a permit is not required.</p> <p>Pursuant to Table 3 of Clause 52.34-5, 1 change room or direct access to a communal change room to each shower is required (if showers are required). The change room may be a combined shower and change</p>

	<p>room.</p> <p>Each shower has direct access to a communal change room with separate male and female facilities, complying with the requirement; therefore, a permit is not required.</p>
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General Provisions	<p>Clause 65 - Decision Guidelines</p> <p>Clause 66.04 - Referral of Permit Applications under Local Provision (Melbourne City Council is a Recommending Referral Authority for any permit application for development with a gross floor area exceeding 25,000 square metres within the Capital City Zone).</p>
Operational Provisions	<p>Clause 72.01 - Administration and enforcement of this scheme (The Minister for Planning is the responsible authority for this planning permit application as the total floor area of the development the sign is applied to exceeds 25,000 square metres).</p>

## 5 PUBLIC NOTIFICATION

Notice of the application is the responsibility of the Minister for Planning (Department of Environment, Land, Water, and Planning).

## 6 INTERNAL REFERRALS

The following internal referrals were undertaken:

### 6.1 Urban Design

The application was referred to the City of Melbourne's Urban Design. They provided the following comments (summarised) and recommendations:

#### Comments (summarised)

- The proposal seeks to construct the building to the boundary removing the pedestrian footpath at Healeys Lane. This raises fundamental concerns regarding pedestrian safety given the lane has high pedestrian traffic.
- A ground floor setback at Healeys Lane is preferred to enable adequate pedestrian movement space. The current configuration of fine grain tenancies, coupled with sufficient space for pedestrian movement and outdoor dining, contributes to the success and character of this laneway.
- A cantilevered setback approach with a double-height clearance is recommended to preserve the pedestrian footpath at Healeys Lane.
- The overall massing strategy, the proposed height and setbacks are supported in principle.
- The active tenancy at the corner of Little Lonsdale Street is supported.
- The notion of a 'book-ending' of active use to this interface at Chisholm Place, includes a micro-tenancy set-up at the southeast corner of the laneway interface. However, we question the retail viability of this micro-tenancy, and encourage further design exploration to ensure a viable active use at this corner.
- While we broadly support the location of the bicycle entry at the furthest entry point along the laneway, the perceived safety of this journey will depend on the success and viability of the aforementioned micro-tenancy.

- The ground level car parking entrance, loading dock and waste collection at Chisholm Place is not supported. Active uses are encouraged at this interface.
- A consolidated approach to bin collection and loading which takes place within the basement level is preferred.
- The proposed design language, the incorporation of material depth and thickness at the podium levels, and indicated details of the ground floor shopfronts/canopy/entries is supported.

### Recommendations

“Notwithstanding some of the positive aspects of the proposal, we are currently unable to support the application due to fundamental concerns regarding the *absence of a ground floor setback and extent of activation to Chisholm Place*.

We also require further information on materiality and façade details, especially as it relates to the lower levels of the building.

Based on our previous engagement with Bates Smart, we have a strong degree of confidence in the design team’s ability to integrate our advice to ensure a high quality urban design outcome. We welcome the opportunity to further discuss our comments with the applicant, if necessary.”

### Planner’s Response

The above advice has been noted. It is acknowledged the proposed ground level interface with the public realm at Healeys Lane and Chisholm Place creates issues, including the removal of the pedestrian footpath at Healeys Lane and traffic conflict. This is not a desirable outcome for the amenity and function of the immediate surrounds.

A complete assessment of the relevant built environment and urban design policies is found at Section 8 of this report.

### 6.2 Traffic

The application was referred to the City of Melbourne’s Traffic Engineers who identified a range of traffic issues. When the applicant submitted amended discussion plans, they also provided an updated Traffic Impact Assessment. The City of Melbourne’s Traffic Engineers maintain the following issues:

- The proposal indicated vehicles accessing the carpark and service vehicles will be required to use Chisholm Place. Despite the reference to tidal flow, this does not rule out the potential for vehicle conflict and the need for a vehicle to reverse, possibly into Lt Lonsdale St. This is undesirable and could be avoided by widening Chisholm Place or relocating the carpark entry / exit point to Little Lonsdale Street. The presence of on street Loading Zone facilities increases the potential chance for conflict. The City of Melbourne will not make parking changes to accommodate this development.
- Sight distance for vehicles exiting Chisholm Place will be poor and a splayed corner should be inserted. Reference is made to other comparable locations across the Melbourne CBD. This should not be used as justification for providing a situation that results in a reduced level of safety. City of Melbourne will not approve the use of mirrors in the public realm. This

situation could be improved by widening Chisholm Place, installing a splay on the south west corner or relocating the carpark entry / exit to Little Lonsdale Street and designed appropriately.

- Access via Chisholm Place introduces a number of safety issues and conflicts with pedestrian traffic. The proposed arrangement of providing car park access to and from Chisholm Place is not considered preferable when compared to the existing direct access via Little Lonsdale Street. Chisholm Place is only 4.6m in width. There is a potential for occasional conflict from vehicles using this lane requiring vehicles to reverse. There are also loading activities occurring in Chisholm Place. The City of Melbourne will not be removing these on street parking facilities to accommodate vehicle movements associated with the development. Swept path diagrams also indicate challenges for large commercial vehicles associated with the development entering / exiting Little Lonsdale Street.
- As indicated previously, the swept path diagrams show that changes to the kerbs at the intersection of Little Lonsdale Street at Chisholm Place will be required, and some parking will be lost along Little Lonsdale Street. The City of Melbourne will not make changes to on street parking in Little Lonsdale Street or Chisholm Place to facilitate vehicles entering / exiting Chisholm Place. The need for these changes has not been mentioned in the GTA report. The design should be altered to accommodate servicing vehicles without the need for parking changes or the undesirable practice of vehicles mounting kerbs.

### **Recommendation**

“It is considered that given the number and severity of the issues raised above, that no permit should be issued for the proposal at this time.”

### **Planner’s Response**

The City of Melbourne’s Traffic Engineers are not satisfied with the proposal’s traffic response. They have identified:

- Entering / exiting the Site via Chisholm Place creates safety issues relating to vehicle and pedestrian conflicts.
- The proposed built form at the corner of Chisholm Place and Little Lonsdale Street creates sight line issues for vehicles existing via Chisholm Place.
- The discussion plans dated 9 November 2020 show two motorcycle parking spaces at Basement Levels 1 and 2 in accordance with the requirement.
- Chisolm Place is too narrow to accommodate the traffic generated by the proposal. There are too many conflicts between pedestrian, car, bicycle (end of trip), waste, and loading traffic.

The traffic relates elements of the proposal are addressed at Section 8 of this report.

### **6.3 Urban Services (Waste)**

The application was referred to the City of Melbourne’s Waste Services Department who provided the following comments:

"I have reviewed the amended WMP and find that it is **unacceptable**, as the below has not been rectified:

- Please show the expected path of travel for the ground floor tenants to the waste storage area, noting that internal access within the development is required.

Please refer to Andrew Cron's comments (traffic) referring to the swept path diagrams.

- Outstanding issues relating to the hook-lift vehicle's access to the property as identified by CoM's Transport Engineering team (see below) are required to be resolved. In addition to the following issues, swept path diagrams for all waste vehicles expected to service the development are required to be attached to the WMP."

### **Planner's Response**

It is noted that there are issues relating to the submitted waste management plan that should be resolved by the applicant. It is recommended this be achieved through conditions on any permit issued.

### **6.4 Civil Design**

The application was referred to the City of Melbourne's Civil Design Engineers who requested that the standard civil design conditions be included on any permit issued as well as the following comments:

"All projections over the street alignment must conform to Building Regulations 2018, Part 6, Sections 98 to 110 as appropriate. Reference can be made to the City of Melbourne's Road Encroachment Operational Guidelines with respect to projections impacting on street trees and clearances from face/back of kerb."

### **Planner's Response**

The provided conditions will adequately address the relevant civil design requirements.

- It is recommended the provided conditions be included on any permit issued.
- It is recommended that the plans be amended to show the proposed building projections complying with the City of Melbourne's Road Encroachment Operational Guidelines.

### **6.5 Environmentally Sustainable Design (ESD)**

The application was referred to the City of Melbourne's ESD and Green Infrastructure team who commented:

"The development generally has very good ESD targets to satisfy Clause 22.19 of the Melbourne Planning Scheme.

The ESD report's proposed Green Star pathway targets 61 points for a 5 Star rating. Overall the Green Star approach is well considered and the ESD aspirations of the project are very good.

A solar PV system with a capacity of 99kW is proposed. This is an excellent inclusion and will contribute greatly to the energy performance of the development. The proposed solar PV system should be shown on the architectural plans.

