#### Report to the Future Melbourne (Planning) Committee

#### Ministerial Planning Referral: TPM-2019-27 633-669 Flinders Street, Docklands

Presenter: Evan Counsel, Director Planning and Building

#### Purpose and background

- 1. The purpose of this report is to advise the Future Melbourne Committee of a Ministerial planning referral seeking approval for the partial demolition and buildings and works for the refurbishment of the existing building at 633-669 Flinders Street, Docklands (refer Attachment 2 Locality Plan).
- 2. The applicant is Ethos Urban, the owner is Zone Q New Central Investments Pty Ltd, and the architect is Cox Architects.
- 3. The land is located within the Capital City Zone Schedule 1 (CCZ1) and is affected by Design and Development Overlay Schedule 10 (DDO10) and Parking Overlay Schedule 1 (PO1).
- 4. The proposal is to refurbish the existing building, which was purpose built for Victoria Police, into an office building. The proposal includes additions to the building envelope, alterations to the building appearance, an increase in office floor space, a reduction in the number of car parking spaces and the introduction of ground level retail tenancies.
- 5. The Minister for Planning has referred the application to the Council as a Recommending Referral Authority.

#### Key issues

- 6. The key issues relevant to this proposal relate to the built form, including setbacks, appearance and streetscape interfaces; public realm impacts, including wind and shadow; equitable development; and environmentally sustainable design.
- 7. The refurbishment of the existing building, together with the reduction of car parking, will make a positive contribution to the World Trade Centre complex and the Docklands precinct.
- 8. Subject to conditions, the proposed building envelope and appearance, public realm impacts and sustainability outcomes are consistent with relevant provisions of the Melbourne Planning Scheme.

#### **Recommendation from management**

9. That the Future Melbourne Committee resolves to advise the Department of Environment, Land, Water and Planning that Melbourne City Council does not object to the application subject to the conditions outlined in the delegate report (refer to Attachment 4 of the report from management).

Attachments:

- 1. Supporting Attachment (Page 2 of 151)
- 2. Locality Plan (Page 3 of 151)
- 3. Urban Context Report (Page 4 of 151)

4. Delegates Report (Page 114 of 151)

21 July 2020

#### **Supporting Attachment**

#### Legal

- 1. The Minister for Planning is the Responsible Authority for determining this application.
- 2. The Minister for Planning has referred the application to Melbourne City Council pursuant to Section 55 of the *Planning and Environment Act 1987* (Act).

#### Finance

3. There are no direct financial issues arising from the recommendations contained within this report.

#### **Conflict of interest**

4. No member of Council staff, or other person engaged under a contract, involved in advising on or preparing this report has declared a direct or indirect interest in relation to the matter of the report.

#### Health and Safety

5. Relevant planning considerations such as traffic and waste management, potential amenity impacts and potentially contaminated land that could impact on health and safety have been considered within the planning permit application and assessment process.

#### Stakeholder consultation

- 6. The application is exempt from the notice of section 52(1)(a), (b) and (d), the decision requirements of section 64(1), (2) and (3), and the review rights of section 82(1) of the Act.
- 7. It is the responsibility of Department of Environment, Land, Water and Planning on behalf of the Minister for Planning to refer this application to other referral authorities where required.

#### **Relation to Council policy**

8. Relevant Council policies are discussed in the attached delegate report (refer Attachment 4).

#### **Environmental sustainability**

- 9. The development has acceptable ESD targets to satisfy the minimum requirements of Clause 22.19 (Energy, Water and Waste Efficiency) and Clause 22.23 (Stormwater Management).
- 10. Conditions are recommended to ensure the development is carried out in accordance with a submitted ESD Report.

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# **Locality Plan**

Attachment 2 Agenda item 6.2 Future Melbourne Committee 21 July 2020



# FLINDERS ST

# Urban Context Report

SEPTEMBER 2019



Attachment 3 Agenda item 6.2 Future Melbourne Committee 21 July 2020

### URBAN CONTEXT REPORT: 637 FLINDERS ST

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# 01/1 INTRODUCTION

# ARCHITECT'S STATEMENT

637 Flinders Street within the World Trade Centre Precinct is situated at a cornerstone location within the city and presents a great opportunity to revive this unique corner of the CBD.

Zone Q and Artifex have brought together a team of experienced consultants, lead by the expertise of COX Architects to implement a compelling concept as outlined in this detailed masterplan presentation for a 14 level commercial tower, activated roof terrace and ground floor business hub related program.

At street level, the proposal seeks to integrate the proposed MCC masterplan for the precinct; to pedestrianise Siddeley St and engage with the river frontage by strengthening the public realm with throughsite connections, and maximise opportunities to activate and enliven the street edges. This further invites pedestrian movement into and through the heart of the site, creating a public thoroughfare to both the river and the neighbouring green-scape, Seafarer's Wharf and Batman Park. A truly technologically connected proposal, the business hub and commercial foyer experience combines to offer an integrated and activated locality, rich in amenity for tenants, entrepreneurs and the public alike.

The core driving principle of re-connecting to the site's historical maritime significance is to ensure that the precinct proposal not only adds value to the evolving urban grain, but continues to pay homage to the preceding narrative which helped shape the city fringes. In doing so, this will further catalyse the river front identity and characterise the collective desire to create a sense of place.

We relish the opportunity to continue our ongoing relationship with and development of the old WTC precinct. We see this as a prospect to work closely with the MCC, and the neighbouring developments to align the evolving urban narrative using our combined experience and expertise. Further enriching and invigorating Melbourne's historical maritime history and global reputation as a liveable city.



### URBAN CONTEXT REPORT: 637 FLINDERS ST

INTRODUCTION

# 01/2 INTRODUCTION

# THIS REPORT /2

This town planning application has been prepared by Cox Architects and Urban Ethos on behalf of Zone Q & Artifex. The site of the proposed development is 637 Flinders Street, Melbourne; comprising a re-generation of the existing 14 storey office tower and podium of the old World Trade Centre site (currently accommodated by the Victoria Police Department) including the implementation of ground floor business hub and retail related program. In addition to the development and activation of 637 Flinders St, Zone Q & Artifex are already working closely with neighbouring developers to re-purpose and re-invigorate the urban realm and site connections to the historical river frontage.

The proposal reflects discussions that Artifex have held to date with the Department of Environment, Land, Water and Planning (DELWP), and Melbourne City Council (MCC) - the intention being to introduce a compelling new development to the evolving 'New Harbour Precinct.'

The report presents the planning and urban design merits of the proposed development. Demonstrating how the building design responds appropriately to its context, site opportunities

### PROJECT SUMMARY /3

637 Flinders Street leverage's the unique amenity offered by the location of the site and its proximity to Sturt Street Reserve. Presenting an opportunity to create a distinctive gateway to the arts precinct, the proposed scheme seeks to reinforce the urban realm objectives of the WTC precinct, as set out in the Melbourne Planning Scheme.

The proposed development comprises:

Levels	14 Levels + plant
Floorplate Size (Average NLA)	615 - 3,580m <sup>2</sup>
Co-Working Hub	1,892m <sup>2</sup>
New Retail GLAR	620m <sup>2</sup>
Car Parking No.	121
Bicycle Parking No.	202
Plot Ratio (FAR)	10.16 / 1
Gross Floor Area (GFA)	42,776m <sup>2</sup>
Net Lettable Area (NLA)	27,341m <sup>2</sup>

and constraints, to support the granting of a planning permit.

The following report will analyse the urban context, both at a macro scale (relating to the city-scape and waterfront heritage considerations), and at a local scale. The Urban Design Response is guided by the following Urban Design Principles:

Creating a 'Gateway to the Arts Precinct.'
 A New Civic Hub
 A Sustainable Future
 Enhancing the Precinct
 A Connected Environment

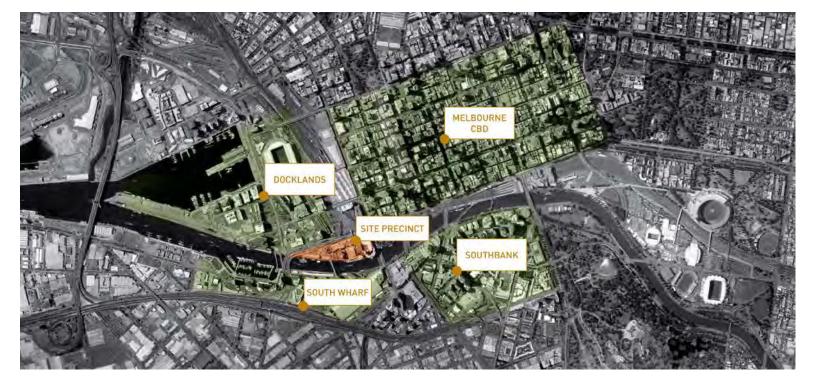
For an assessment of the proposal's consistency with relevant policies and controls in regards to State and Local Planning Policy, please refer to Ethos Urban's Planning Report.

The report concludes that the proposed development is an appropriate response to its context and will make a positive contribution to the quality of the Architecture and the Public Realm of South Melbourne and the Arts Precinct.

### PROJECT TEAM /4

Developer:	Zone Q & Artifex
Architecture & Urban Design:	Cox Architecture
Structural Engineering:	Aurecon
Building Services Engineer	NDY
Town Planning Consultants:	Ethos Urban
Project Managers:	Initial Cap
Quantity Surveyor:	Slattery
Building Surveyor:	PLP
Facade Engineer:	Aurecon
ESD Consultant:	NDY
Wind Consultants:	TBC
Waste Consultants:	TBC
Landscape Architecture:	OCULUS
Traffic Consultants:	Traffix Group
Land Surveyor:	Veris
Real Estate:	Colliers

#### COX Urban Context Report



# 01/5 INTRODUCTION

# THE SITE

The site has an Area of 4,209m<sup>2</sup> with the following frontages;

104.04 m to Flinders St (Northern Boundary) 40.46 m to 671 Flinders St (Western Boundary) 104.04 m to Siddeley St (Southern Boundary) 40.46 m to Flinders St (Eastern Boundary)

The subject site lies between the Flinder's Street to the North, 671 Flinders St property to the West, Siddeley Street (and the Yarra River within proximity) to the South, and the extended road and connection of Siddeley St to the East.

The Northern boundary interfaces with the Western end of Flinders St. An aggressive road, the pedestrian activity is minimal and primarily functions as a transport connection to the Docklands, Western suburbs and Fisherman's Bend.

The Western boundary adjoins the neighbouring property of 671 Flinders St. The subsequent

title boundary encroaches the proposed site by 1.5m. The nearby 'Seafarer's Rest' concludes the pedestrian link, and development of the riverfront civic space, to Batman Park along Siddeley St.

The Southern boundary is adjacent to Siddeley St, an under-utilised, tree lined pedestrian back street. Siddeley St currently services commercial loading zones and car parking into neighbouring properties. The key building demographic separating the 637 Flinders St site and the river is of commercial use. An above ground walkway links the South-Eastern corner of the site with the 38 Siddeley St site.

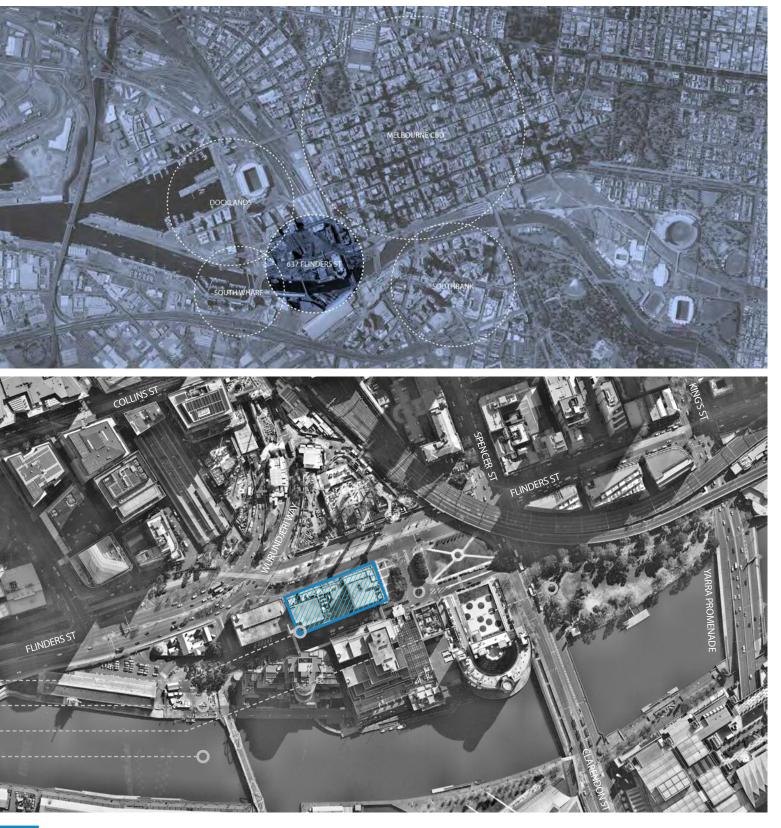
The Eastern boundary interfaces the WTC plaza site, owned by Riverlee. It is enclosed by the extension of Siddeley St, with MCC undertaking urban design initiatives to link (the under construction) Melbourne Quarter and surrounding area, with Siddeley St and the Yarra River.

Subject Site - 637 Flinders Street

Seafarer's Rest Park

Batman Park

Yarra River



Building Footprint

# URBAN CONTEXT REPORT: 637 FLINDERS ST

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# **URBAN CONTEXT**

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SEAFARERS PLACE:

# **URBAN CONTEXT ANALYSIS**

# SITE & SURROUNDS

Transport & Amenity:

The site is centred on a transport and amenity nodal point in Melbourne. Transport and main road arterials connecting to the Western suburbs, Melbourne's South and the Airport Link are within immediate proximity.

The Dockland's, Fisherman's Bend and Melbourne's Southbank areas have seen significant development over the past 20 years. 637 Flinder's St forms the interstitial layer. A prominent address which interlocks these neighbouring precincts, acting as a nexus for growth and activity.

As such, the proposal can be read in conjunction with the surrounding developments. This precinctual design response breeds a sense of place, community and urban identity.

#### Surrounding Precinct Development:

#### Melbourne Quarter:

Mid-rise development comprising a 6 Star hotel, residential apartments, 3 commercial towers and plentiful publicly accessible green space.

One of the latest all encompassing precinctual developments, MQ will feature a mix of retail (4,000m<sup>2</sup>) and commercial opportunities, with the cities first elevated 'Skypark.' Due for completion July 2020.

### World Trade Centre Plaza:

A site owned by Riverlee.

#### Flinders Bank / St Regis Hotel:

A unique development comprising a 5 Star hotel (167 keys), residential apartments, retail amenity (2,100m<sup>2</sup>) and open civic space.

A Century Group development consisting of three interconnecting buildings, the proposal terraces toward and provides connections to the river frontage. Due for competion in Q4 2021

#### Seafarers Place:

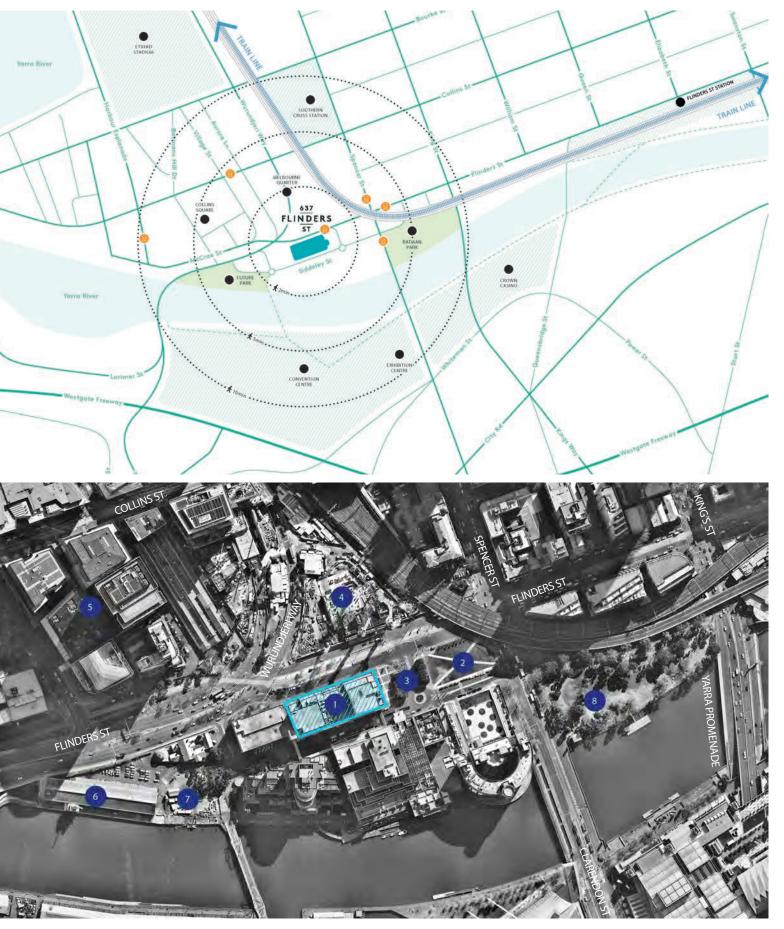
Mid-rise development comprising a 5 Star hotel, residential apartments, function and civic space.

A Riverlee development in a truly mixed-modal hotel, residential and civic proposal. The development, estimated for completion in 2022, looks to engage and activate the river front. A 1000 seat function/ events space in the restored Northbank Goods Shed, will include a 3,500m<sup>2</sup> public park.





FLINDERS BANK/ ST REGIS HOTEL:





Subject Site - 637 Flinders St /1 /5 Collins Square Flinder's Bank - St. Regis Hotel /2 /6 Seafarer's Place WTC Plaza Site - Owned by Riverlee /3 /7 Seafarer's Rest Park Melbourne Quarter - 691 Collins St /4 /8 Batman Park

# 02/2 URBAN CONTEXT ANALYSIS

# URBAN CONNECTIONS

Precinct and transport linkages:

The 'Harbour Precinct' and Melbourne Quarter (and the nearby Collins Square) have been designed in a campus style, precinctual development. This has, to some degree, taken shape because of the surrounding infrastructure; Flinders Street, Wurundjeri Way, Collins St and the trainline. This urban typology focuses on encapsulating a variety of workplace, residential, retail, commercial, hotelier and lifestyle based amenity. The purpose being to populate and activate the area as a 24 hour entity.

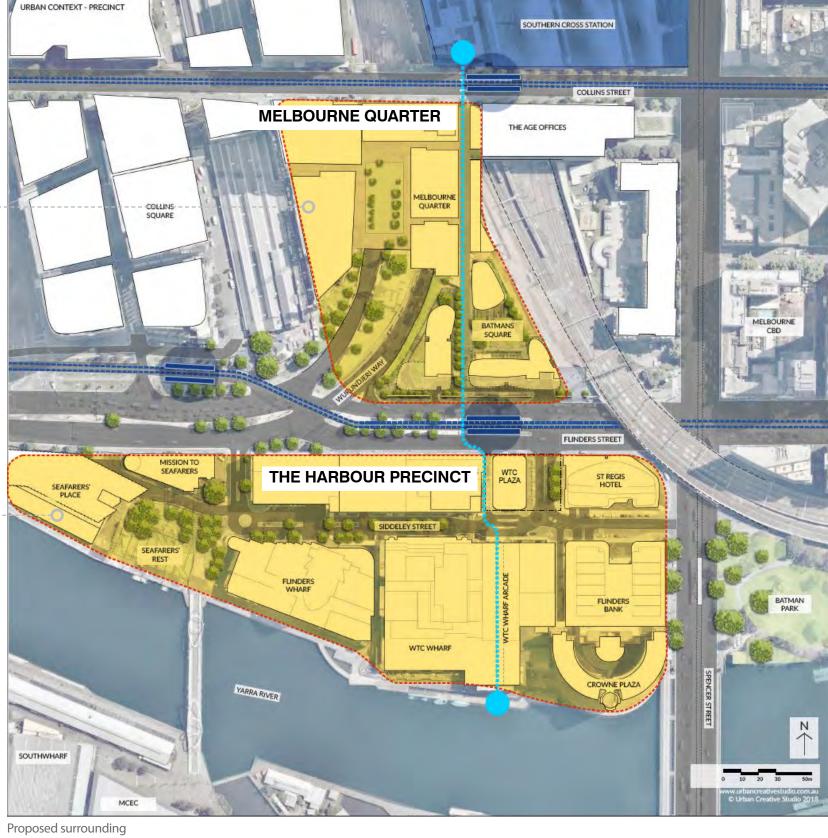
Creating linkages between these neighbouring precincts is integral to the urban growth and longevity of the area. Ensuring a strong connection to public transport facilities and clear linkages to the river edge are eminent to the success of the project.

PRECINCT POPULATION Apartments 1,594 Commercial Retail

105,500m<sup>2</sup> 4,200m<sup>2</sup>

#### PRECINCT POPULATION

Apartments 946 89,000m<sup>2</sup> Commercial Retail 6,280m<sup>2</sup> Hotel Rooms 1,099m<sup>2</sup>



urban developments

# 02/3 urban context analysis

# EXISTING STREETSCAPES & INTERFACES

The existing conditions of 637 Flinders St are defined by; the proximity of and interface with the agressive Flinders St to the North, the open public forecourt of the old World Trade Centre (WTC) precinct to the East, the narrow back of house (BOH) service road to the South, Siddeley St, and the abutting car park/ property to the West.

There are a series of discussions being held between the owners surrounding the precinctual development, including the use of and upgrade of building electrical services (currently running underside of the red bridge).

Melbourne City Council (MCC) are proposing to take out the car parks running along Siddeley St and repurpose the area for more green-scape and planting, the outcome seeking to further pedestrianise the street and create a more amenable connection to both the neighbouring Sea-farers Rest Park, Batman Park and the Yarra River.





North - East Facade: The view indicates the prominent view along Flinders St, and the now distinctive terraces from the old WTC centre site.

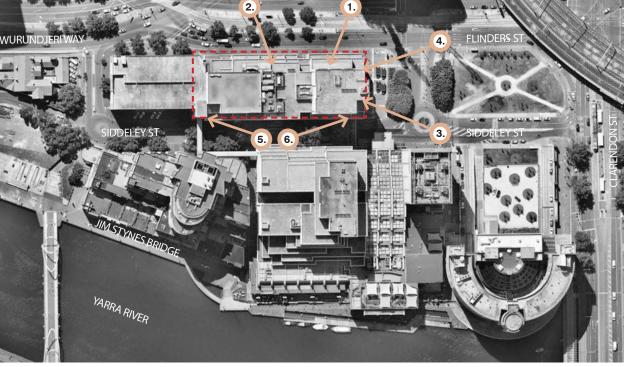


3.

East Facade 01:



5.



637 Flinders St Existing Conditions;





The view highlights the aggressive nature of the Flinders St frontage, with heavily vehicle traffic.

Discussions are ongoing between surrounding precinct owners around the worth of and desire to remove the red box and bridge link.

East Facade 02:

The Eastern core is a prominent pivoting point in the original brutalist form. It anchors the site, and adjoins the main commercial entry.

South - West Facade: This image depicts the character of Siddeley St, a back of house thoroughfare with entry points to loading a service zones.

#### South - East Facade:

6. Minimal terracing of the built form occurs on the Southern side, resulting in quite an imposing street-scape. Several planar trees are maintained at the pedestrian scale. Melbourne City Council (MCC) have plans to widen the greenscaping along Siddeley St, resulting in a more pedestrian friendly link.



# **URBAN CONTEXT ANALYSIS**

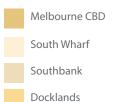
# URBAN CONTEXT



#### 2.2.1 Urban Structure

The site sits within the capital city zone, and is defined by the urban scale shift in development between the Docklands and Melbourne CBD.

The area has been transformed over time from an industrial, marshy landscape to an area of urban development of multiresidential, retail and commercial grade.



#### 2.2.2 Building Heights

The built form in the 'harbour preinct' is quickly developing, with predominantly mid-rise to tall residential and commercial towers of between 14 to 35 levels. The result is a tapered height and urban density from the CBD and Docklands towards the riverfront.

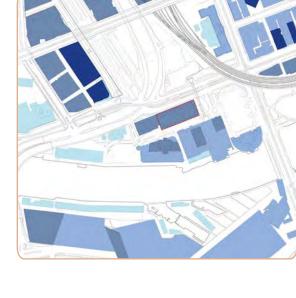
Both Flinders & Collins Street are evolving streetscapes, with several mixed-use multi-residential, commercial and hotel opportunities having already received planning approval varying in height up to and in excess of 100m.

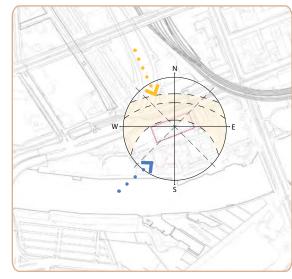
- 0 to 20m 21m to 50m
- 51m to 100m
- >100m

### 2.2.5 Urban Environment

The site, with its large North site frontage, has ample access to light (under construction Melbourne Quarter providing future shade), while direct sunlight from the undesirable glare in the morning and warm afternoon sun will define the articulated North facade.

The prevailing warm winds will come from the North whilst the cool winds typically enter from a South Westerly direction.







#### 2.2.3 Land Use

Land use within the Art Precinct Blueprint is mixed. It incorporates an emerging commercial and retail component defined by Southbank, the rivers edge and the evolving arts blueprint. The East and Southern precincts adjoining the site remain heavily scattered with a mix of residential, commercial and institution. The King's Way freeway continues to disconnect the southern and neighbouring city zone.

- Institutional/Education
- Residential
- Retail
- Hotel
- Commercial





#### 2.2.4 Urban Respite

Melbourne's CBD and Dockland areas are defined by a mix of fine grain and high density urban forms. This scale shift offers exciting and diverse spatial opportunities, however this urban environment relies upon the scale breaking down within areas to offer respite from the nature of large built up areas.

These areas offer a reprieve from the busy nature of the urban surrounds and act as a place to gather, collect and observe. As this diagram shows, respite areas within this precinct are clustered towards the riverfront, and provides the site the opportunity to further add linkages and engage with these public areas of relief.

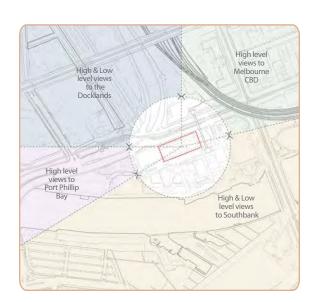


#### 2.2.6 Views & Vistas

This site will offer panoramic views to the CBD, Yarra River the Docklands and Southbank.

The East tower will offer lower level views and interactivity with Flinders St, with the higher levels having mostly unobstructed views to Melbourne city. The neighbouring Riverlee Plaza has an approved scheme for 35 floors above ground (directly to the East) and a series of 40 level buildings, namely 'Melbourne Square' (to the North) are currently under construction.

Unique aspects to the Yarra River, and both Dockland and the CBD area will largely remain in tact due to the unlikely devepment over Flinders St and the railway line.

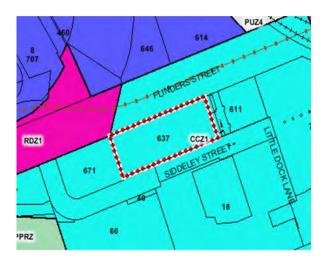


# 02/5 urban context analysis

### PLANNING CONTROLS

Docklands has transformed from one of Victoria's main ports into an extension of the Melbourne Central Area and features a range of residential, commercial and visitor attractors that accommodate housing, office, industry, research, institutional, business, education, entertainment, sporting and open space uses.

High level planning policy identifies the Melbourne Central Area as the highest order centre in Victoria and a principal location of growth and investment in the state. The policy also articulates narrative for excellent design quality and public realm outcomes to achieve the vision for Melbourne as a bold, inspirational and sustainable city.

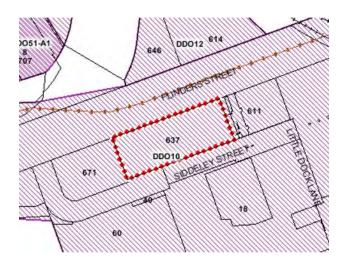


#### Schedule 1 - Zoning - Capital City Zone

- Schedule 1 of the Capital City Zone seeks a range of uses that complement the capital city function of the locality while creating an attractive, pleasurable, safe and stimulating environment in the Melbourne CBD.

Office and Retail uses are as-of-right do not require a permit to operate.

A floor area ratio of 1:18 is set whereby proposals that exceed this ratio must provide a public benefit comparative to the quantum the ratio would be varied.



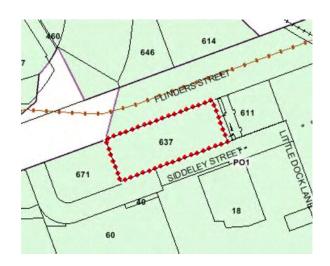
#### Schedule 10 - Design and Development Overlay

- Schedule 10 to the Design and Development Overlay sets a range of built form, wind and overshadowing requirements that require development to achieve high quality pedestrian amenity, built form, equitable development and public realm outcomes.

- The built form requirements set a mandatory maximum 40 metre street wall as well as setback and floorplate requirements for development above this street wall.

- The overlay requires development in excess of 40 metres ensures comfortable window conditions are achieved in all publicly accessible places.

- Overshadowing controls ensure key public open spaces in the central city receive adequate sunlight. The key open space identified in proximity to the subject site is the Yarra River Corridor.



#### Schedule 1 - Parking Overlay

- Schedule 1 to the Parking Overlay (PO1) identifies appropriate parking rates for various uses within the CCZ.

- PO1 triggers the requirement for a planning permit where car parking rates are provided in excess of the rates nominated in this schedule. No minimum parking requirement applies.