The overall landscape concept is well realised with greening distributed across the built form. The proposal is attractive and will provide multiple

benefits for the public and occupants. The quality of documentation provided for the landscape concept is excellent.

It is recommended that a complete Landscape Plan, Landscape Maintenance Plan and Irrigation Performance Specification to be submitted to the satisfaction of the Responsible Authority prior to commencement of development.

The landscape plans should include detailed planter sections including soil volumes and schedules of species with specific consideration given to soil volume requirements and growing medium proposed.

The Landscape Maintenance Plan should provide further detail with respect to ongoing maintenance of on-structure planters, including specific provision for maintenance beyond the fifty two week period following Practical Completion.

It is also suggested that the application could incorporate benchmarking of the buildings' green infrastructure quality by voluntary use of the City of Melbourne's Green Factor tool.

The project has over 10% of parking spaces provided with electric vehicle charging points, which exceeds the Green Star requirement.

The WSUD report details how the required treatment levels to satisfy Clause 22.23 and Green Star credit requirements are achieved by a rainwater capture and reuse system with a 60,000L tank. A STORM report is included demonstrating how this is achieved. The tank must be shown on architectural plans."

#### **Further Information Required**

- Proposed solar PV system to be shown on architectural plans as per ESD report.
- 60kL rainwater tank shown on architectural plans as per ESD report.

#### **Planner's Response**

It is noted that the ESD elements of the proposal are an acceptable response to Clause 22.19. It is agreed that a landscape plan should be prepared to ensure the longevity of the proposed landscaping.

The discussion plans dated 9 November 2020 show the 60,000 litre rainwater tank located at Basement Level 3.

### **6.6 Urban Forest and Ecology**

The application was referred to the City of Melbourne's Urban Forest and Ecology team who provided the following comments and standard permit conditions:

"The only apparent works into the public realm seems to be the building canopy.

Given the form of the two mature public trees growing within footpath plots adjacent to the Lonsdale Street site boundary, this canopy is unlikely to result in adverse effects. But, proposed conditions are provided to ensure this.

The canopies of the two of public trees may encroach over the boundary and as such may require pruning. It is not considered that this would be detrimental to the trees but most negative impacts to public trees occur at demolition or construction stages when space constraints lead to increased pressure for removal or significant pruning.

Whilst the planning scheme does not require applicants to consider the impacts of demolition or construction, these are significant factors for council. The Tree Retention and Removal Policy will support minimal pruning and the retention of trees.

In view of the above, the following conditions are appropriate for any permit DELWP may issue.”

### **Planner’s Response**

The provided permit conditions will adequately ensure that the proposed development will not have an adverse impact on the street trees adjacent to the proposed development.

- It is recommended the provided conditions be included on any permit issued.

## **7 EXTERNAL REFERRALS**

The application was not required to be externally referred by the City of Melbourne who is a referral authority for this application.

## **8 ASSESSMENT**

The application seeks planning approval for demolition and buildings and works to construct a multi-storey building. The key issues in the consideration of this application are:

- Built Environment (Urban Design).
- The design objectives, requirements, and built form outcomes of DDO10.
- Traffic.

The plans assessed are the discussion plans prepared by Bates Smart Architecture, dated 9 November 2020.

### **8.1 Built Environment**

The proposal generally responds to the purpose and decision guidelines of the Capital City Zone and relevant built environment (urban design) policies for the following reasons:

- The proposed works align with the purpose of the Capital City Zone as they seek to provide for uses that complement the function of the locality.
- The proposal inserts a high level of active frontages through the inclusion of active uses (retail) at the ground level interface that contain extensive glazing.
- The proposal acknowledges the built environment character at this section of Lonsdale Street and proposes a design response that reasonably limits adverse amenity impacts to the public realm.
- The design of the proposal is well integrated with the street and adjoining properties.
- The proposal does not obscure views to the civic landmarks listed at Clause 22.01-1.
- The proposal removes an above ground car park.
- The Wind Impact Assessment (WIA) provided by the application confirms the proposed built form passes the walking criterion with many test locations satisfying the sitting and standing comfort criteria. The WIA has not recommended any design modifications or mitigation measures.



- The proposal provides for adaptable floor plans that can be altered at a later date to accommodate a range of uses.
- The proposal reasonably limits amenity impacts to the surrounds. The proposed setbacks ensure the development will not unreasonably impact the amenity of the residential uses to the west at 560 Lonsdale Street.
- The design of the proposal is well integrated with the street and adjoining properties as it responds to the emerging higher built form to the north, south, and west.
- The proposed site access points are well defined.
- The proposal does not cast any shadow on the key public spaces listed at Clause 22.02, nor does it introduce any additional unreasonable reduction of solar access to Lonsdale Street. Where the Lonsdale Street road reserve is impacted, it is limited to selected hours throughout the day.
- The floor plans of the upper level contain extensive glazing with a layout that ensures high levels of internal amenity.
- The proposal complies with the floor area uplift and delivery requirements of public benefits found at Clause 22.03 which is required by Clause 37.04 (CCZ). The proposal has a Floor Area Ratio of 18.76:1. However the Capital City Zone does provide that where a building exceeds a FAR of 18:1, this can be accepted if the proposal includes a public benefit as calculated and specified in a manner agreed to by the responsible authority.

In *How to Calculate Floor Area Uplifts and Public Benefits* the City of Melbourne indicates that Floor Area Uplift can be achieved by providing a public benefit which relevantly can include '*Strategically justified uses including Office on site or within proposed building*'. The Site is located within the Flagstaff precinct. The gross realisation value for Commercial in this precinct is \$5,500, whilst the value for residential is \$7,000, a 27.27% increase.

Steps as per DELWP instructions for FAU and Public Benefit:

1. Base gross floor area = 2828 x 18 = 50,904 square metres
2. Proposed development gross floor area = 53,042 square metres
3. Floor area Uplift = 2,138 square metres
4. Base data for valuing FAU (GRV) = \$5,500/square metre
5. Land value of each square metre of FAU (10% of GRV) = \$550/m<sup>2</sup>
6. Total value of FAU = \$1,175,900
7. Value of public benefit to be provided = \$1,175,900
8. Agreed public benefit = Up to \$59M (offices instead of residential)\*

(\*Difference in value of FAU if it were residential, rather than commercial = \$7,000 - \$5,500 = \$1,500 x 39,393m<sup>2</sup> = \$59M).

Despite the above elements that are positive, the application has not adequately addressed issues relating to the function and safety of the public realm. The application in its current form is not supported for the following reasons:

- The removal of the pedestrian footpath at Healeys Lane is not supported as it will not improve the function of the public realm; it will create unsafe conditions. In the event that pedestrians are seeking to access the proposed retail spaces lining Healeys Lane, they will be forced to walk down the road reserve that contains car traffic. While the discussion plans marginally

decrease the buildings footprint at this section of the development, a clear and direct footpath is not provided.

- The loss of the publicly accessible space at Lonsdale Street is not supported as this space contributes to Healeys Lane being a desirable pedestrian thoroughfare. The loss of this space reduces the perceived width of Healeys Lane and potentially implies safety issues. It should be retained with a higher quality design response inserted into the proposal.
- The traffic conflict created by the proposal between pedestrians and motor vehicle traffic at Chisholm Place is not supported. The lack of sight lines (lack of a splayed building corner) and width of the lane cannot safely provide for multiple users of the lane which generates high movement from waste collection, end of trip facilities, loading, and retail activities in Chisholm Place.

These issues could be resolved through conditions on a permit requiring ground level setbacks at Healeys Lane and Chisholm Place as well as introducing a splayed corner to the built form where Little Lonsdale Street intersects with Chisholm Place.

The setback to Chisholm Place should provide sufficient carriageway width for passing and manoeuvring vehicles from Lt Lonsdale Street south to the building access points, with a corner splay as needed to facilitate sightlines to Lt Lonsdale Street.

In accordance with the City of Melbourne’s Road Encroachment Operational Guidelines, it is recommended that the building at Healeys Lane be setback two metres (clear of any columns) from the west boundary to allow a comfortable pedestrian thoroughfare to be introduced, resolving concerns relating to safety and movement.

Given the lower and upper ground levels combine to measure approximately 6.3 metres in height at Lonsdale Street, introducing a setback / colonnade at Healeys Lane up to Level 1 would provide for adequate clearance and comfort for pedestrians.

Due to the land falling from the north to the south, the colonnade would be required to maintain a consistent height. Due to this, it is recommended the setback / colonnade be inserted up to Level 2 at Little Lonsdale Street.

It is also recommended that a permit condition requiring a Section 173 legal agreement for ownership, maintenance, access, and liability of the pedestrian footpath be applied to any permit issued.

The performance standards of Clause 22.01 have been assessed below as a method of determining the appropriateness of the proposal:

<b>Building Envelope (Clause 22.01-1)</b>
<p>The building envelope is assessed under the requirement of DDO10 which is found at Section 9.2 of this report. The proposed development responds appropriately to the street wall, tower, and setback provisions of DDO10.</p> <p>The overall form does not unreasonably detract from the public realm or any of the civic landmarks identified in Clause 22.01-1.</p>
<b>Building Design (Clause 22.01-2)</b>
<p>The proposed development is an acceptable response to the design objectives and</p>

requirements for the following reasons:

- The lower portion of the building aligns with the existing built form street pattern, respecting the existing street wall continuity expressed by the buildings to the east and west.

The building is proposed to be constructed to the street boundaries at ground level with an interface that is pedestrian focused and safe. This is achieved through extensive windows and openings, as well as by avoiding entrapment spaces.

- The podium and tower have distinguishable yet cohesive design elements and architectural detail that promote visual interest. This is also supported by the tower setback.
- The buildings frontages contain vertical detailing to ensure the mass of the podium is broken down, mitigating visual bulk.

#### **Pedestrian Permeability and Connectivity (Clause 22.01-3)**

Clause 22.01-3 recommends sites with an average length exceeding 100 metres provide through block connections for pedestrians. The Site is approximately 90 metres long and provides the following access which is acceptable:

- Pedestrian access is possible via Lonsdale Street, Little Lonsdale Street and Healeys Lane entrances.
- The main lobby can be accessed via Lonsdale Street, Healeys Lane, and Chisholm Place.
- Both side entrances at Healeys Lane and Chisholm Place are located in the centre of the site to reduce pedestrian travel.
- The proposal contains side entrances to the main lobby which acts as a pedestrian thoroughfare.
- End of trip access is accommodated at Chisholm Place.
- All entrances are located adjacent to active uses (retail/café).

#### **Facades (Clause 22.01-4)**

The proposed development is an acceptable response to the design objectives and requirements for the following reasons:

- The proposal avoids blank walls adopts well-articulated fenestration, materials and colours that result in a façade that is respectful to the streetscape.
- The materials and detailing are of high quality, enhancing the streetscape through visual interest.
- The design of the building addresses all frontages that adjoin lanes and streets, providing high levels of pedestrian interaction.
- The proposal contains limited visible services at the ground level which would detract from the Site's ability to provide high quality pedestrian interaction. The fire services fronting Lonsdale Street are appropriately located as these services are required to be readily accessed.

#### **City and Roof Profiles (Clause 22.01-5)**

The proposed development is an acceptable response to the design objectives and requirements for the following reasons:

- The proposed roof and building services are well integrated into the design of the tower and appropriately screened.
- The proposed roof profile is well integrated into the design of the tower.

**Projections (Clause 22.01-6)**

The proposed development is an acceptable response to the design objectives and requirements for the following reasons:

- The proposed projections do not appear to have a significant impact on the existing street trees or the function of the public realm. The dimensions and clearance of the projections proposed are not clearly shown on the plans. It is recommended this be resolved via a permit condition that relates to the City of Melbourne's Road Encroachment Operational Guidelines.
- The architectural features that project beyond the title boundary are minor and do present as integral elements of the building design.

**Wind and Weather Protection (Clause 22.01-7)**

The wind impact assessment provided by the application states:

“For the Proposed Configuration, wind conditions for all Test Locations in the streetscapes surrounding the development have been shown to pass the walking criterion, with many Test Locations satisfying the sitting and standing comfort criteria. No wind mitigation strategies or modifications to the 550 Lonsdale Street development design have been recommended. The wind conditions for the Proposed Configuration in the streetscapes that surround the 550 Lonsdale Street development pass the safety criterion. The Existing Configuration wind conditions have been included for comparison. The wind conditions on the Podium and Rooftop Levels for the Proposed Configuration have been shown to pass the walking criterion.”

The proposed development is an acceptable response to the design objectives and requirements for the following reasons:

- The site is not included in DDO4 (weather protection) and is not required to provide weather protection above the street. Despite this, the application proposes adequate canopies for weather protection at the Lonsdale Street and Little Lonsdale Street frontages.
- The advice relating to wind impacts states that its calculation does not rely on existing street furniture or landscaping within the public real, ensuring the comfort of pedestrians will be preserved should the development be supported.

**Public Spaces (Clause 22.01-8)**

The proposal does not provide public spaces such as a plaza, forecourt, or arcades.

As identified by the City of Melbourne's Urban Design team, the loss of the publicly accessible space where Lonsdale Street and Little Lonsdale Street intersect with Healeys Lane is problematic. The existing conditions at this section of the Site contain a cantilevered section of the building over a pedestrian footpath at Little Lonsdale Street (Figure 17) and a building setback and publicly accessible area at Lonsdale Street (Figure 18).

The proposal seeks to remove these features and replacement them with a building constructed to the Site boundary, resulting in the loss of the footpath and sitting space.

The removal of the pedestrian footpath is not supported as it will not improve the function of the public realm; it will create an less desirable and less accessible scenario for pedestrians seeking to access the proposed retail spaces as they will be forced to walk down Healeys Lane which is a road that contains car traffic except at its southern end.

The loss of the publicly accessible space at Lonsdale Street is not supported as this space contributes to Healeys Lane being a desirable pedestrian thoroughfare. Clause 22.01-8 and Clause 22.20 seek to promote the inclusion of public spaces while also ensuring lanes maintain a human scale. The loss of this space does not respond to these policies and may introduce less desirable public realm conditions.

It is recommended that a full length two metre clear width pedestrian footpath / colonnade be inserted along Healeys Lane to resolve these two issues. A full length footpath would introduce clear sightlines to promote higher levels of pedestrian amenity and comfort while increasing the lane's ability to provide a high quality pedestrian thoroughfare. This could be achieved through a permit condition.

#### **Access and Safety (Clause 22.01-9)**

While the application contains high levels of activation, there are fundamental issues relating to traffic conflicts and access which are not supported as they unreasonably impact the function of the public realm.

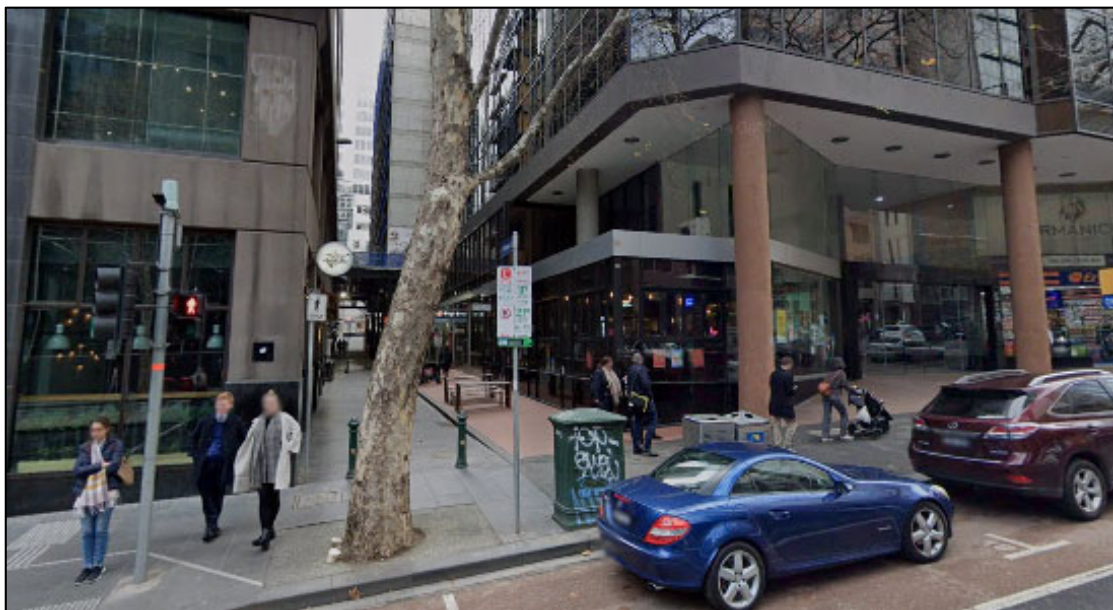
This includes:

- The removal of the pedestrian footpath at Healeys Lane which provides pedestrian refuge (Figure 17). The removal of the pedestrian footpath at Healeys Lane is not supported as it will not improve the function of the public realm; it will create less safe conditions. In the event that pedestrians are seeking to access the proposed retail spaces lining Healeys Lane, they will be forced to walk down the road reserve that contains car traffic. While the discussion plans marginally decrease the building's footprint at this section of the development, a clear and direct footpath is not provided. It is recommended a two metre wide colonnade be inserted to allow for a pedestrian footpath. As discussed above, this could be achieved through permit condition.
- The removal of the publicly accessible open space at the intersection of Healeys Lane and Lonsdale Street (Figure 18). The loss of the publicly accessible space at Lonsdale Street is not supported as this space contributes to Healeys Lane being a desirable pedestrian thoroughfare. It should be retained with a higher quality design response integrated into the proposal. Retaining this space will combine with the above recommended setback for a pedestrian footpath to improve the public realm and enhance pedestrian movement. Further, a full length footpath / colonnade at this section of the Site will enable clear views up and down the lane which will promote use of the lane.
- The traffic conflict created by the proposal between pedestrians and motor vehicle traffic at Chisholm Place is not supported. The lack of sight lines (lack of a splayed building corner) and width of the lane cannot safely provide for multiple users of the lane which generates high movement from waste collection, end of trip facilities, loading, and retail activities in Chisholm Place. Chisholm Place should be widened at its northern end and a splay to Lt Lonsdale Street created as needed, by permit condition.