- The relevant rate nominated for the development is 5 x net floor area of buildings on the site in m<sup>2</sup> divided by 1,000 m<sup>2</sup>; or 12 x site area in m<sup>2</sup> divided by 1,000sqm.

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DESIGN RESPONSE

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# **DESIGN RESPONSE**

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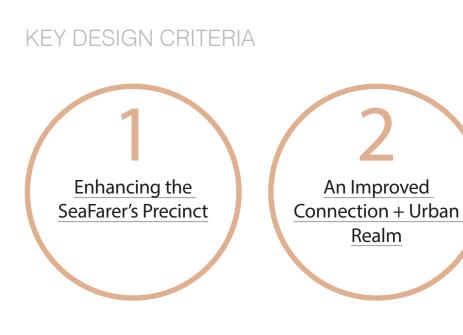
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# 03/1 URBAN INTEGRATION

# URBAN CONNECTIONS

Interconnected green-scape & Connecting to the Yarra River:

The success of the urban development, and 637 Flinder's St contribution towards it, are assistig in the activation of the street edges and facilitating a porous connection to the rivers edge.

Melbourne City Coucil (MCC) has placed an even greater emphasis in recent years on both engaging with the Yarra River and re-vitalising open green space within the city. The extention of this is not simply to re-vitalise these spaces, but to create connections and clear sightlines between them. This further encourages pedestrian movement, catalyses public amenity and supports a connection to place. Planning controls recognise the importance of engaging with and minimising overshadowing of the Yarra River. The DA approved surrounding context is made up of a variety of building heights and urban densities which taper off towards the historically significant river front.

Siddeley Street is seeing an evolving streetscape, from an acknowledged back of house, services road to a more pedestrian friendly boulevard. With a host of commercial, hotel and retail outlets encompassing the street, a more connected and activated urban vernacular is being created.

Prop

Proposed surrounding urban developments

Proposed 637 Flinder's St development

roved + Urban m
<u>Uplift the Historically</u> <u>Significant Flinders St</u> <u>Frontage</u>



### Re-Life

- Re-life the site by providing an internal coworking reprieve, retail and landscaping amenity.
- Re-imaginging a new identity for Flinders St. One which reflects on the maritime history of the site.
- Re-vitalisation & extension of Siddeley Street, working with the proposed MCC masterplan.

3

#### A Connected Environment

- Shared responsibility in creating an entrance to the Sea Farers precinct
- Promote connectivity through the site, by introducing more porous linkages and visual connections.
- Permeability enhanced, particularly to the river's edge.

KEY ELEMENTS

# 03/2 PROJECT VISION

### DESIGN PRINCIPLES

#### Design Principles:

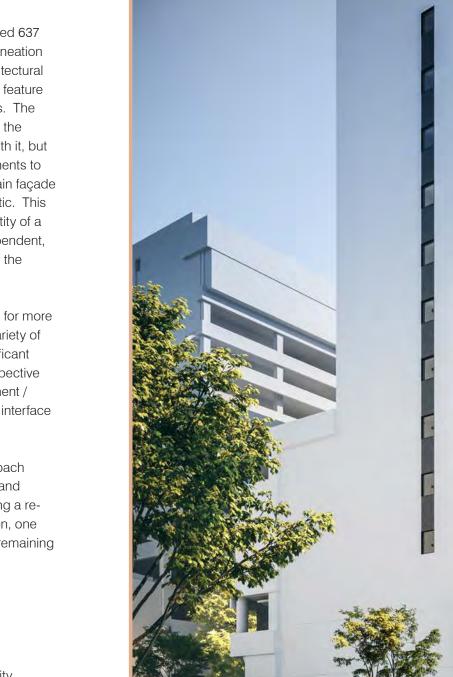
One of the key principles for the proposed 637 Flinders St development is both the delineation and accentuation of the old & new architectural form, highlighting the most pronounced feature of the existing site, the external terrraces. The brutality of the existing form is such that the design proposal should not compete with it, but integrate and compliment. The attachments to the new building in turn provides the main façade with a new modern, yet timeless aesthetic. This injects the site with a unique urban identity of a commercial A-Grade tower that is independent, and moves away from the perception of the existing WTC precinct as run-down.

The proposed façade gives opportunity for more program to Flinders Street, through a variety of possible workplace activations. A significant increase to the project NLA allows prospective tenants to engage with the more prominent / North facing façade, the Flinders Street interface and newly created green balconies.

Importantly, the integrated design approach has the ability to stitch together the old and new architectural forms - thereby creating a repurposed and unique design proposition, one established by a sense of place, whilst remaining identifiable to the buildings history.

#### Design Methodology generates;

- A delineation between the old & new architectural eras.
- A clear and newly formed urban identity
- An intelligent structural solution, providing a valuable return to the asset.
- A unique design gesture in a fast evolving commercial precinct.
- Connection back to the maritime historical context of exposed steel structural elements.



programmatic functions of the space beyond - most critically, the commercial lobby, and coworking environment, whilst minimising the

impact of the existing car park.

Activated Ground Plane

The new retail interface and enhancement of the

Siddeley St edge will encourage a more activated

and pedestrianised landscape. With a proposed

new 'skin' to the first 20m of the podium-scape,

the Flinders St frontage similarly responds to the

### Enhance Precinct

- Urban Sea Farer's Hub
- Enhanced Siddeley St, with greater urban connections to quality Green Space
- Catalyst for activity and re-generation of the Sea Farer's precinct.
- Development of Siddeley St, as a more pedestrian-focused, community link.

#### COX Urban Context Report

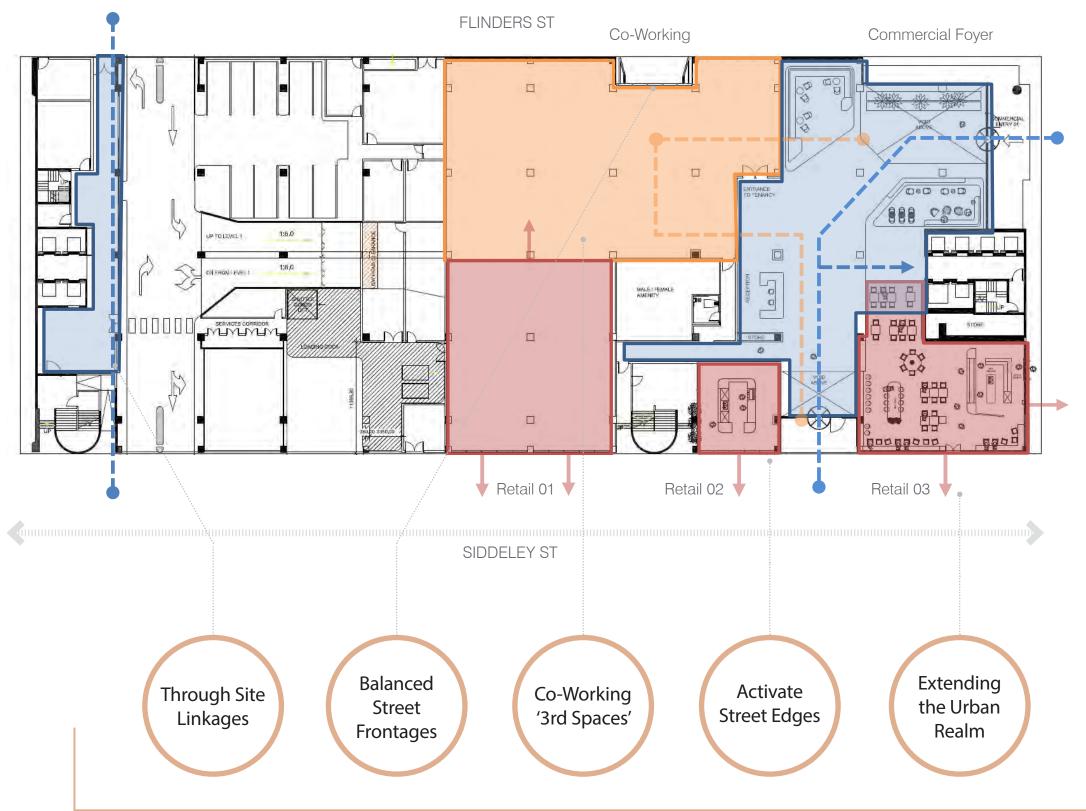


# URBAN INTEGRATION

Activated Edges - Co-Working + Retail Ground Floor Connections

The evolving precinctual developments provides Siddeley St with an opportunity to create new linkages and urban amenity. The diversity of spaces provided between the retail, commercial foyer and co-working environment will help instill a sense of human interaction whilst providing the flexibility for growth.





# **DEVELOPMENT STRATEGY**

# ACTIVATE EDGES

#### Existing Condition - Ground Floor Plan

With the Victoria Police currently accommodating the 637 Flinders St building, and with the inherent sensitivities surrounding building security, the existing ground floor condition is both insular in nature and closed off to the neighbouring Flinders / Siddeley Street. Primarily treating Siddeley St as a back of house corridor, the 102m frontage to the South is not currently in alignment with the 'pedestrian-focused' masterplan the Melbourne City Coucil (MCC) have proposed.

The aggressive Flinders St frontage is met with between 85m - 102m of 'inactive' car parking across 3 levels, ground floor - level 2.

The existing commercial entrance currently adds no street presence, nor an amenable engagement with the surrounding civic space.

The brutalist built form is part of a series of precinctual buildings purposeful for their time. Now the precinct is seeing rapid change, and the purpose of the building is evolving, the expectations of its place, interaction with its neighbours and presence to Flinders St are in question.

#### Proposed Condition - Ground Floor Plan

The intentions of the proposed ground floor configuration is to create an interactive sequence of spaces between retail, commercial foyer and the co-working environment. By removing the physical boundary's typical to most retail outlets, the proposed design looks to counteract the inherited spatial height constraints of the existing floor to floor spans.

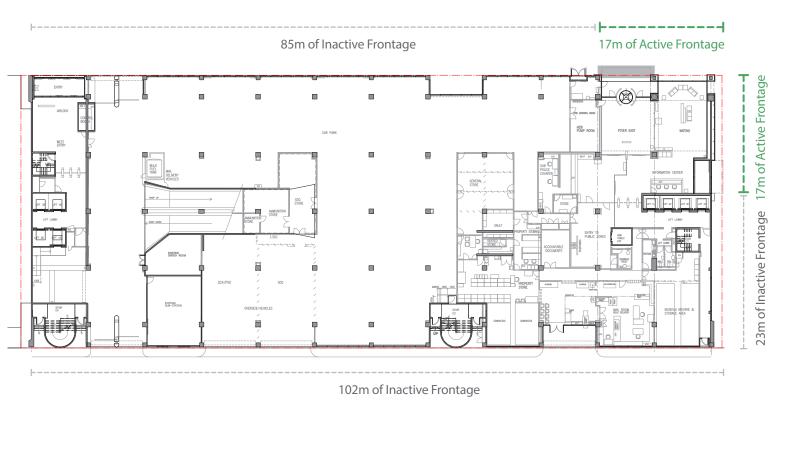
The extended co-working space encourages both a visual connection between Flinders St and Siddeley St, and provides a more 'active' frontage to the busy street-scape.

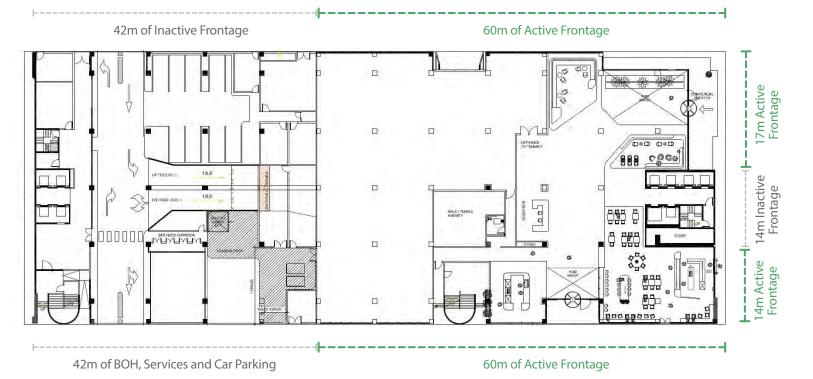
Furthermore, by minimising the impact of the

building services and loading into a consolidated zone, allows 60% of the Northern and Southern facades to be more engaging with their respective street frontages.

The commercial entry has been extended from the N/E Flinders St edge through to Siddeley St, allowing a more porous connection through the site and beyond.

The increased 'active' edges to the site are intending to engage more with the evolving surrounding developments, incentivising a more considered civic space - one which is responsive to its sense of place.





### URBAN INTEGRATION

Activated Edges - Co-Working + EOT Level 01 - Amenity

The intervention of the End-of-Trip and potential for co-working space creates a more balanced floor plan configuration and subsequent building elevation. Having the opportunity to improve the visual connection with the Flinders Street and Siddeley Street frontages uplifts the existing condition, and provides further incentive to align any future works in the area with the MCC masterplan - to pedestrianise, activate and connect the surrounding urban realm (as identified in the urban context analysis section of the report).







FLINDERS ST

# 03/3 DEVELOPMENT STRATEGY

# ACTIVATE EDGES

#### Existing Condition - Level 01 Floor Plan

The level 1 floor plan is currently accommodated by car parking, back of house services and two double height voids; 1. Leading down to the loading and services bay to Siddeley St, and 2. An enclosed void connecting back to the commercial entry foyer.

The existing floor plan is configured with two existing (and non-compliant) stair cases facilitating the 'podium levels' - Ground Floor to Level 03 - to the South.

The car parking configuration is neither efficient no amenable from the Flinders / Siddeley Street frontages. A delapidated wire metal mesh spanning between the existing pre-cast openings forms the main elevation of this level.

Another challenging constraint for proposing any works to the level 01 floor plan, is the reduced head height clearances through the space spanning 2740mm from the top of structural slab level (SSL), to SSL of the level above, Level 02. With existing services (proposed to be replaced) the remaining head clearances make this a difficult space to accommodate, beyond retaining car parking.

The existing foor plate holds only 32 lineal metres of 'active' frontage in total.

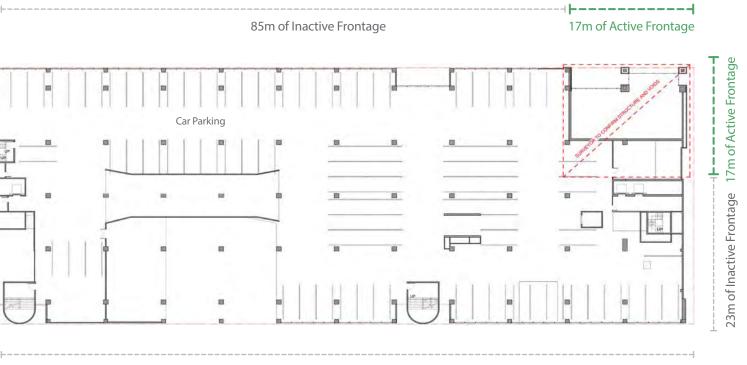
#### Proposed Condition - Level 01 Floor Plan

Given the inherited challenges surrounding the existing head height clearances, the proposed architectural solutions available are limited. However, the proposal for level 1 looks to incorporate a combination of End-of-Trip (EOT) facilities, a continuation of the co-working tenant, and utilising the existing car parking amenity.