Figure 17 – Healeys Lane and Little Lonsdale Street



Figure 18 – Healeys Lane and Lonsdale Street



## 8.2 DDO10

The proposed development is an acceptable response to DDO10 and its design objectives for the following reasons:

- The proposal satisfies the design objectives of Clause 1.0 to DDO10 including:
  - Respecting the surrounding built form in terms of scale and design.
  - Avoiding unreasonable amenity impacts to the public realm by limiting overshadowing and avoiding the introduction of uncomfortable wind impacts (this is assessed at Section 8.2.1 of this report).
  - The ground level interfaces potentially provide for public safety through active frontages.

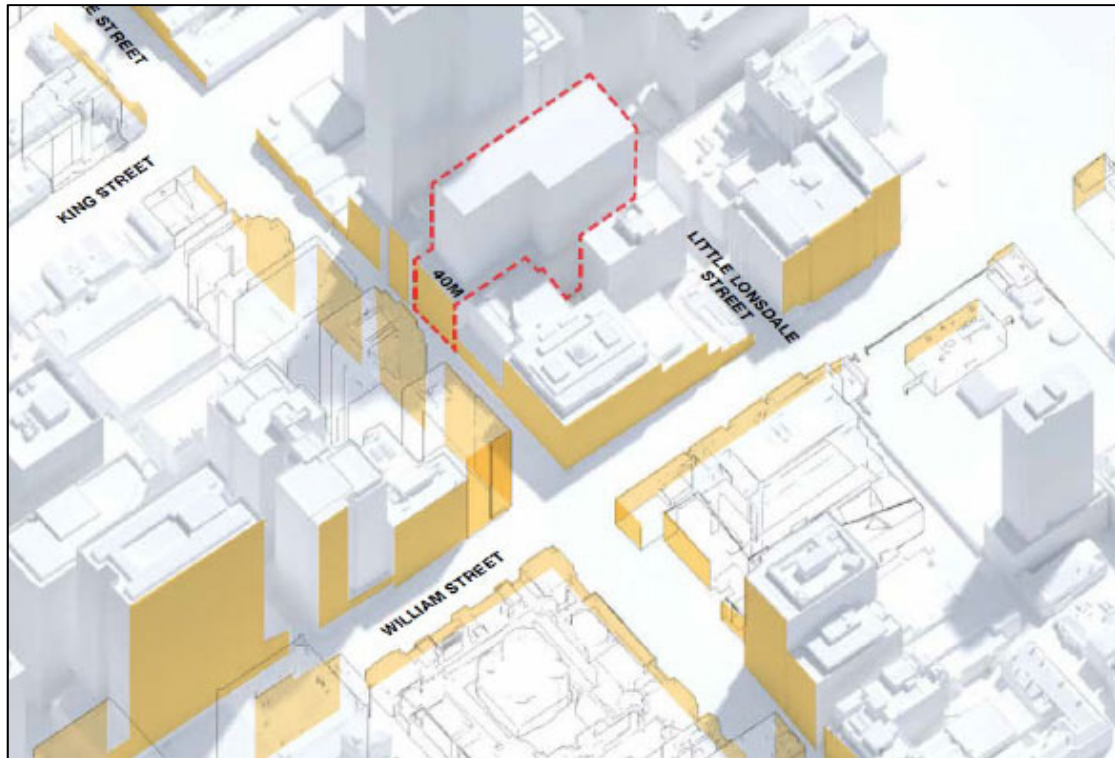
- While the proposal achieves the built form outcomes for each relevant Design Element in Table 3 of DDO10, the plans show architectural features and canopies that encroach into the mandatory setback requirement. This issue does not infer the proposal cannot be supported however it must be addressed as these projections are prohibited. These matters are assessed at Section 8.2.1 of this report.
- The proposal appropriately justifies the variation of the Preferred Requirements specified in each Design Element in Table 3 of DDO10. These matters are assessed at Section 8.2.1 of this report.
- The proposal does not introduce unreasonable wind impacts to the public realm.
- The proposal does not cast additional shadow to any of the places listed in Table 1 or Table 2 of DDO10.

### 8.2.1 Table 3 to Clause 43.02, Schedule 10 (DDO10)

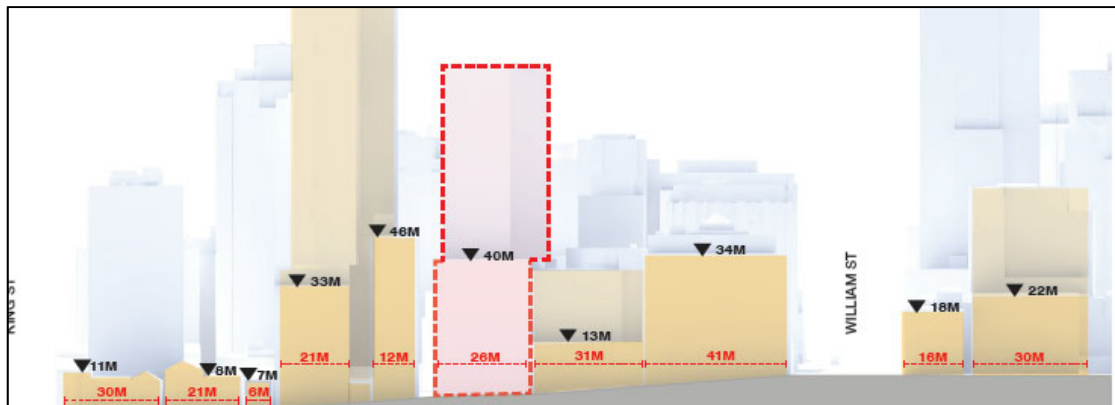
Design Element	Preferred Requirement	Modified Requirement	Built form outcomes
Street wall height	Up to 20 metres	The street wall height must be no greater than 40 metres; or	Street wall height is scaled to ensure: <ul style="list-style-type: none"> <li>▪ a human scale.</li> <li>▪ an appropriate level of street enclosure having regard to the width of the street with lower street wall heights to narrower streets.</li> <li>▪ consistency with the prevalent parapet height of adjoining buildings.</li> <li>▪ height that respects the scale of adjoining heritage places.</li> <li>▪ adequate opportunity for daylight, sunlight and sky views in the street.</li> <li>▪ definition of main street corners and/or public space where there are no significant impacts on the amenity of public spaces.</li> <li>▪ maintenance of the prevailing street wall height and vertical rhythm on the street.</li> </ul>
<b>Assessment</b>			
<p>The application seeks to comply with the modified requirement of 40 metres.</p> <p>The variation is acceptable for the following reasons:</p> <ul style="list-style-type: none"> <li>▪ The proposal inserts a street wall height and vertical rhythm that responds to the surrounding built form context. The height of the street wall is consistent with the street wall heights of 233 Lonsdale Street, 552 Lonsdale Street, 560 Lonsdale Street, 525 Lonsdale Street, 555 Lonsdale Street, and 565 Lonsdale Street (Figure 19 and 20).</li> <li>▪ The shadow cast by the proposed development is not unreasonable, affecting the Lonsdale Street road reserve throughout the day but not affecting any areas of public open space or parks.</li> <li>▪ The proposed street wall provides a human scale through vertical and horizontal architectural expression with a pedestrian focused ground level interface.</li> <li>▪ The 37 metre street wall height at Little Lonsdale Street provides an appropriate transition from the 52 metre height street wall of building adjoining the site to the east</li> </ul>			

and the 12 metre street wall of the building adjoining the site to the west (Figure 21).

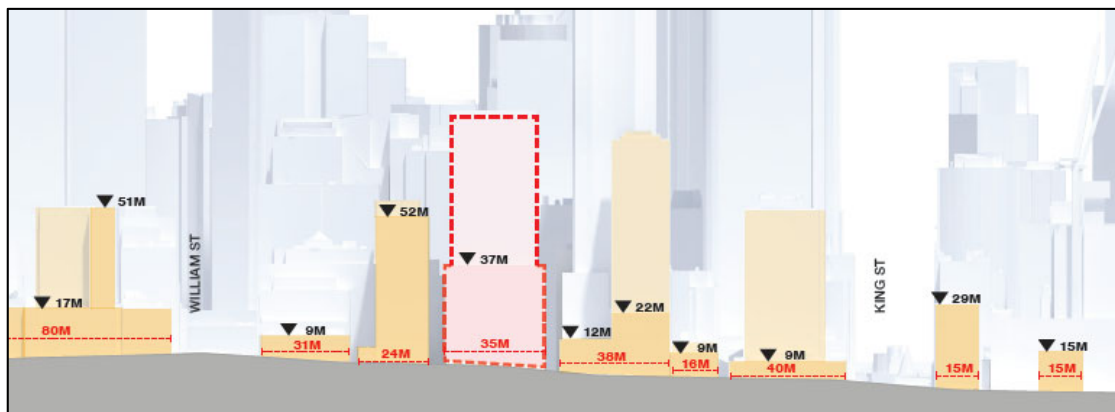
**Figure 19 – Proposed development including surrounding street wall height analysis**



**Figure 20 - Built form pattern at the north side of Lonsdale Street**



**Figure 21 - Built form at the south side of Little Lonsdale Street**

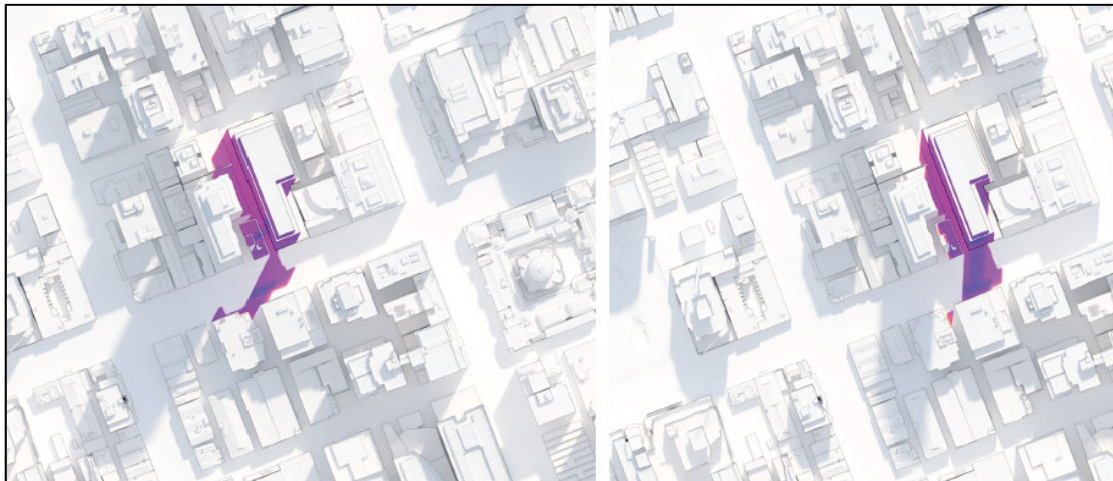




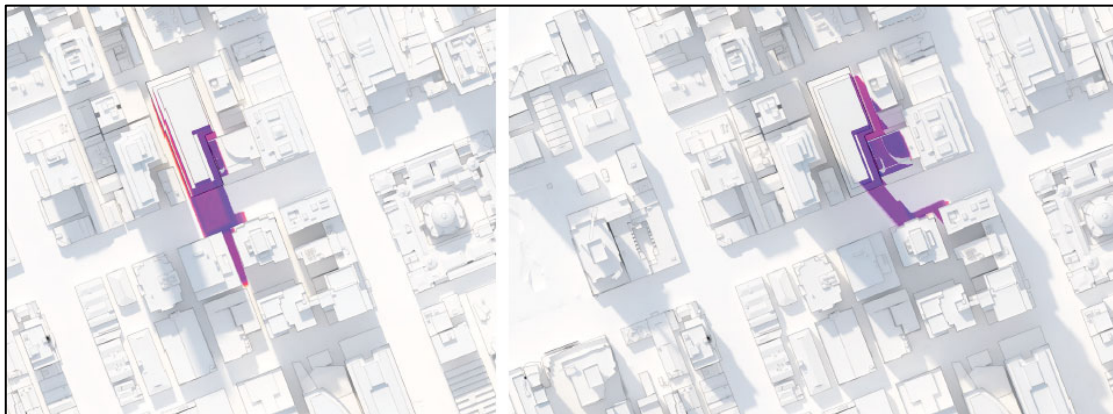
Design Element	Preferred Requirement	Modified Requirement	Built form outcomes
Building setback(s) above the street wall	Above the street wall, towers and additions should be setback 10 metres from the title boundary.	Above the street wall, towers must be setback a minimum of 5 metres from the title boundary.	Towers and additions are setback to ensure: <ul style="list-style-type: none"> <li>▪ large buildings do not visually dominate the street or public space.</li> <li>▪ the prevalent street wall scale is maintained.</li> <li>▪ overshadowing and wind impacts are mitigated.</li> <li>▪ The tower or addition includes a distinctly different form or architectural expression.</li> </ul>
<b>Assessment</b>			
<p>The application seeks to comply with the modified requirement by setting back the tower 5 metres from the title boundary at both Lonsdale Street and Little Lonsdale Street.</p> <p>The supplied application and discussion plans show the tower setback less than 5 metres due to the presence of architectural details at the east side of the north elevation (Little Lonsdale Street).</p> <p>The definition of a setback allows for an architectural feature of 300mm encroaching into the 5 metre setback requirement. The plans show architectural features resulting in the setback being 4.695 metres from the title boundary which is 5mm outside the mandatory control which effectively allows a setback of 4.7 metres when there is an architectural feature. This is a minor matter and is recommended to be resolved through a permit condition to increase the setback to meet the mandatory provision.</p> <p>The podium terrace canopy, which has been introduced to mitigate unreasonable wind conditions, is located above the street wall however is less than 40 metres in height. As such, it meets the definition of an 'addition' in DDO10.</p> <p>DDO10 provides the following relevant definitions:</p> <p style="padding-left: 40px;"><b>“Tower</b> means a building that exceeds the street wall, excluding an addition.</p> <p style="padding-left: 40px;"><b>Addition</b> means a building that exceeds the street wall and which is less than 40 metres in height.”</p> <p>Pursuant to the modified requirement in DDO10 for building setbacks above the street wall, an addition has no required setback; only a tower is required to be set back. The canopy above the street wall complies with DDO10 and is not prohibited.</p> <p>Aside from the minor architectural feature setback above, the proposed modified requirement is acceptable for the following reasons:</p> <ul style="list-style-type: none"> <li>▪ The setbacks proposed allow for the street wall scale to prevail within both the Lonsdale Street and Little Lonsdale Street streetscapes.</li> <li>▪ The design, materiality and articulation of the upper level read as distinct yet cohesive when viewed against the podium.</li> <li>▪ Unreasonable overshadowing is considered to be mitigated by avoiding impacts to important public areas.</li> <li>▪ The building is large however does not visually dominate the streetscape. The setbacks are consistent with other larger buildings in the immediate surrounds including 560 Lonsdale Street.</li> </ul>			

Design Element	Preferred Requirement	Modified Requirement	Built form outcomes
Building setbacks from side boundaries and rear boundaries (or from the centre line of an adjoining laneway) and tower separation within a site	Above the street wall or 40 metres (where there is no street wall), towers and additions should be setback a minimum of 5 metres or 6% of the total building height whichever is greater.	Towers exceeding 80 metres in total height: Above the street wall or 40 metres (where there is no street wall), towers and additions must be setback a minimum of 5 metres and must meet the design element requirements for tower floorplate.	Towers and additions are designed and spaced to ensure: <ul style="list-style-type: none"> <li>▪ sun penetration and mitigation of wind impacts at street level.</li> <li>▪ provision of reasonable sunlight, daylight, privacy and outlook from habitable rooms, for both existing and potential developments on adjoining sites.</li> <li>▪ floorplate layout or architectural treatment limits direct overlooking between habitable rooms.</li> <li>▪ buildings do not appear as a continuous wall at street level or from nearby vantage points and maintain open sky views between them.</li> <li>▪ buildings do not visually dominate heritage places and streetscapes, nor significant view lines.</li> </ul>
<b>Assessment</b>			
<p>The application seeks to comply with the modified requirement, setting back the tower 5 metres from the title boundary.</p> <p>The proposed modified requirement is acceptable for the following reasons:</p> <ul style="list-style-type: none"> <li>▪ The proposed shadow impacts resulting from the tower are limited to the Lonsdale Street road reserve, impacting the public realm less than the larger buildings on the same section of Lonsdale Street (Figures 22 and 23).</li> <li>▪ The proposal does not contain dwellings. Regardless, the proposed setbacks combined with the lanes either side of the Site allow for reasonable (approximately 9 metres) separation between the proposed tower and the adjoining properties, avoiding unreasonably privacy impacts and impacts to solar access. This specifically relates to 560 Lonsdale Street which contains dwellings. The remaining adjoining properties do not contain dwellings.</li> <li>▪ The setbacks and façade treatment ensure the building does not present as a continuous wall at the street level.</li> <li>▪ The proposal does not have any significant impacts to heritage buildings in the immediate surrounds.</li> </ul>			