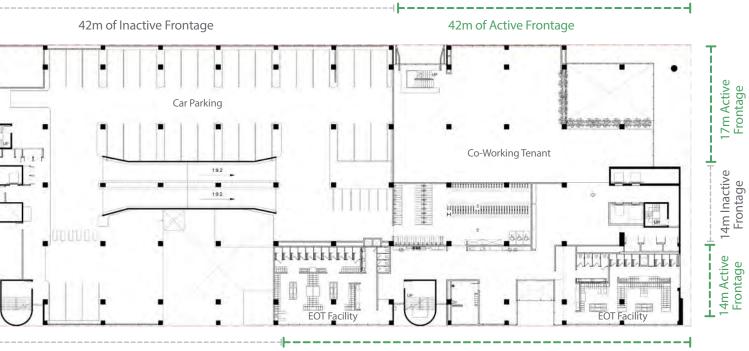
The difficult floor plan configurations are such that the arrangement of the affore-mentioned amenites become inefficient. Yet principally the narrative of the proposed works are positive, with an increase in the 'active' frontage from around 32 lineal metres, to around half of the usable building perimeter, 133m. The End-of-Trip is proposed to enagage with the Siddeley Street frontage, with a frosted glass elevation providing a diffused natural daylight. The elongated plan configuration also allows for an EOT lounge to separate the changing facilities, looking out onto the newly pedestrianised and more activated Siddeley Street.

The continued co-working hub seeks to open out the void leading to the ground floor foyer (currently enclosed), creating a visual connection between the two spaces. This move utlises and accentuates the double height space, in turn allowing more natural daylight to reach deep into the floor plate.

### Co-Working







42m of BOH, Services and Car Parking

60m of Active Frontage

# 03/3 DEVELOPMENT STRATEGY

# ACTIVATE EDGES

#### Typical Commercial Levels

The existing built form of 637 Flinders St tapers away from the street edge, both as you step up, elevating through the building (creating the terraces), and internally into the floor plate. This may have assisted orignially in bringing more natural daylight into the deep campus-size floor plates, given the small window openings between the pre-cast panelled facade. However, in the evolving commercial landscape the existing condition reads as both recessive to the Flinders St frontage and does not utilise the spatial capacity the building has to offer.

The development strategy seeks to further engage and re-connect with the historical Flinders Street and evolving surrounding landscape, by extending the commercial floor plate to the site boundary edge. Creating a narrative whereby the old and the new architectural forms 'talk' with one another, the intention is to celebrate the existing terraces and the delineation between the two forms.

The extended typical commercial floor plates are designed to engage with the surrounding terraces to provide access to green and open space with the opportunity to 'bring the outside in', and as a result maximise the commercial leasing. With full floor to floor glazing, and A-Grade ESD standard compliance, the proposed floor plates also enables a much greater allowance of natural daylight.

The extended / tinted commercial form offers the opportunity to 'reveal' an interstitial space between the old and new forms. This rebated space allows for a more transparent glazing type, exposing the activity of the people beyond and further providing a visual and physical connection to the re-purposed external terraces.





Existing typical commercial floor plate.

#### COX Urban Context Report



Siddeley St

Proposed typical commercial floor plate.

# URBAN CONNECTIONS

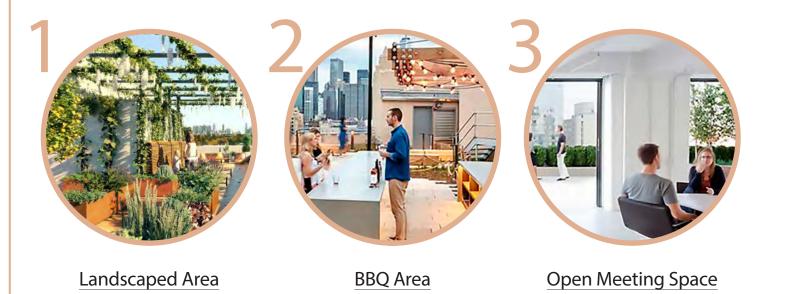
Interconnected Co-working space & Connection to the wider city.

The extended level 9 terrace is the perfect platform for tenants and co-working groups alike to connect and collaborate in a shared environment. Offering bookable '3rd Spaces', private meeting rooms, and open plan collaboration areas, the rooftop connects the outdoor entertaining and event space with the indoor floor plate.

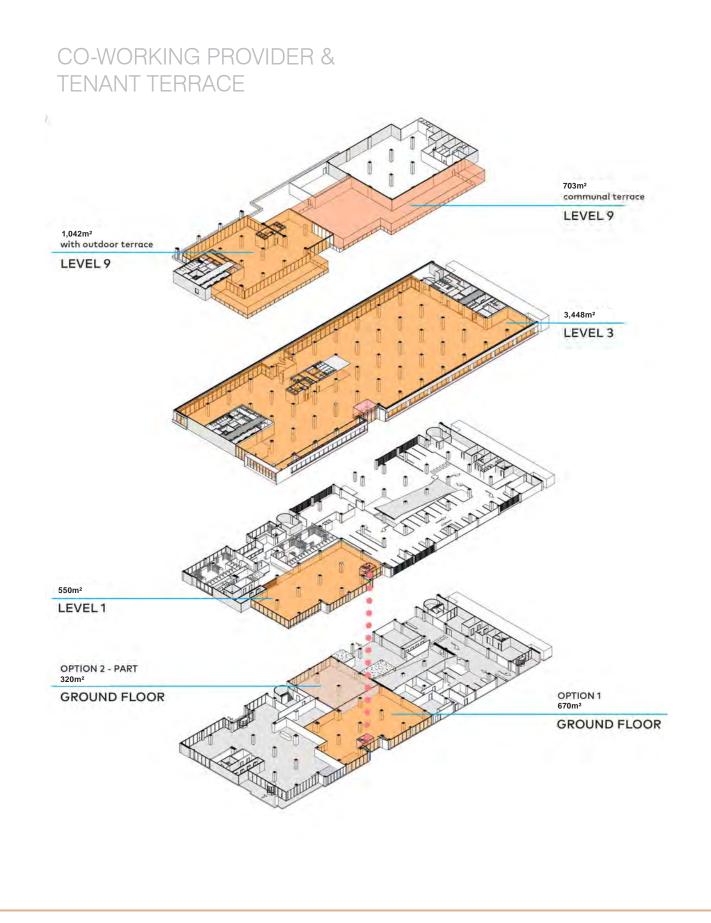
Similarly Level 9 opens an avenue for either a co-working provider or anchor tenant to embody the space as their front of house. Presenting this open, green and flexible working environment as a 'first impression' for visiting business.

A nod to Melbourne's renowned urban rooftop bars and terraces, this unique 'campus' sized floor area continues the citywide narrative and visual connection of landscaped rooftop terraces.

### CO-WORKING / TENANT TERRACE







Artist Impression: Level 09 External Terrace





# 03**DEVELOPMENT STRATEGY**

# ACTIVATE EDGES

Level 09 Floor Plate & Terrace Existing Condition

The subject site lies in a unique pocket of land known as the Seafarer's Precinct; enclosed between Flinders St to the North and the Yarra River to the South. As a result of its location, the precinct, and more specifically 637 Flinders St, is home to hundreds of nested and protected seagulls. The issue as a result of this, coinciding with the fact the external spaces are currently not accessible, is that the condition of the existing terraces have become uninhabitable. This is partly due to the dilapidated condition of the existing terraces, and in part because of the aggressive nature and faecal mess created by the birds themselves. It has become quite a distressing situation for the current building tenants (Victoria Police) and will remain a risk for the construction team during any works to be undertaken on site.

Principally what this means, is that the Level 09 roof-scape to the 637 Flinders St building is both derelict and in danger of further disrepair if left unattended. This also results in an under-utilised asset commercially, and one with real potential to engage with Melbourne's valued and evolving rooftop cultutre.

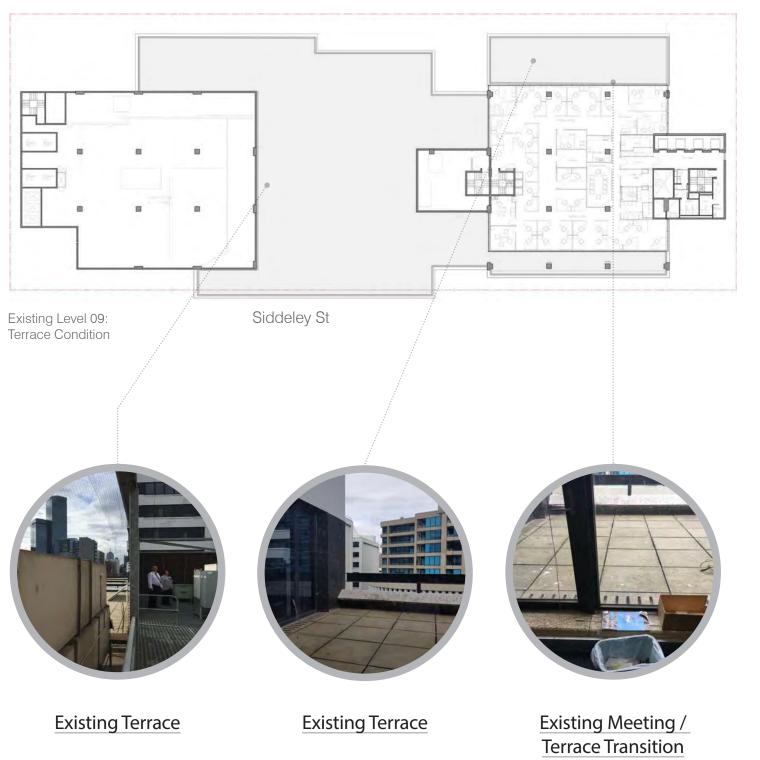


The proposition for Level 09 is to create a series of indoor / outdoor spaces which seeks to accommodate differing tenant and co-working groups. Given the extensive size of the floor plate and hence the opportunities of use available, allowing for a flexible program of the space (both during the day and at night time) encourages continued activity and diversity.

This is important on a number of fronts; firstly to mitigate the ongoing and inherent issue with the seagulls on site. An active, habitable space will discourage further nesting and dilapidation of the terrace moving forward. Secondly, the programmatic diversity will ensure the area is

both future-proofed for ongoing tenants and resonates with the true meaning of a collaborative, '3rd working space' environment. One premised on the serendipity and engagement of different working groups.

The opportunity to landscape, activate and accommodate the rooftop terrace further engages the project with the evolving surrounding developments, and Melbourne's CBD skyline.



Flinders St

### 03/ **′**4 DESIGN PROPOSAL

# URBAN INTEGRATION

Existing Built Form:

637 Flinders St is a true representation of the building typology of the time. Dedicated to a different purpose and commercial outcome, the building was a late response to the brutalist architectural period of the 1950's - 1960's. Fast foward to the present day, and the market expectations for a sustainable, efficient and visually engaging commercial building means the re-furbishment of 637 Flinders St is essential for the longevity and future of the existing form.

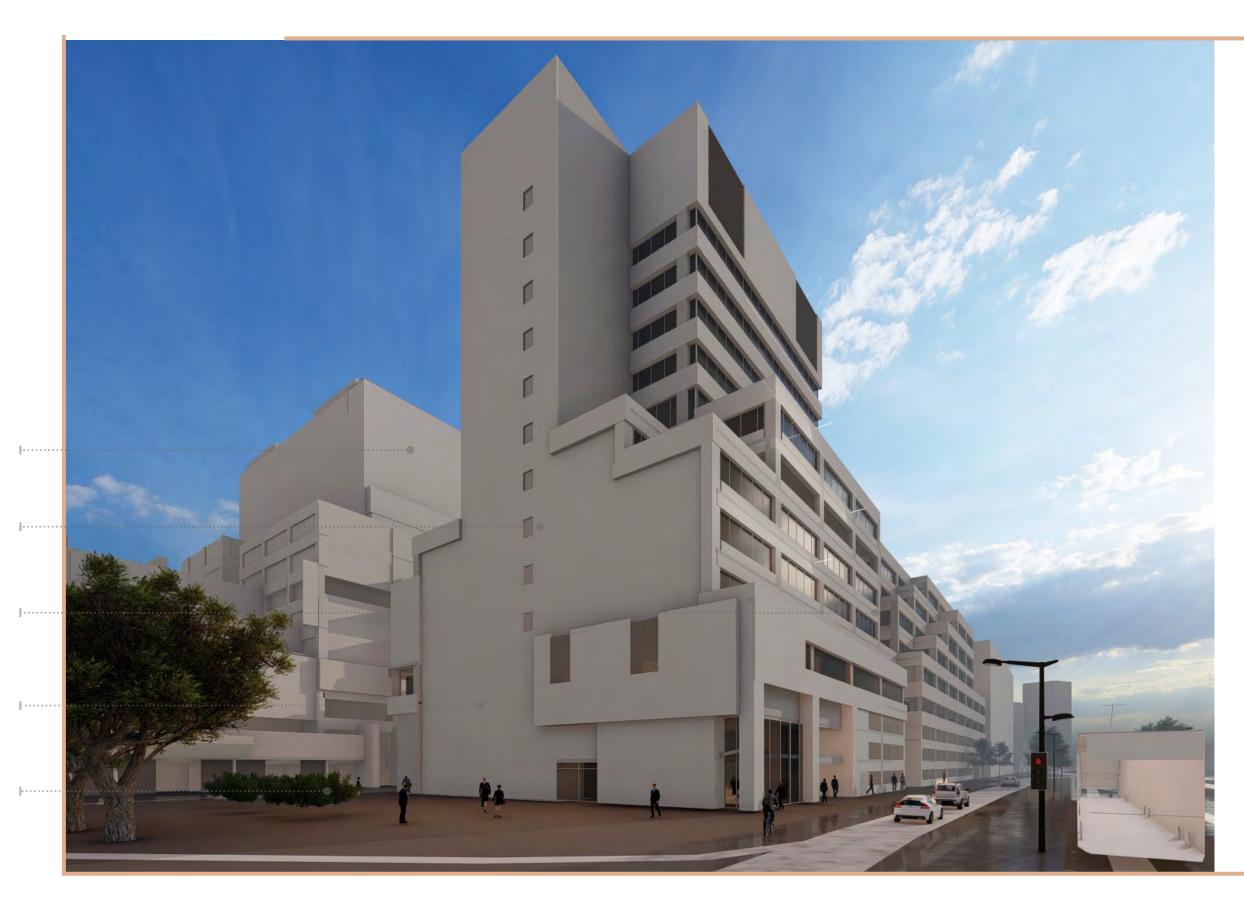
### Existing Building Characteristics:



Existing terraces locked off to building tenants - the terraces being one of the most defining features of existing building.