**Figure22 – Additional shadow cast by the proposal at 11am and 12pm on September 22**



**Figure 23 – Additional shadow cast by the proposal at 1pm and 2pm on September 22**



Design Element	Preferred Requirement	Modified Requirement	Built form outcomes
Tower floorplate	The tower floorplate is determined by the preferred requirement for building setbacks from side and rear boundaries and tower separation within a site, and the modified requirement for building setback(s) above the street wall	<p>The tower floorplates above the street wall for a tower above 80 metres in height may be adjusted in terms of location and/or shape but must not:</p> <ul style="list-style-type: none"> <li>▪ Result in an increase in the floorplate area;</li> <li>▪ be situated less than 5 metres from a side or rear boundary (or from the centre line of an adjoining laneway);</li> <li>▪ be less than 5 metres to a street boundary;</li> <li>▪ be less than 10 metres to an adjoining tower on</li> </ul>	<p>The adjusted floorplate is designed and spaced to:</p> <ul style="list-style-type: none"> <li>▪ reduce impact on existing and potential neighbours in terms of privacy, outlook, daylight and sunlight access.</li> <li>▪ minimise visual bulk.</li> <li>▪ reduce impact on public spaces, including overshadowing and wind effects and reduced visual dominance.</li> <li>▪ buildings do not visually dominate heritage places and streetscapes, nor significant view lines.</li> <li>▪ buildings do not appear as a continuous wall at street level or from nearby vantage points</li> </ul>

		the site.	and maintain open sky views between them.
<b>Assessment</b>			
<p>The application seeks to comply with the modified requirement, which is necessary as the proposal is above 80 metres in height. Side and rear setbacks are reduced below 10% of the total height in places, but increased in others (positively to Healeys Lane) such that the overall floor plate retains the same area (Figure 26). The resulting setbacks should never be below 5.0m (excluding architectural features).</p> <p>Again, the supplied plans show the tower setback less than 5 metres due to the presence of architectural details at the east side of the north elevation (Little Lonsdale Street). The definition of a setback allows for an architectural feature of 300mm encroaching into the 5 metre requirement. The plans show architectural features resulting in the setback being 4.695 metres from the title boundary which is 5mm outside the mandatory control. This is a minor matter and can be resolved through a permit condition to limit the architectural feature.</p> <p>The proposed modified requirement is acceptable for the following reasons:</p> <ul style="list-style-type: none"> <li>▪ The proposal contains setbacks that align with the requirements, setting back the tower 5 metres from each boundary or from the centre line of the adjoining lane (provided the plans are amended to show compliance with the mandatory setback as noted above).</li> <li>▪ The setbacks have guided the floorplate area and the proposal does not seek to increase the floorplate area (Figures 24 and 25).</li> <li>▪ The proposed tower is more than 10 metres from an adjoining tower.</li> <li>▪ The proposed tower does not introduce unreasonable bulk to the public realm or impact any civic landmarks or significant open space.</li> <li>▪ The proposal does not unreasonably impact any nearby heritage place or streetscape.</li> <li>▪ The design of the proposal allows for reasonable light and air to the proposed building and to the adjoining larger scale developments.</li> </ul>			

**Figure 24 – Proposed development showing setbacks**

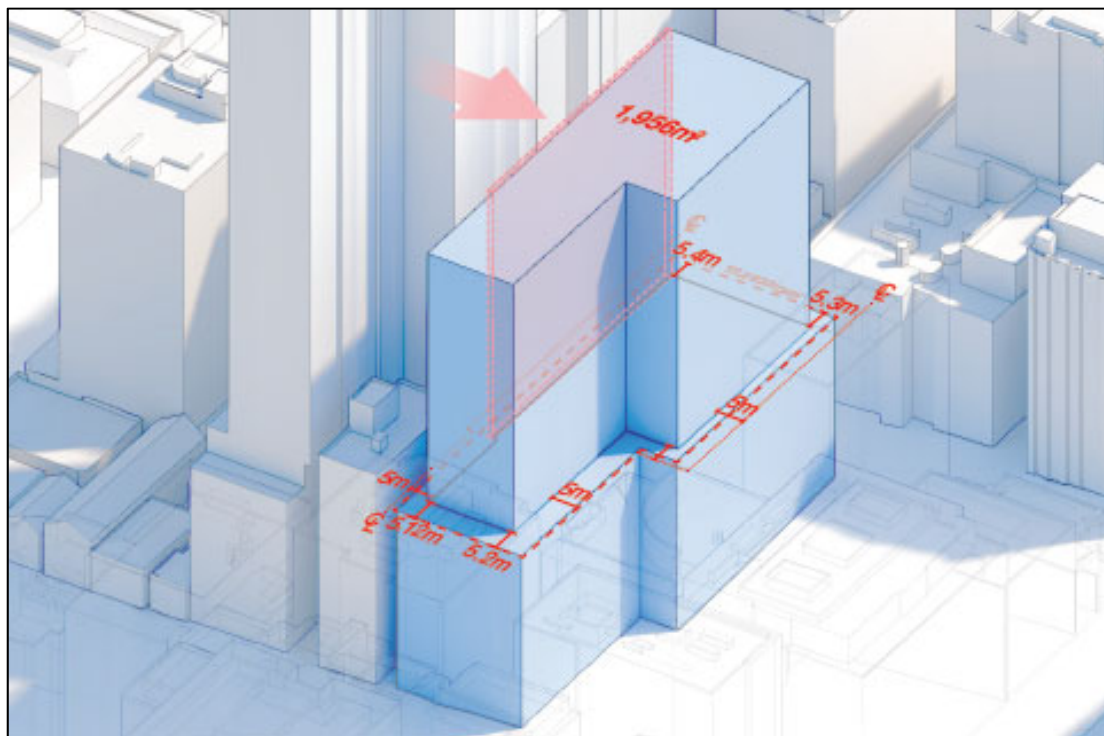
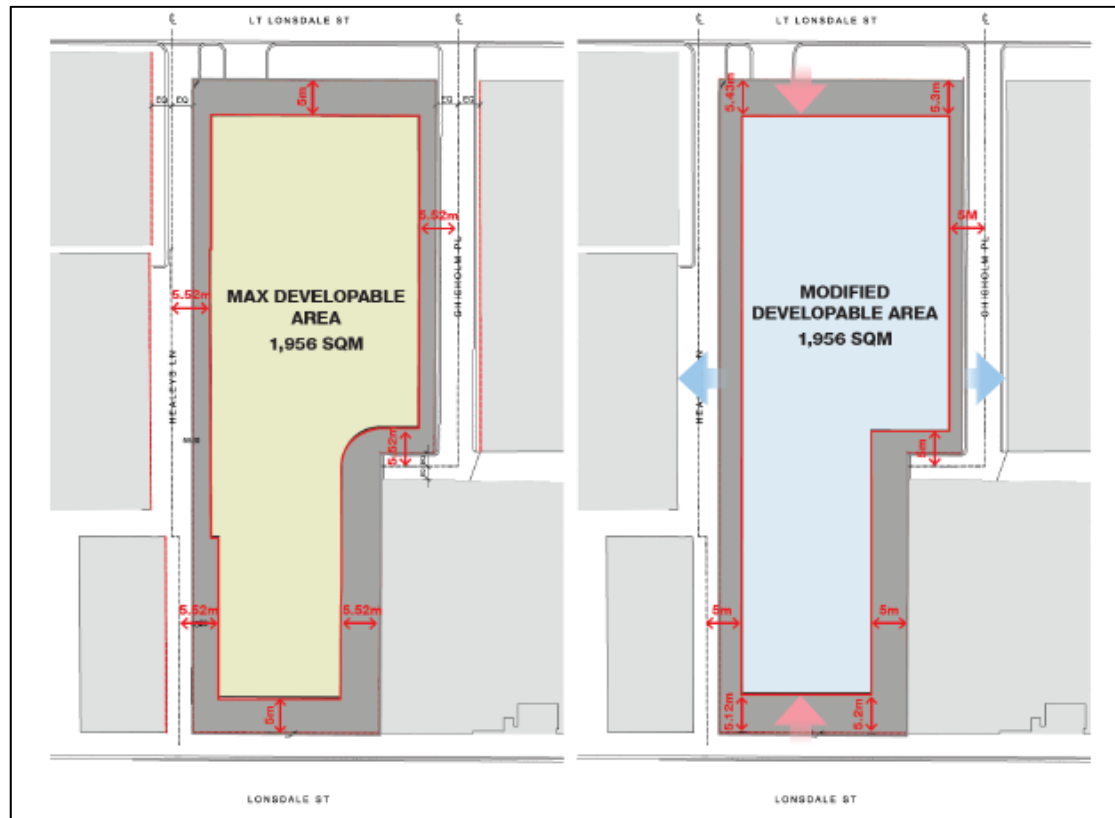


Figure 25 – Modified floorplate shown complying with maximum developable area



### 8.3 Sustainability

#### 8.3.1 Energy, Water and Waste Efficiency

The proposed development has been assessed by the City of Melbourne's ESD officer who has confirmed the proposal contains appropriate sustainability elements and satisfies the performance measures set out in Clause 22.19-5.

#### 8.3.2 Stormwater Management (Water Sensitive Urban Design)

Clause 22.23 provides that it is policy that development applications relating to new buildings incorporate water sensitive urban design that achieve the best practice water quality performance objectives set out in the Urban Stormwater Best Practice Environmental Guidelines, CSIRO 1999 (or as amended).

The supplied WSUD report details how the required treatment levels to satisfy Clause 22.23 and Green Star credit requirements are achieved by a rainwater capture and reuse system with a 60,000L tank. A STORM report is included demonstrating how this is achieved.

### 8.4 Traffic

As identified in this report, the application has not adequately addressed issues relating to the function and safety of the public realm. The application in its current form is not supported for the following reasons:

- The removal of the pedestrian footpath is not supported as it will not improve the function of the public realm; it will create an unsafe scenario for pedestrians seeking to access the proposed retail spaces as they will be

forced to walk down Healeys Lane which is a road that contains car traffic, except at its southern end.

- The Traffic conflict created by the proposal between pedestrians and motor vehicle traffic in Chisholm Place is not supported. The lack of sight lines (lack of a splayed corner) and width of the lane cannot safely provide for multiple users of the lane which generates high movement from waste collection, end of trip facilities, loading, and retails activation at Chisholm Place.
  - If a permit is issued, it is recommended that it includes conditions requiring an updated Traffic Impact Assessment (TIA) that addresses the issues identified by the City of Melbourne's Traffic Engineers. This updated TIA condition will specifically require changes relating to the widening of Chisholm Place, introduction of a pedestrian footpath at Healeys Lane, and introducing a splayed corner where Little Lonsdale Street intersects with Chisholm Place.

## **8.5 Other Matters to Consider**

### **8.5.1 Construction matters**

Given the scale of the proposed development, it is recommended that any permit issued includes a permit condition requiring a demolition and construction management plan to the satisfaction of the City of Melbourne's Construction Group. This will allow for the reasonable control of any impact the proposed construction works may have on the surrounding locality.

## **9 RECOMMENDATION**

That DELWP be advised that the City of Melbourne does not object to the granting of a planning permit provided the recommended conditions are applied. The permit should contain the following preamble:

*Demolition and buildings and works to construct a multi-level building (Office and Retail Premises) in accordance with the endorsed plans.*

## **10 CONDITIONS**

In the event a permit is issued by DELWP, it is recommended the following conditions be applied.

### **Amended Plans**

1. Prior to commencement of development, including demolition and bulk excavation, an electronic copy of the plans, drawn to scale must be submitted to the Responsible Authority generally in accordance with the discussion plans prepared by Bates Smart Architecture, dated 9 November 2020, but amended to show:
  - a) The tower setback increased to a minimum of 5 metres from the Little Lonsdale Street boundary in accordance with the definition of a setback at DDO10 (clear of intrusions other than architectural features less than 300mm).
  - b) The setback dimensions corrected so that the elevation and plan drawings match.

- c) A setback at Healeys Lane to create a continuous pedestrian colonnade of at least 2 metres clear width and full height up to Level One at Lonsdale Street and Level Two at Little Lonsdale Street.
- d) Introducing a splayed corner at the intersection of Little Lonsdale Street and Chisholm Place to provide safe vehicle sightlines.
- e) Widening the northern end of Chisholm Place up to the vehicle entry point to provide adequate passing and manoeuvring space.
- f) The solar PV system to be shown on architectural plans as per ESD report.
- g) Any changes as required by the Traffic Impact Assessment required by condition 12.
- h) Any changes required by the Waste Management Plan required by condition 13.

The amended plans must be to the satisfaction of the Responsible Authority and when approved will be the endorsed plans of this permit.

#### **Compliance with Endorsed Plans**

- 2. The development and uses as shown on the endorsed plans must not be altered or modified unless with the prior written consent of the Responsible Authority.
- 3. Once the development has started it must be continued and completed to the satisfaction of the Responsible Authority.
- 4. Prior to the occupation of the development hereby approved, all buildings and works required by this permit must be completed to the satisfaction of the Responsible Authority.

#### **Façade Strategy**

- 5. Prior to the commencement of the development, a façade strategy, including a schedule and samples of all external materials, colours and finishes and colour rendered and notated plans/elevations, must be submitted to and approved by the Responsible Authority. The façade strategy must address the following matters:
  - a) Plans, elevations and sections generally at a scale of 1:20 illustrating typical podium details, entries and doors, utilities and key junctures.
  - b) Provision of detailed glazing specification outlining colour, reflectivity and transparency of glazing.
  - c) A schedule and physical sample board of all external materials.
- 6. Glazing materials used on all external walls must be of a type that does not reflect more than 20% of visible light when measured at an angle of 90 degrees to the glass surface, to the satisfaction of the Responsible Authority.
- 7. External building materials and finishes must not result in hazardous or uncomfortable glare to pedestrians, public transport operators and commuters, motorists, aircraft, or occupants of surrounding buildings and public spaces, to the satisfaction of the Responsible Authority.

#### **Legal Agreement – Pedestrian Footpath**

- 8. Prior to the commencement of the development, the owner of the land must enter into an agreement with the City of Melbourne pursuant to Section 173 of the Planning and Environment Act 1987 regarding the introduction of a

pedestrian footpath along Healeys Lane and widening of Chisholm Place. The Agreement must:

- a) Provide that the Healeys Lane footpath and widened section of Chisholm Place (including any corner splay) will remain privately owned and controlled.
- b) Require the Owner to maintain 24-hour unobstructed public access (7 days a week) to the Healeys Lane footpath and widened section of Chisholm Place.
- c) Provide that the Owner is solely responsible for the care and maintenance of the Healeys Lane footpath and widened section of Chisholm Place at the Owners cost and to the satisfaction of Council.
- d) Indemnify Melbourne City Council from liability.
- e) The owner must, at its costs construct the Healeys Lane footpath and widened section of Chisholm Place to Melbourne City Council's – Engineering Services standards.
- f) Be to the satisfaction of the Melbourne City Council.

The Owner must pay all of the Melbourne City Council's reasonable legal costs and expenses of this agreement, including preparation, execution and registration on title.

#### **Legal Agreement – Demolition in the Capital City Zone**

9. Prior to the commencement of the demolition or removal of existing buildings or works (excluding demolition or removal of temporary structures) on the land, the owner of the land must enter into an agreement with the Responsible Authority pursuant to Section 173 of the *Planning and Environment Act 1987*. The agreement must provide the following:
  - a) If the land remains vacant for 6 months after completion of the demolition;
  - b) Demolition or construction activity ceases for a period of 6 months; or
  - c) Construction activity ceases for an aggregate of 6 months after the commencement of the construction,

The owner must construct temporary works on the land to the satisfaction of the Responsible Authority.

Prior to the commencement of construction of the temporary works, details of the works must be submitted to and be to the satisfaction of the Responsible Authority.

Temporary works may include:

- a) The construction of temporary buildings for short-term retail or commercial use. Such structures shall include the provision of an active street frontage; or
- b) Landscaping of the site for the purpose of public recreation and open space.

The owner of the land must pay all of the Responsible Authority's reasonable legal costs and expenses of this agreement, including preparation, execution and registration on title.

#### **Bicycle Parking**



10. The design/dimensions of the bicycle parking should comply with the relevant Australian Standards or Bicycle Network guidelines to the satisfaction of the Responsible Authority or Melbourne City Council - Engineering Services.

### **3D Modelling**

11. Prior to the commencement of the development (excluding any demolition, bulk excavation, construction or carrying out of works) a 3D digital model of the approved development must be submitted to, and must be to the satisfaction of the Responsible Authority. The model should be prepared having regard to the Advisory Note – 3D Digital Modelling Melbourne City Council.