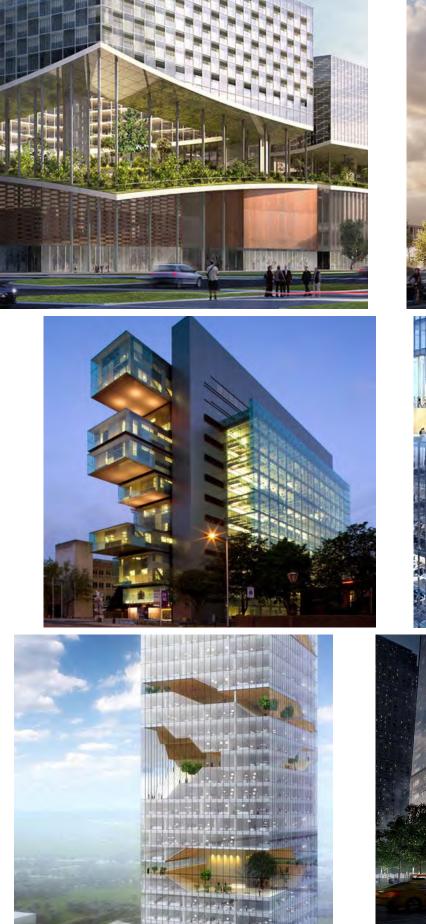
'Internalised' pre-cast concrete frontage to main commercial entry results in poor urban and civic integration. 4

Existing Siddeley Street interface is predominantly dedicated to back of house 5 services & car parking.



# DESIGN CONCEPT

3 Key Ideas



# I. \_\_\_\_

#### 2. EXTENSION

# 3.

- Give prominence to existing building terraces.

03/DESIGN PROPOSAL

# URBAN INTEGRATION

#### Proposed Built Form:

The upgraded 637 Flinders St seeks to find a balance between the existing architectural language and the new intervention, and in doing so create a new urban identity for the building within the perceived 'World Trade Centre' precinct. In maintaining and re-furbishing the existing form, the intention is to accentuate and enjoy the most prominent visual feature of the building, the external terraces. Creating a dialect between the old and new forms in turn focuses our attention to the newly accommodated external spaces.

### Proposed Building Characteristics:

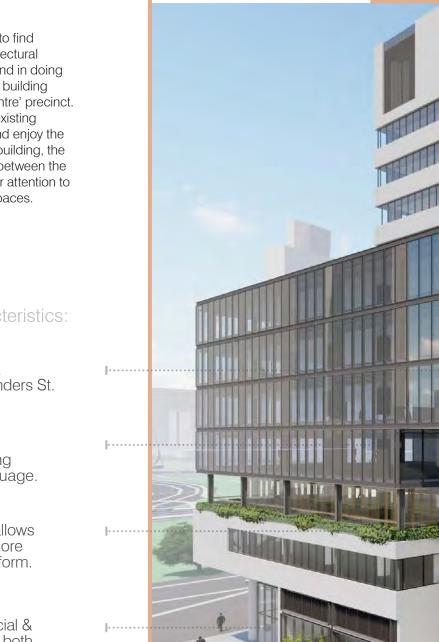
Strong Urban Identity & presence to historic Flinders St.

2 Integrated Old & New architecture re-enforcing prominent terrace language.

The proposed facade allows for an integrated and more engaging architectural form.

New activated commercial & co-working frontages to both Flinders St & the civic plaza. 4

New activated retail to the newly pedestrianised Siddeley St.



### INTERSTITIAL SPACE

- Bonding old & new architectural form - 'Stitching' the interior with exterior - Visual connection between terraces

- Clear Commercial Address - Urban Identity for Flinder's St - Definitive Northern Value - Historical Reference

# **RELEASE/ REVEAL**

- Accentuate transition between old & new - Sense of discovery
- Reveal the activity of the building beyond



#### Tower Design Strategy:

The tower facade typology is a response to the clear design gesture, which delineates the old architectural form from the new. The extruded modules interfacing with the Flinders St frontage intentionally focus on accentuating the horizontal datum lines inherent in the existing condition. This move helps to situate the refurbishment as an extension of the existing building, yet in turn create a clear and new urban identity for the development.

The tinted glass further emphasises this horizontality, stitching together the differing forms with a slender break in the mass.

Whilst the extrusions look to accentuate the form of the building, the recessed clear glazing typology seek to 'reveal' the warmth and activity of the people beyond. This interstitial space



03/4 DESIGN PROPOSAL

# INTEGRATED FACADE



(discussed further in the following page) allows the 'heavier' looking tinted form to elevate from the existing condition. Given the vast extent of the Northern facade (around 102m) this simple design gesture not only breaks down the overall mass of the building, but creates a dialect between the differing architectural era's - the brutalism of the 1970's to the contemporary commercial built language of the 2020's.

#### Terrace Design Strategy:

The tower design strategy is based on creating a clear delineation between the old and new forms. In creating this clear break, and as discussed in the previous segment, the recess not only allows the heavier formal (tinted) mass to appear as to 'float' above the existing building, but also reveals the activity of the interstitial space in between.

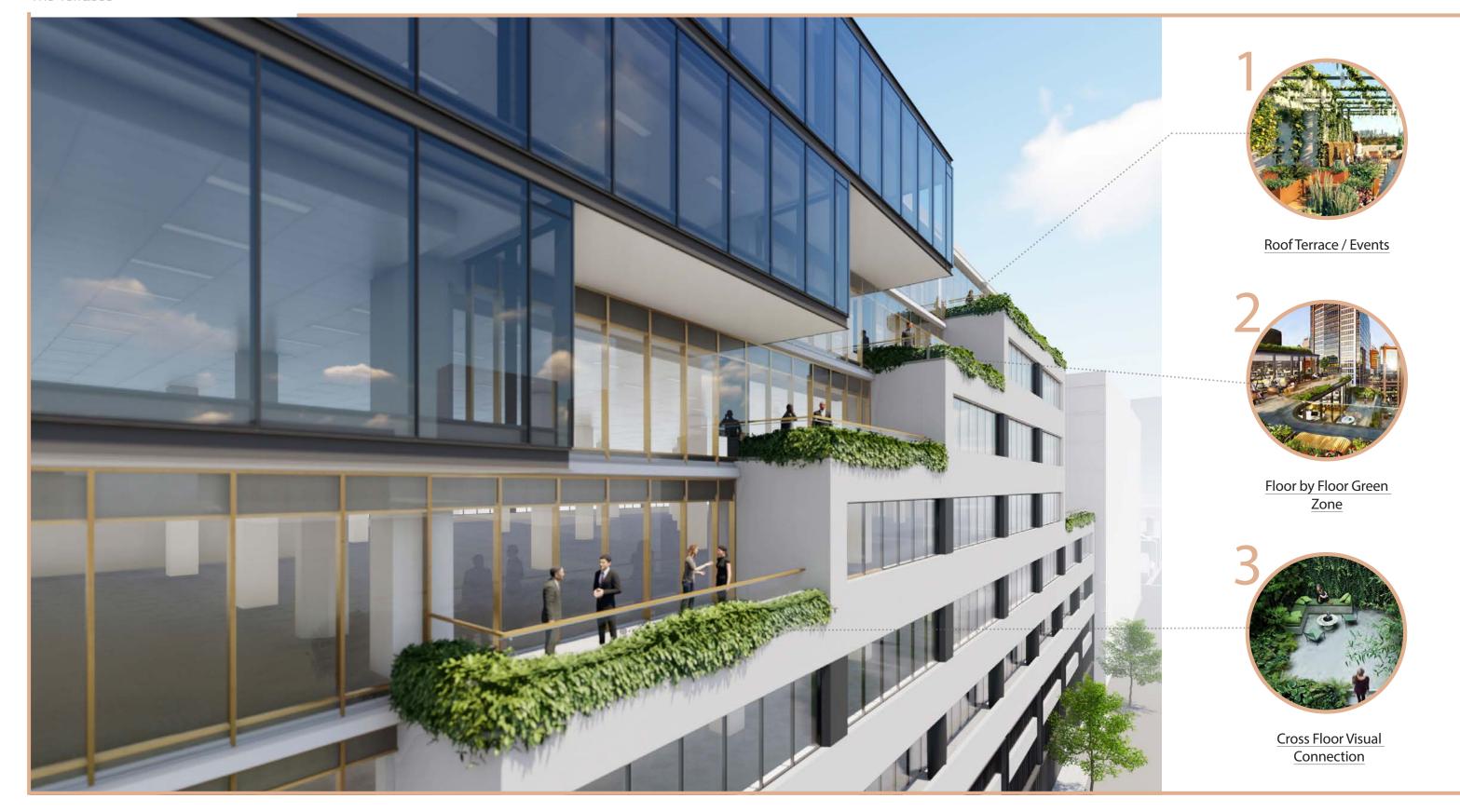
What this does in turn is create an emphasis on what is the most prominent feature of the existing building - the staggered external terraces.

In re-purposing the external terraces the proposal creates a migrating landscaping narrative along the northern facade and up to the roof-scape of the building. Tying the form together, the terraces visually connects the stacked floor plates, whilst also engaging the internal space with the previously closed off Flinders St frontage. This



03/4 DESIGN PROPOSAL

# INTEGRATED FACADE



new interaction with the recessed facade line encourages the tenants to connect with the surrounding city-scape, whilst forming a new urban identity for the previously perceived World Trade Centre Precinct.

#### The Podium Design Strategy:

The existing building podium (0-20m) currently abutts the site title boundary along Flinders St. This results in a challenging proposition for any remedial facade upgrades. The proposed design looks to counter this by incorporating a series of 'architectural features' along the Street frontage.

The embedded units are made up of steel angle surrounds, with cable wires running from top to bottom and creeper vines migrating their way up the form. This simple gesture helps to provide an amenity both externally along the streescape and internally enclosing the co-working space and commercial foyer. Providing a visual buffer to the aggressive existing conditions of Flinders Street, the interior space provides a connection to the outside world without feeling exposed. Similarly, the simple landscaped feature externally helps to provide a visual amenity along the streetscape





# INTEGRATED FACADE

'The Podium - Flinders St'



(for cars and pedestrians) whilst also breaking down the sheer mass of the 102m facade length. As a result this makes the various programmatic spaces internally appear more contiguous architecturally from the outside.

#### Commercial Entry Design Strategy:

The proposed commercial entry sequence is defined by the overhanging extruded pre-cast concrete module. Carving away the existing north-Eastern corner, currently a heavy-set / internalised space, intends to create a more welcoming and inviting commercial entry transition. In doing so, this strengthens the civic connection between the 637 Flinders St building with the neighbouring (and newly pedestrianised) Siddeley Street Plaza - blurring the boundaries between the internal foyer space and the external civic activity.

The extension of the street level, steel plate cladding leading to the proposed retail outlets further highlight the intention of the MCC masterplan - which seeks to catalyse pedestrian activity along Siddeley St. This interaction between the main commercial entry along Flinders



03/4 DESIGN PROPOSAL

#### PODIUM INTERFACE Commercial Entry



Street and the currently regarded 'back of house' Siddeley Street, is fundamental to the continued growth and ultimate success of the building and greater precinct linkages. This small architectural gesture and programmatic move to embody the retail outlets along the Southern boundary will assist in creating the micro-connections through the site and beyond, as further described in the urban context analysis section of the report.

The framed portal to Level 3 is an opportunity to provide a 'sneak peak' into the newly developed build. With the flexibility to accommodate a coworking provider, or commercial tenant, level 3 has the ability to include interconnecting stairs both up (inheriting further floor plates), or down toward the co-working space on Level 01/ Ground Floor Level.

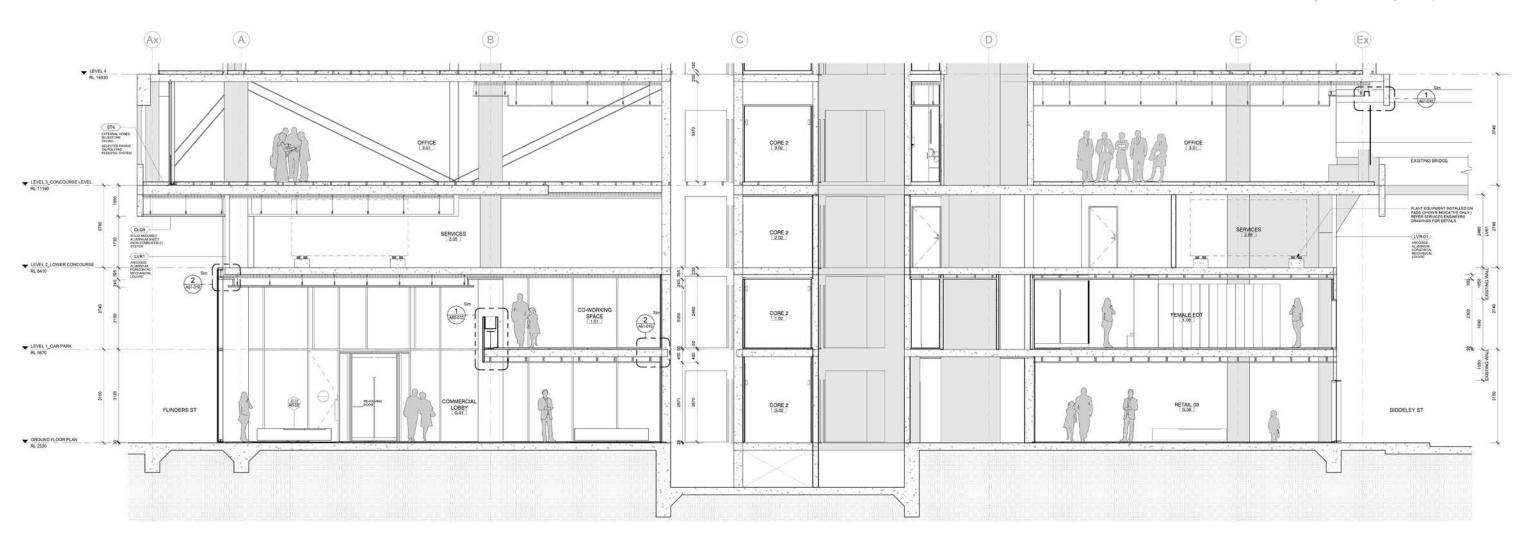
# 03/5 STREET INTERFACES

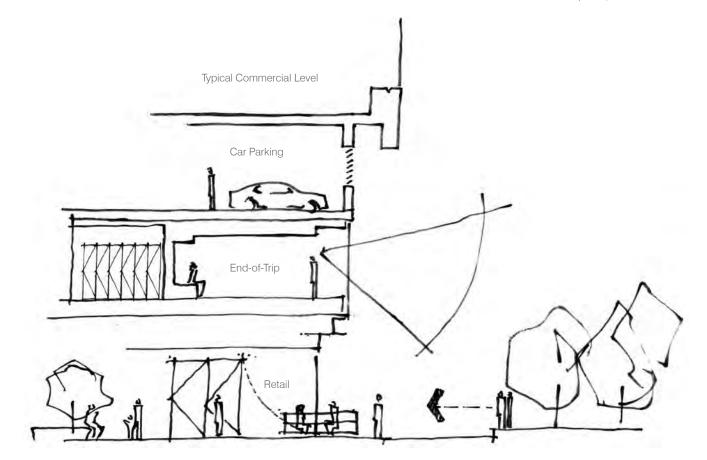
#### PODIUM INTERFACE Siddeley Street

The first 20 metres of the podium level is a key pedestrian interface consideration and planning criterion for the MCC. The proposed Siddeley Street frontage has set out to address the aforementioned urban design principals, relating to urban integration, Siddeley St activation and placemaking identity through the utility and design of the retail frontage and commercial entry.