Digital models provided to the Melbourne City Council may be shared with other government organisations for planning purposes. The Melbourne City Council may also derive a representation of the model which is suitable for viewing and use within its own 3D modelling environment. In the event that substantial modifications are made to the building envelope a revised 3D digital model must be submitted to, and be to the satisfaction of the Responsible Authority.

### **Traffic Impact Assessment**

12. Prior to the commencement of development (including demolition and bulk excavation), an updated Traffic Impact Assessment (TIA) report generally in accordance with the TIA prepared by GTA Consultants, dated 29 July 2020, and the additional response to addressing issues raised prepared by GTA Consultants, dated 10 November 2020 must be submitted to and approved by the Melbourne City Council - Infrastructure and Assets. The updated TIA must include revised plans and associated updated swept path assessments, which detail a layout for all vehicle parking and access ways on-site that demonstrates compliance with relevant Australian Standards and the design standards of the Melbourne Planning Scheme, and provide the following:
  - a) Widening of Chisholm Place.
  - b) Introducing a splayed corner where Little Lonsdale Street intersects with Chisholm Place.
  - c) Updated swept path diagrams that show access to the site that does not rely on the removal of on street parking or reliance on vehicles mounting kerbs.

When provided to the satisfaction of the Responsible Authority, the TIA will be endorsed to form part of this permit.

### **Waste**

13. Prior to the commencement of the development, an amended Waste Management Plan (WMP) must be submitted to and approved by Melbourne City Council – Waste and Recycling. The amended WMP must be generally in accordance with the WMP prepared by Leigh Design Pty Ltd, dated 10 November 2020, but amended to include:
  - a) Show the expected path of travel for the ground floor tenants to the waste storage area, noting that internal access within the development is required.
  - b) Provide swept path diagrams that allow for suitable access without relying on changes to the on street car parking or mounting kerbs.

The amended WMP must not be altered without prior consent of the Melbourne City Council – Waste and Recycling.

14. No garbage bin or waste materials generated by the development may be deposited or stored outside the site and bins must be returned to the garbage storage area as soon as practical after garbage collection, to the satisfaction of the Responsible Authority.

#### **Environmentally Sustainable Design (ESD)**

15. Prior to the occupation of any building approved under this permit, a report from the author of the endorsed ESD report, or similarly qualified persons or companies, outlining how the performance outcomes specified in the amended ESD report have been implemented must be submitted to the Responsible Authority. The report must be to the satisfaction of the Responsible Authority and must confirm and provide sufficient evidence that all measures specified in the approved ESD report have been implemented in accordance with the relevant approved plans. This may include a GBCA Green Star Design & As Built Design Review Rating to confirm compliance with the stated Green Star targets.
16. Prior to commencement of development, a complete Landscape package, Landscape Maintenance Plan and a Landscape Irrigation Performance Specification in connection with the proposed development must be submitted to, and be approved by the Responsible Authority. The landscape package should include detailed planter sections including soil volumes and schedules of species with specific consideration given to soil volume requirements and growing medium proposed. The Landscape Maintenance Plan should provide details of proposed maintenance regimes with provision for maintenance beyond the fifty two week period following Practical Completion. Except with the prior written consent of the Responsible Authority the approved landscaping must be implemented prior to the occupation of the development. The landscaped area(s) must be maintained to the satisfaction of the Responsible Authority.

#### **Engineering Services**

17. All projections over the street alignment must be drained to a legal point of discharge in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
18. The title boundaries for the property may not exactly agree with the road alignments of the abutting the Melbourne City Council's laneways. The approved works must not result in structures that encroach onto the Melbourne City Council's laneways.
19. Prior to the commencement of the development, a stormwater drainage system, incorporating integrated water management design principles, must be submitted to and approved by Melbourne City Council – Infrastructure and Assets. This system must be constructed prior to the occupation of the development and provision made to connect this system to the Melbourne City Council's underground stormwater drainage system. Where necessary, the Melbourne City Council's drainage network must be upgraded to accept the discharge from the site in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
20. Prior to the commencement of the development all Council drains in Chisholm Place, Healeys Lane and within the subject site must be localised and if necessary relocated outside the property boundary at no cost to Council and in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.

21. The Council stormwater drain located across the subject site from Chisholm Place to Healeys Lane must be relocated within the proposed new building in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets. The drain must be constructed clear of any permanent internal structures and be easily accessible by Council and its employees and representatives for inspection, maintenance and repair.
22. Prior to the commencement of the use/occupation of the development a drainage easement must be created on a plan of subdivision over the drain located across the site from Chisholm Place. The easement must be no less than 3m wide and be to the satisfaction of Melbourne City Council - Infrastructure and Assets and Team Leader Land Survey.
23. Prior to the commencement of the use/occupation of the development, all necessary vehicle crossings must be constructed and all unnecessary vehicle crossings must be demolished and the footpath, kerb and channel reconstructed, in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
24. All portions of roads and laneways affected by the building related activities of the subject land must be reconstructed together with associated works including the reconstruction or relocation of services as necessary at the cost of the developer, in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
25. The footpaths adjoining the site along Lonsdale Street and Little Lonsdale Street must be upgraded in new sawn bluestone together with associated works including the relocation of kerb and channel, provision of street furniture and modification of services as necessary at the cost of the developer, in accordance with plans and specifications first approved by City of Melbourne – Infrastructure and Assets.
26. The footpath adjoining the site along Chisholm Place must be reconstructed together with associated works including the renewal of kerb with new sawn 300mm wide bluestone kerbs and modification of services as necessary at the cost of the developer, in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
27. Existing street levels in roads adjoining the site must not be altered for the purpose of constructing new vehicle crossings or pedestrian entrances without first obtaining approval from Melbourne City Council – Infrastructure and Assets.
28. All street lighting assets temporarily removed or altered to facilitate construction works shall be reinstated once the need for removal or alteration has been ceased. Existing public street lighting must not be altered without first obtaining the written approval of City of Melbourne – Infrastructure and Assets.
29. Existing street furniture must not be removed or relocated without first obtaining the written approval of Melbourne City Council – Infrastructure and Assets.
30. All street furniture such as street litter bins recycling bins, seats and bicycle rails must be supplied and installed on Lonsdale Street and Little Lonsdale Street footpaths outside the proposed building to plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.
31. Prior to the commencement of the use/occupation of the development, the owner of the land must enter into an agreement with the City of Melbourne, pursuant to Section 173 of the Planning and Environment Act 1987. The owner of the land must pay all of the Melbourne City Council's reasonable legal costs

and expenses of this agreement, including preparation, execution and registration on title. The agreement must provide for the following:

- a) Access from Council and its employees and representatives for inspection, maintenance and repair of the stormwater drain located within the drainage easement to be created over the drain from Chisholm Place across the subject site.
  - b) Maintenance and repair of all structures above or below the drainage easement at the Owner(s) cost and to the satisfaction of the Melbourne City Council whether or not the need for maintenance or repair of the structures arose from the act, neglect or default of the Owner(s)
  - c) Indemnify the Melbourne City Council against any claims arising from the existence of structures above and or below the drainage easement.
32. Prior to the commencement of the development, excluding preliminary site works, demolition and any clean up works, or as may otherwise be agreed with the Responsible Authority or Melbourne City Council, a lighting plan must be prepared to the satisfaction of Melbourne City Council. The lighting plan should be generally consistent with Melbourne City Council's Lighting Strategy, and include the provision of public lighting in Lonsdale Street and Little Lonsdale Street adjacent the subject site and Healeys Lane and Chisholm Place. The lighting works must be undertaken prior to the commencement of the use/occupation of the development, in accordance with plans and specifications first approved by Melbourne City Council – Infrastructure and Assets.

#### **Demolition Management Plan**

33. Prior to the commencement of the development, including demolition and bulk excavation, a detailed demolition management plan must be submitted to and be approved by the Melbourne City Council – Construction Management Group. This demolition management plan must be prepared in accordance with the Melbourne City Council – Construction Management Plan Guidelines and is to consider the following:
- a) public safety, amenity and site security.
  - b) operating hours, noise and vibration controls.
  - c) air and dust management.
  - d) stormwater and sediment control.
  - e) waste and materials reuse.
  - f) traffic management.

#### **Construction Management Plan**

34. Prior to the commencement of the development, excluding demolition and including bulk excavation, a detailed construction management plan must be submitted to and be approved by the Melbourne City Council – Construction Management Group. This construction management plan must be prepared in accordance with the Melbourne City Council – Construction Management Plan Guidelines and is to consider the following:
- a) public safety, amenity and site security.
  - b) operating hours, noise and vibration controls.