Furthermore, the commercial frontage to Flinders St both adds a visual amenity, whilst diffusing the agressive nature of the traffic from internally.

This relief also provides an opportunity to migrate the landscaped greenery from the Siddeley Street masterplan works and neighbouring open urban areas into the commercial interior of the building.





Activated ground floor thoroughfare, creating linkages to Siddeley St and the newly developed retail outlets.



# SOLAR ANALYSIS\_DDO010

Overshadowing the river: Existing Condition

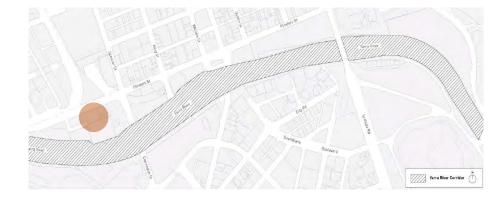
Testing has been undertaken to establish the shadow impact on the river implied by the existing site DDO controls (DDO010).

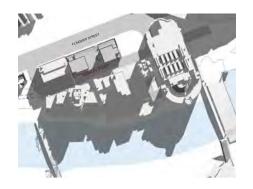
The controls specify the impact caused by built forms on The Yarra River corridor, including 15m from the edge of the North bank of the river to the South bank of the river.

Assessed Hours: 11.00am and 2.00pm - 22 June

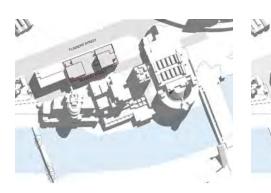




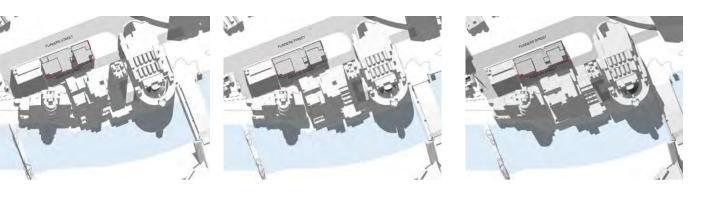


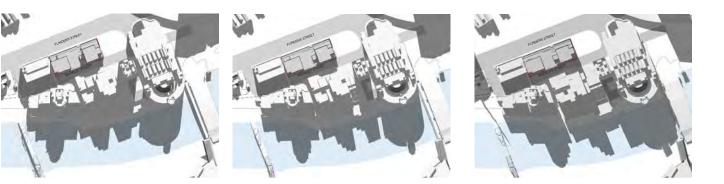


Sunlight for June Solstice - 11am - 2pm



Sunlight for September Solstice - 11am - 2pm









# SOLAR ANALYSIS\_DDO010

Overshadowing the river: Proposed Condition

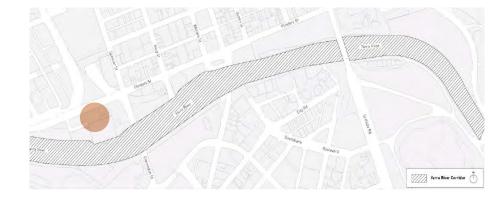
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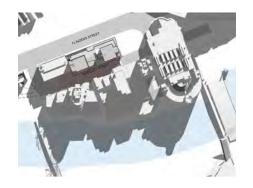
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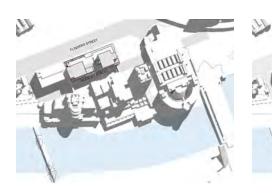




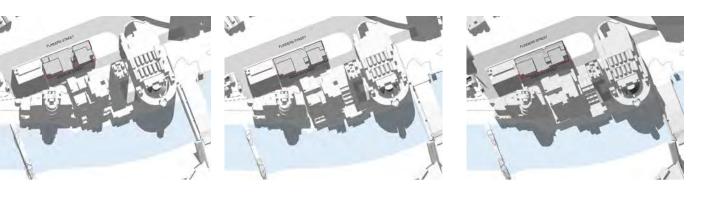


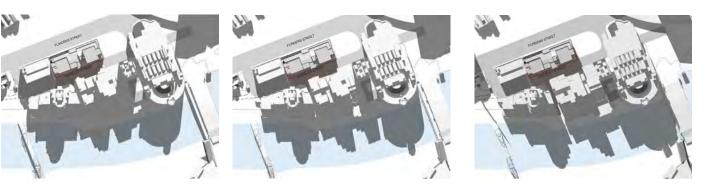


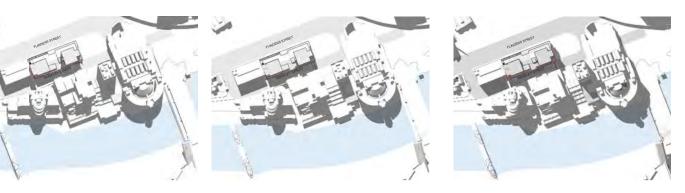
Sunlight for June Solstice - 11am - 2pm



Sunlight for September Solstice - 11am - 2pm







### URBAN CONTEXT REPORT: 637 FLINDERS ST SUPPORTING DOCUMENTS

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# SUPPORTING DOCUMENTS

# / COX

# 01 Introduction

- / 1 Architect's Statement
- / 2 Project Introduction
- / 3 Project Description
- / 4 Project Team
- / 5 The Site

# 02 Urban Context

- / 1 Site & Surrounds Transport, Amenity and Development
- / 2 Urban Context Analysis
- / 3 Urban Connection to River Front
- / 4 Existing Streetscapes & Interfaces
- / 5 Planning Controls

# 03 Design Response

- / 1 Urban Integration
- / 2 Proposed Development
- / 3 Development Strategy
- / 4 Design Response
- / 5 Street Interfaces
- / 6 Massing Study & Shadow Diagrams

# 04 Supporting Documents

- / 1 Integrated ESD Principles
- / 2 Facade Strategy
- / 3 Building Fabric Concept
- / 4 Materials & Finishes Palette
- / 5 Development Summary

# 05 Appendices

/ 1 Architectural Drawings

# 04/1 INTEGRATED ESD PRINCIPLES

### INTEGRATED ESD PRINCIPLES

Environmentally Sustainable Design is integral to this development. The following initiatives are targeted by the proposal to ensure that the project meets best practice Environmentally Sustainable Design principles appropriate to a Commercial Grade A development of an existing building refurbishment.

#### 5 Star NABERS

#### Indoor Environmental Quality

- High natural daylight penetration
- High Level of Thermal Comfort
- Low VOC finishes

#### Energy

- Greenhouse gas emissions (ghg) reduction eg use of efficient AC systems.
- Thermally efficient façades, with highly performative glazing systems
- Roof solar panel implementation.

#### Emissions

- No refrigerants or use of those with zero odp ozone depleting potential
- Stormwater, potential for reuse on landscape.
- Minimization of light pollution

#### Materials

- Timber products to be sustainably selected
- Best practice use of PVC

#### Land Use & Ecology

- Landscape enhancement of site Rain water catchment and treatment integrated into landscape
- Minimal demolition/ re-use of existing carparking

#### Water

- Reduction in potable water use via rain water harvesting
- Landscape irrigation via rain water harvesting
- High efficiency fixtures & fittings

#### Transport

- End Of Trip Facilities to meet NABERS rating
- Accessibility to public transport
- Excess bicycle parking facilities.
- Walking proximity to CBD, South Wharf and the Docklands



#### COX Urban Context Report

# 04/2 FACADE STRATEGY

# MATERIALS & FINISHES PALETTE

Typical Commercial Levels

The materials proposed for 637 Flinders St have been selected to compliment the commercial A-Grade standard, retail and urban realm tenancies typical of the Melbourne city capital zone, that will provide a sympathetic backdrop and bookend to the Seafarer's Precinct.

The public foyer and external ground plane will continue to use the bluestone consistent with the civic realm of the Melbourne cosmopolitan area.

The podium treatment will be simple in its material palette and will compliment the presence and visual connection between the inhabited co-working environment, and the retail program beyond.

The commercial tower glazing system has been selected to optimise the environmental impact of the site.

FA01

.....

FA02

ST01 / STO2 / ST03 / TB01

FA03 / SC01 / WT306





Tower

Podium

Foyer and Landscape

)1	Curtain Wall Facade	Silver/Grey/Blue
)2	Curtain Wall Facade	Silver/Clear
)1	Steel Columns	Chrome
)3	Structurally Glazed Facade	Clear
306	Blackened Steel Surround	Dark Charcoal
)1	Ground Level Internal	Bluestone Honed
)2	Ground Level Paving	Quartz Polished
)3	Wall Tile	Bluestone Honed
)1	Ground Level Landscape Seating	Aged Timber

# 04/3 FACADE STRATEGY

# BUILDING FABRIC CONCEPT

The tower facade has been designed to respond to the specific conditions of the site and to implement the 5 Star NABERS environnmental principles.

The highly transparent, yet performative portions of the tower curtain wall system have been utilised to maximise the exposure of the internal program - the dedicated co-working group, and the typical commercial office tenants, further enhancing the street-frontage and urban presence to Flinders Street.

The more tinted, predominantly North facing high performance glazed curtain wall system, assists with the direct impact of the morning glare and late afternoon heat. Optimisation analysis has been used to maximise the solar efficiencies of the existing site conditions and to provide the best ESD performance for the A-grade commercial tenancy.

### Design Strategy

- Type A Extruded Form
  Type B Recessed Form
  Type C Existing Form

### Facade Strategy

- Type A Accentuate the Form
  Darker Material Palette
  Formal Expression
  Reflective glass typology

- Type B Reveal the Texturality
   Warmer Materials
- Reveal activity beyond
  Clearer glass type

# Type C - Building Foundation Heavier Material

- Brutalist in Form
- Historical Reference



#### Urban Context Report COX



## 637 FLINDERS STREET AREA SCHEDULE

DATE UPDATED 1/11/2019 Rev 14

FLOOR TO FLOOR	RL	LEVEL (PROPOSED NAMING)	LEVEL (EXITSTING NAMING)
m	m	no.	name
	58.790	ROOF	
3.40	55.390	14 MEZ.	12
3.04	52.350	14	11 (PLANT )
3.71	48.640	13	10
3.75	44.890	12	9
3.74	41.150	11	8
3.74	37.410	10	7
3.76	33.650	9	6
3.73	29.920	8	5
3.73	26.190	7	4
3.76	22.430	6	3
3.75	18.680	5	2
3.75	14.930	4	1
3.74	11.190	3	Concourse
2.78	8.410	2	Lower Concourse
2.74	5.670	1	Carpark L2
3.15	2.520	G	Ground

	LIFT MR							
PLANT	LIFT OR	PLANT						
COMM.	CORE	COMM.						
COMM.	CORE	COMM.						
COMM.	CORE	COMM.			PLANT	LIFT MR	PLANT	
COMM.	CORE	COMM.				LIFT MR		
CO-WORKING	CORE	CO-WORKING	TERRACE		PLANT	LIFT OR	PLANT	
COMM.	CORE		СОММ.					
COMM.	CORE		COMM.					
COMM.	CORE		COMM.			CORE		
COMM.	CORE		COMM.			CORE		
COMM.	CORE		COMM.			CORE		
COMM.	CORE		COMM.			CORE		
PLANT	CORE		CARPARK					
EOT	CORE	CO-WORKING	CARPAR	<		CORE		
LOBBY	CORE	CO-WORKING	CARPAR	<		CORE	LOBBY	

	BUI

GFA / Floor Are



Cox Architecture Level 2, 167 Flinders Lane Melbourne 3000 VIC

EXISTING BUILT CONDITION									
				TAL REA					
			Ar						
EXISTING CARPARK	CORE/ CIRCULATION	EXISTING BALCONIES	EXISTING NLA	GFA	EFFICIENCY				
no.	sqm	sqm	sqm	sqm	%				
	60								
	142			825					
	142		674	825	82%				
	142		674	825	82%				
	142		674	825	82%				
	142		674	825	82%				
	143	274	580	3195	18%				
	335	199	2815	3148	89%				
	328	451	2981	3328	90%				
	328	122	3423	3785	90%				
	324	131	3548	3917	91%				
	324	102	3692	4058	91%				
	409		3363	3867	87%				
108	302			4060					
86	208			4060	0%				
38	320		829	4070	20%				

APPR	OVED (	CONCE	PT DE	SIGN
то	TAL AF			
TOTAL NLA (INTERNAL)	TOTAL NLA (EXTERNAL)	TOTAL NLA (COMBINED)	GFA	EFFICIENCY
sqm	sqm	sqm	sqm	%
0	0	0	005	0%
0	0	0	825	
0	0	0	825	0%
612	0	612	825	74%
612	0	612	825	74%
612	0	612	825	74%
612	0	612	939	65%
597	0	597	3772	16%
3122	321	3443	3887	80%
3203	359	3562	4048	79%
3403	266	3669	4179	81%
3578	127	3705	4201	85%
3591	90	3681	4238	85%
3372	0	3372	4206	80%
0	0	0	3908	0%
0	0	0	3895	0%
1707	0	1707	4041	42%

**25021** 1163 26184 **45439** 

									PR	OPO	SED D	ESIG	N							
					0 <b>.</b>								ADD			<b>T</b> 0-				
				EXI	STIN	G FL(	JOR	PLATE					-	AREA	4	101	AL A	REA		
CAR PARK	CARPARK + LOADING AREA+DRIVEWAY+RAMPS	CORE/ CIRCULATION	SERVICES (PLANT, BOH, STORE)	AREA CONTRIBUTING TO GFA_(WALLS, LEDGES)	ROOF/ VOID	COMMERCIAL LOBBY	AMENITIES	EOT & BIKE PARKING	NLA_ CO-WORKING	<b>GLAR_RETAIL</b>	NLA_ (INTERNAL)	NLA_(EXTERNAL BALCONIES)	NLA_ADDITIONAL (INTERNAL)	NLA_ADDITIONAL (EXTERNAL)	TOTAL ADDITIONAL NLA (COMBINED)	TOTAL NLA (INTERNAL)	TOTAL NLA (EXTERNAL)	TOTAL NLA (COMBINED)	GFA	EFFICIENCY
no.	sqm	sqm	sqm				sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	sqm	%
		200														0	0	0	200	0%
		200	633													0	0	0	856	0%
		223	000	19							614					614	0		856	72%
		223		10	627						614					614	0	614	856	72%
		318	757	19	130						614					614	0	614	1709	36%
		223		19	80						614					614	0	614	856	72%
		409	797	16	630				693			1008		169		693	1177	1870	1905	36%
		482		59							2689	91	511	46	557	3200	137	3337	3710	86%
		482		67							2831	208	490	0	490	3321	208	3529	3845	86%
		482		57							3294	74	230	0	230	3524	74	3598	4071	87%
		482		63							3418	81	164	0	164	3582	81	3663	4129	87%
		495		121							3540	71	19	0	19	3559	71	3630	4245	84%
		572		211							3280	0	139		139	3419	0	3419	4202	81%
85	2807	263	848	2												0	0	0	3927	
32	1594	371	122	21	436			843	522	0						522	0	522	3486	15%
4	709	281	638	80		740	204	0	627	619						1246	0	1246	3937	32%

TOTAL 263 3791 1279 23927 41613

BASE RL	2.5	m	
DING HEIGHT	58.8	m	
Site Area	4,209	m²	
Above Ground	42,790	m²	
ea Ratio (FAR)	10.17		

## \* PROPOSED SCHEME ASSUMPTIONS

Co-working & Retail included in Total NLA and GFA Calculations L9 Accessible Terrace Area reflected in Roof / Void Column

NLA External Balconies include Northern and Southern Terrace GFA is taken as the total floor area of the building, measured from the outside of external walls or the centre of party walls, and includes all roofed areas, as defined in http://planningschemes.dpcd.vic.gov.au/schemes/melbourne

121 5110 5729 3795 773 1903 740 204 843 <mark>1842 619</mark> 21508 1533 1553 215 1599

RETAIL AREA	GL	L1
RETAIL 01	322	
RETAIL 02	64	
RETAIL 03	233	1
	619	0
TOTAL AREA	(	619

PARKING SUMMARY	
CARPARK	121
MOTOR BIKE PARK	5
BIKE PARK	202
LOCKERS	192

**BUSINESS HUB/ CO-WORKING** GL L1 L9 NLA (INTERNAL) 627 522 693 NLA (EXTERNAL) 638 627 522 1331 TOTAL AREA (INTERNAL) 1842 TOTAL AREA (INTERNAL + EXTERNAL) 2480 ANCHOR TENANT TERRACE 539

**25522** 1748 27270 **42790** 

INACCESSIBLE TERRACE AREA L9 630

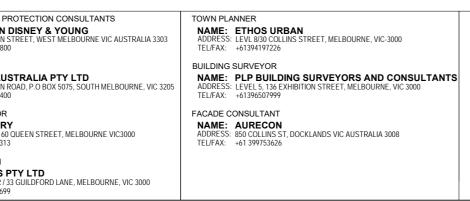
## 637 FLINDERS ST, MELBOURNE FOR TENDER - 70% DESIGN DEVELOPMENT

PROJECT: 318034.00

	DRAWING LIST	
SHEET 00 Cover S	Rev SHEET NAME	Date
A01-000	2 COVER SHEET - DRAWING INDEX	01/11/19
11 Site Pla A11-000	ns 2 SITE PLAN	01/11/19
14 Existing A14-001	3 EXISTING PLAN - GROUND FLOOR	01/11/19
A14-010 A14-020 A14-030	<ul> <li>3 EXISTING PLAN - LEVEL 1</li> <li>3 EXISTING PLAN - LEVEL 2</li> <li>3 EXISTING PLAN - LEVEL 3</li> </ul>	01/11/19 01/11/19 01/11/19
A14-040 A14-050	<ul> <li>3 EXISTING PLAN - LEVEL 4</li> <li>3 EXISTING PLAN - LEVEL 5</li> <li>2 EXISTING PLAN - LEVEL 6</li> </ul>	01/11/19 01/11/19
A14-060 A14-070 A14-080	<ul> <li>3 EXISTING PLAN - LEVEL 6</li> <li>3 EXISTING PLAN - LEVEL 7</li> <li>3 EXISTING PLAN - LEVEL 8</li> </ul>	01/11/19 01/11/19 01/11/19
A14-090 A14-100	<ul> <li>3 EXISTING PLAN - LEVEL 9</li> <li>3 EXISTING PLAN - LEVEL 10</li> <li>2 EXISTING PLAN - LEVEL 14</li> </ul>	01/11/19 01/11/19
A14-110 A14-120 A14-130	<ul> <li>3 EXISTING PLAN - LEVEL 11</li> <li>3 EXISTING PLAN - LEVEL 12</li> <li>3 EXISTING PLAN - LEVEL 13</li> </ul>	01/11/19 01/11/19 01/11/19
A14-140 A14-150	<ul> <li>3 EXISTING PLAN - LEVEL 14</li> <li>3 EXISTING PLAN - ROOF</li> </ul>	01/11/19 01/11/19
A15-001	ion Plans + Elevations 4 DEMOLITION PLAN - GROUND FLOOR	01/11/19
A15-010 A15-020 A15-030	<ul> <li>4 DEMOLITION PLAN - LEVEL 01</li> <li>4 DEMOLITION PLAN - LEVEL 02</li> <li>4 DEMOLITION PLAN - LEVEL 03</li> </ul>	01/11/19 01/11/19 01/11/19
A15-040 A15-050	<ul><li>4 DEMOLITION PLAN - LEVEL 04</li><li>4 DEMOLITION PLAN - LEVEL 05</li></ul>	01/11/19 01/11/19
A15-060 A15-070 A15-080	<ul> <li>4 DEMOLITION PLAN - LEVEL 06</li> <li>4 DEMOLITION PLAN - LEVEL 07</li> <li>4 DEMOLITION PLAN - LEVEL 08</li> </ul>	01/11/19 01/11/19 01/11/19
A15-090 A15-100	<ul> <li>4 DEMOLITION PLAN - LEVEL 09</li> <li>4 DEMOLITION PLAN - LEVEL 10</li> <li>4 DEMOLITION PLAN - LEVEL 10</li> </ul>	01/11/19 01/11/19
A15-110 A15-120 A15-130	<ul> <li>4 DEMOLITION PLAN - LEVEL 11</li> <li>4 DEMOLITION PLAN - LEVEL 12</li> <li>4 DEMOLITION PLAN - LEVEL 13</li> </ul>	01/11/19 01/11/19 01/11/19
A15-140 A15-150	<ul> <li>4 DEMOLITION PLAN - LEVEL 14</li> <li>4 DEMOLITION PLAN - ROOF LEVEL</li> <li>4 DEMOLITION ELEVATION - NOPTUELEN(ATION)</li> </ul>	01/11/19 01/11/19
A15-210 A15-220 A15-230	<ul> <li>4 DEMOLITION ELEVATION - NORTH ELEVATION</li> <li>4 DEMOLITION ELEVATION - SOUTH ELEVATION</li> <li>4 DEMOLITION ELEVATION - EAST &amp; WEST ELEVATIONS</li> </ul>	01/11/19 01/11/19 01/11/19
21 General A21-001	I Arrangement Floor Plans 8 GA PLAN - GROUND FLOOR	01/11/19
A21-010 A21-020	<ul><li>7 GA PLAN - LEVEL 1</li><li>7 GA PLAN - LEVEL 2</li></ul>	01/11/19 01/11/19
A21-030 A21-040 A21-050	<ul> <li>7 GA PLAN - LEVEL 3</li> <li>7 GA PLAN - LEVEL 4</li> <li>7 GA PLAN - LEVEL 5</li> </ul>	01/11/19 01/11/19 01/11/19
A21-030 A21-060 A21-070	7     GA PLAN - LEVEL 3       7     GA PLAN - LEVEL 6       7     GA PLAN - LEVEL 7	01/11/19 01/11/19
A21-080 A21-090 A21-100	<ul> <li>7 GA PLAN - LEVEL 8</li> <li>7 GA PLAN - LEVEL 9</li> <li>7 GA PLAN - LEVEL 10</li> </ul>	01/11/19 01/11/19
A21-100 A21-110 A21-120	<ul> <li>7 GA PLAN - LEVEL 10</li> <li>7 GA PLAN - LEVEL 11</li> <li>7 GA PLAN - LEVEL 12</li> </ul>	01/11/19 01/11/19 01/11/19
A21-130 A21-140	<ul> <li>7 GA PLAN - LEVEL 13</li> <li>7 GA PLAN - LEVEL 14</li> <li>7 A PLAN - LEVEL 14</li> </ul>	01/11/19 01/11/19
A21-145 A21-150	<ul> <li>7 GA PLAN - LEVEL 14 (MEZZ)</li> <li>7 GA PLAN - ROOF</li> </ul>	01/11/19 01/11/19
25 Partition A25-001 A25-010	n Plans           3         PARTITION PLAN - GROUND FLOOR           3         PARTITION PLAN - LEVEL 1	01/11/19 01/11/19
A25-010 A25-020 A25-030	<ul><li>3 PARTITION PLAN - LEVEL 2</li><li>3 PARTITION PLAN - LEVEL 3</li></ul>	01/11/19 01/11/19
A25-040 A25-050 A25-060	<ul> <li>3 PARTITION PLAN - LEVEL 4</li> <li>3 PARTITION PLAN - LEVEL 5</li> <li>3 PARTITION PLAN - LEVEL 6</li> </ul>	01/11/19 01/11/19 01/11/19
A25-000 A25-070 A25-080	<ul> <li>3 PARTITION PLAN - LEVEL 0</li> <li>3 PARTITION PLAN - LEVEL 7</li> <li>3 PARTITION PLAN - LEVEL 8</li> </ul>	01/11/19 01/11/19
A25-090 A25-100 A25-110	<ul> <li>3 PARTITION PLAN - LEVEL 9</li> <li>3 PARTITION PLAN - LEVEL 10</li> <li>3 PARTITION PLAN - LEVEL 11</li> </ul>	01/11/19 01/11/19 01/11/19
A25-120 A25-130	<ul><li>3 PARTITION PLAN - LEVEL 12</li><li>3 PARTITION PLAN - LEVEL 13</li></ul>	01/11/19 01/11/19
A25-140 A25-145	<ul> <li>3 PARTITION PLAN - LEVEL 14</li> <li>3 PARTITION PLAN - LEVEL 14 (Mezz.)</li> </ul>	01/11/19 01/11/19
A26-001	ed Ceiling Plans       3     RCP - GROUND FLOOR	01/11/19
A26-010 A26-020 A26-030	3         RCP - LEVEL 1           3         RCP - LEVEL 2           3         RCP - LEVEL 3	01/11/19 01/11/19 01/11/19
A26-040 A26-050	3         RCP - LEVEL 4           3         RCP - LEVEL 5           2         RCP - LEVEL 6	01/11/19 01/11/19
A26-060 A26-070 A26-080	3         RCP - LEVEL 6           3         RCP - LEVEL 7           3         RCP - LEVEL 8	01/11/19 01/11/19 01/11/19
A26-090 A26-100 A26-110	3         RCP - LEVEL 9           3         RCP - LEVEL 10           3         RCP - LEVEL 11	01/11/19 01/11/19 01/11/19
A26-120 A26-130	3         RCP - LEVEL 12           3         RCP - LEVEL 13	01/11/19 01/11/19
A26-140 A26-145	3         RCP - LEVEL 14           3         RCP - LEVEL 14 (Mezz.)	01/11/19 01/11/19
27 Finishes A27-001	3 FINISHES PLAN - GROUND FLOOR	01/11/19
A27-010 A27-020 A27-030	<ul> <li>3 FINISHES PLAN - LEVEL 1</li> <li>3 FINISHES PLAN - LEVEL 2</li> <li>3 FINISHES PLAN - LEVEL 3</li> </ul>	01/11/19 01/11/19 01/11/19
A27-040 A27-050	<ul> <li>3 FINISHES PLAN - LEVEL 4</li> <li>3 FINISHES PLAN - LEVEL 5</li> </ul>	01/11/19 01/11/19
A27-060 A27-070 A27-080	<ul> <li>3 FINISHES PLAN - LEVEL 6</li> <li>3 FINISHES PLAN - LEVEL 7</li> <li>3 FINISHES PLAN - LEVEL 8</li> </ul>	01/11/19 01/11/19 01/11/19
A27-090 A27-100 A27-110	<ul> <li>3 FINISHES PLAN - LEVEL 9</li> <li>3 FINISHES PLAN - LEVEL 10</li> <li>3 FINISHES PLAN - LEVEL 11</li> </ul>	01/11/19 01/11/19 01/11/19
A27-120 A27-130	<ul> <li>3 FINISHES PLAN - LEVEL 12</li> <li>3 FINISHES PLAN - LEVEL 13</li> </ul>	01/11/19 01/11/19
A27-140 A27-145	<ul> <li>3 FINISHES PLAN - LEVEL 14</li> <li>3 FINISHES PLAN - LEVEL 14 (Mezz.)</li> </ul>	01/11/19 01/11/19
28 FF&E P A28-001 A28-010	Plans 4 FF&E PLAN - GROUND FLOOR 3 FF&E PLAN - LEVEL 1	01/11/19 01/11/19
A29-001	Setout Plans 2 FAÇADE SETOUT PLANS - GROUND FLOOR 2 FAÇADE SETOUT PLANS - LEVEL 1	01/11/19
A29-010 A29-020 A29-030	<ul> <li>2 FAÇADE SETOUT PLANS - LEVEL 1</li> <li>2 FAÇADE SETOUT PLANS - LEVEL 2</li> <li>2 FAÇADE SETOUT PLANS - LEVEL 3</li> </ul>	01/11/19 01/11/19 01/11/19
A29-040 A29-050	<ul> <li>2 FAÇADE SETOUT PLANS - LEVEL 4</li> <li>2 FAÇADE SETOUT PLANS - LEVEL 5</li> </ul>	01/11/19 01/11/19
A29-060 A29-070 A29-080	<ol> <li>FAÇADE SETOUT PLANS - LEVEL 6</li> <li>FAÇADE SETOUT PLANS - LEVEL 7</li> <li>FAÇADE SETOUT PLANS - LEVEL 8</li> </ol>	01/11/19 01/11/19 01/11/19
A29-080 A29-090 A29-100	<ol> <li>FAÇADE SETOUT PLANS - LEVEL 9</li> <li>FAÇADE SETOUT PLANS - LEVEL 10</li> </ol>	01/11/19 01/11/19
A29-110	2 FAÇADE SETOUT PLANS - TOWER LEVELS (11-13)TYP.	01/11/19

SHEET	Rev		Date
30 Externa A30-010 A30-020	l Ele <sup>r</sup> 6 6	vations ELEVATIONS - NORTH ELEVATION ELEVATIONS - SOUTH ELEVATION	01/11/1
A30-030	6	ELEVATIONS - EAST & WEST ELEVATION	01/11/1
40 Section A40-010 A40-020	s 4 7	SECTIONS - CROSS SECTIONS N/S 1 SECTIONS - CROSS SECTIONS N/S 2	01/11/1
A40-020 A40-030 A40-040	7 7 1	SECTIONS - CROSS SECTIONS N/S 2 SECTIONS - CROSS SECTIONS N/S 3 SECTIONS - CROSS SECTIONS N/S 4	01/11/1
A40-040 A40-050 A40-060	6	SECTIONS - LONG SECTIONS E/W 1 SECTIONS - LONG SECTIONS E/W 2	01/11/1
A40-080 A40-080	6 5	SECTIONS - LONG SECTIONS E/W 2 SECTIONS - LONG SECTIONS E/W 3 SECTIONS - LONG SECTIONS E/W 4	01/11/1
45 Facade			04/44/4
A45-010 A45-020	3 3 3	FACADE SECTION DETAIL FACADE SECTION DETAIL FACADE SECTION DETAIL	01/11/1 01/11/1 01/11/1
A45-030 A45-040	3	FACADE SECTION DETAIL	01/11/1
A45-050 A45-060 A45-070	3 3 3	FACADE SECTION DETAIL FACADE SECTION DETAIL FACADE SECTION DETAIL	01/11/1 01/11/1 01/11/1
A45-080	3	FACADE SECTION DETAIL	01/11/1
51 Detail D A51-001	3	DETAIL PLANS - GROUND FLOOR LOBBY PLAN	01/11/1
A51-002 A51-003	3 3	DETAIL PLANS - GROUND FLOOR LOBBY RCP DETAIL ELEVATIONS - GROUND FLOOR INTERNAL ELEVATIONS	01/11/1
A51-004 A51-005	3 3	DETAIL ELEVATIONS - GROUND FLOOR INTERNAL ELEVATIONS DETAIL ELEVATIONS - GROUND FLOOR INTERNAL ELEVATIONS	01/11/1 01/11/1
A51-010 A51-011	3 3	DETAIL PLAN - LEVEL 1 EOT BREAK OUT PLAN DETAIL PLAN - LEVEL 1 EOT BREAK OUT RCP	01/11/1
A51-012 A51-020	3 2	DETAIL ELEVATIONS - LEVEL 1 EOT BREAK OUT ELEVATIONS DETAIL PLAN - LEVEL 1 EOT MALE AMENITIES	01/11/1
A51-021 A51-030 A51-040	2 3 1	DETAIL PLAN - LEVEL 1 EOT FEMALE AMENITIES DETAIL PLAN - TYPICAL LIFT LOBBY PLAN AND ELEVATIONS LEVEL 9 TERRACE PERGOLA	01/11/1 01/11/1 01/11/1
55 Core			
A55-001 A55-020	2	CORE 1 PLAN & RCP- GROUND FLOOR- LEVEL 2 CORE 1 PLAN & RCP - TYP. LEVELS (3-10)	01/11/1
A55-030 A55-100	2	CORE 1 PLAN & RCP - LEVEL 9 CORE 2 PLAN & RCP- GROUND FLOOR - LEVEL 2	01/11/1
A55-120 A55-140	2 2	CORE 2 PLAN & RCP - TYP. LEVELS (3-13) CORE 2 PLAN & RCP - LEVEL 14 - 14MEZZ.	01/11/1
A55-210 A55-220	2 2	STAIR 1 DETAILS STAIR 2 DETAILS	01/11/1
A55-230 A55-231	3 2	STAIR 3 DETAILS STAIR 3 DETAILS	01/11/1
A55-240 A55-250	2 2	STAIR 4 DETAILS STAIR 5 DETAILS	01/11/1
60 Floor Ju	unctio	on Details	
A60-010 A60-100	2 1	FLOOR JUNCTION DETAILS FLOOR JUNCTION DETAILS	01/11/1 01/11/1
61 Ceiling A61-010 A61-020	Deta 2 1	ils CEILING DETAILS - SHEET 1 CEILING DETAILS - SHEET 2	01/11/1
62 Facade	-		01/11/1
A62-001 A62-002	1	FACADE DETAILS_PODIUM FACADE DETAILS PODIUM	01/11/1
A62-003 A62-005	1	FACADE DETAILS_PODIUM FACADE DETAILS_PODIUM	01/11/1
A62-003 A62-100 A62-200	1 1	FACADE DETAILS_FODIOM FACADE DETAILS_TYPICAL FACADE DETAILS_MISC	01/11/1
63 Wall De	etails		
A63-001 A63-002	1	WALL LINING DETAILS DOOR JAMB DETAILS	01/11/1
A63-010 65 Joinery	1 Deta		01/11/1
A65-001 A65-010	1 2	TYPICAL JOINERY DETAILS JOINERY DETAILS - JG-01	01/11/1
A65-020 A65-021	3	JOINERY DETAILS - JG-02/JG-03 JOINERY DETAILS - JG-04/JG-05	01/11/1
A65-022	3	JOINERY DETAILS - JG-06/JG-07	01/11/1
A65-023 A65-024	3	JOINERY DETAILS - JG-08 JOINERY DETAILS - JG-09	01/11/1
A65-025 A65-030	3 3	JOINERY DETAILS - JG-10 JOINERY DETAILS - JG-11/ JG-12	01/11/1 01/11/1
A65-031 A65-032	3 3	JOINERY DETAILS - JG-13/JG-14/JG-15 JOINERY DETAILS -JG-16/JG-17/JG-18/ JG-19	01/11/1
A65-033 A65-034 A65-035	1 1 1	JOINERY DETAILS -JG-20/JG-24 JOINERY DETAILS - JG-21,JG-22,JG-23 JOINERY DETAILS - JG-25/JG-026	01/11/1 01/11/1 01/11/1
A65-035 67 Wet Are	•		UI/1//
A67-001 A67-002	3 3	WET AREA DETAILS - GF MALE AMENITIES WET AREA DETAILS - GF FEMALE AMENITIES	01/11/1
A01-002	3 3	WET AREA DETAILS -GF DDA AMENITIES WET AREA DETAILS - MALE EOT AMENITIES	01/11/1
A67-002 A67-010	3	WET AREA DETAILS - MALE LOT AMENITIES WET AREA DETAILS - FEMALE EOT AMENITIES WET AREA DETAILS - DDA EOT AMENITIES	01/11/1
A67-003 A67-010 A67-011		WET AREA DETAILS - MALE EOT SHOWERS AMENITIES	01/11/1
A67-003 A67-010 A67-011 A67-012 A67-013	2		ויט  /11/1
A67-003 A67-010 A67-011 A67-012 A67-013 A67-014 A67-015	2 2 1 1	WET AREA DETAILS - FEMALE EOT SHOWERS AMENITIES WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES PLAN WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES	
A67-003 A67-010 A67-011 A67-012 A67-013 A67-014 A67-015 A67-016 A67-017	2 1 1 1	WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES PLAN WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES ELEVATIONS WET AREA DETAILS - FEMALE EOT CHANGE AREA AMENITIES PLAN	01/11/1
A67-003 A67-010 A67-011 A67-012 A67-013 A67-014 A67-015 A67-016 A67-017 A67-018	2 1 1 1 1	WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES PLAN WET AREA DETAILS - MALE EOT CHANGE AREA AMENITIES ELEVATIONS WET AREA DETAILS - FEMALE EOT CHANGE AREA AMENITIES PLAN WET AREA DETAILS - FEMALE EOT CHANGE AREA AMENITIES ELEVATIONS	01/11/1 01/11/1 01/11/1
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EVISION	DESCRIPTION	ВY	DATE	CONSULTANTS	
				PROJECT MANAGEMENT	HYDRAULICS & FIRE PROT
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ZONE Q INVESTMENTS PTY LTD

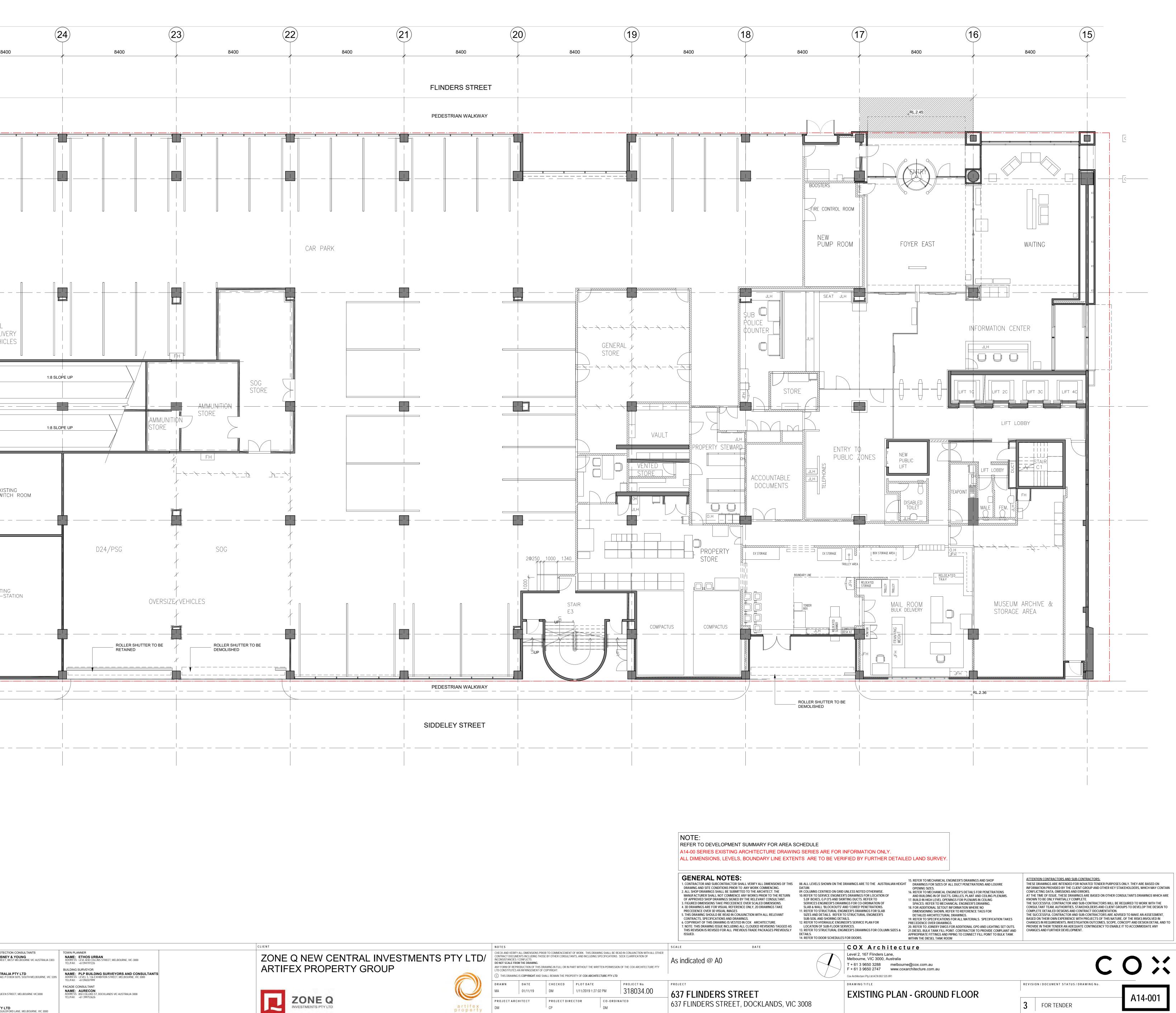
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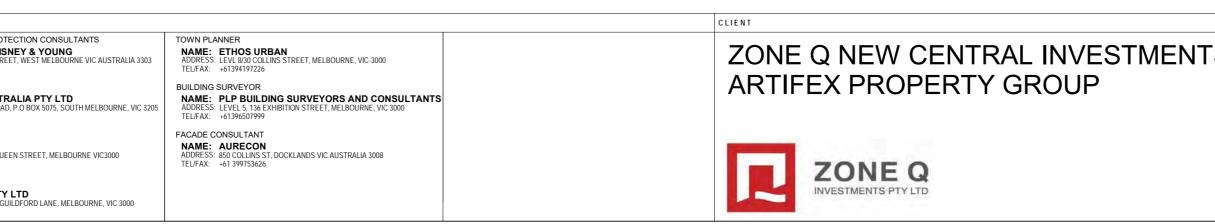


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STREET, MELBOURNE VIC3000	NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626
TD	

ARE TO THE AUSTRALIAN HEIGHT ITED OTHERWISE. GS FOR LOCATION OF TS. REFER TO CO-ORDINATION OF ' PENETRATIONS. AWINGS FOR SLAB JRAL ENGINEER'S ICE PLAN FOR AWINGS FOR COLUMN SIZES & S.	<ol> <li>REFER TO MECHANICAL ENGINEER'S DRAWINGS AND SHOP DRAWINGS FOR SIZES OF ALL DUCT PENETRATIONS AND LOUVRE OPENING SIZES.</li> <li>REFER TO MECHANICAL ENGINEER'S DETAILS FOR PENETRATIONS AND BUILDING IN OF DUCTS, GRILLES, PLANT AND CEILING PLENUMS.</li> <li>BUILD IN HIGH LEVEL OPENINGS FOR PLENUMS IN CEILING SPACES; REFER TO MECHANICAL ENGINEER'S DRAWING.</li> <li>FOR ADDITIONAL SETOUT INFORMATION WHERE NO DIMENSIONING SHOWN, REFER TO REFERENCE TAGS FOR DETAILED ARCHITECTURAL DRAWINGS.</li> <li>REFER TO SPECIFICATIONS FOR ALL MATERIALS. SPECIFICATION TAKES PRECEDENCE OVER DRAWINGS.</li> <li>REFER TO JOINERY DWGS FOR ADDITIONAL GPO AND LIGHTING SET OUTS.</li> <li>LDIESEL BULK TANK FILL POINT. CONTRACTOR TO PROVIDE COMPLIANT AND APPROPRIATE FITTINGS AND PIPING TO CONNECT FILL POINT TO BULK TANK WITHIN THE DIESEL TANK ROOM</li> </ol>	THESI INFOR CONF AT TH KNOW THE S CONS COMP THE S BASEI CHAN PROV	SE DF RMA FLIC <sup>-</sup> HE TI SUCC SULT PLET SUCC ED O NGES VIDE	ON CONTRACTORS AND SUB-CONTRACTOR RAWINGS ARE INTENDED FOR NOVATED TE TION PROVIDED BY THE CLIENT GROUP AN TING DATA, OMISSIONS AND ERRORS. IME OF ISSUE, THESE DRAWINGS ARE BASE O BE ONLY PARTIALLY COMPLETE. CESSFUL CONTRACTOR AND SUB-CONTRA ANT TEAM, AUTHORITIES, STAKEHOLDERS TE DETAILED DESIGNS AND CONTRACT DOC CESSFUL CONTRACTOR AND SUB-CONTRACT N THEIR OWN EXPERIENCE WITH PROJECTS SIN REQUIREMENTS, INVESTIGATION OUTC IN THEIR TENDER AN ADEQUATE CONTING S AND FURTHER DEVELOPMENT.	ED ON OTHER KEY STA ED ON OTHER KEY STA ED ON OTHER CON CTORS WILL BE RI AND CLIENT GRO CUMENTATION. CTORS ARE ADVIS S OF THIS NATURE OMES, SCOPE, CO	KEHOLDERS, WHICH SULTANTS DRAWING EQUIRED TO WORK W UPS TO DEVELOP THE ED TO MAKE AN ASSE , OF THE RISKS INVOI NCEPT AND DESIGN [	MAY CONTAIN S WHICH ARE ITH THE E DESIGN TO ESSMENT, VED IN DETAIL AND TO
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	NOTE:		
	REFER TO DEVELOPMENT SUMMARY FOR AREA S	CHEDULE	
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	ALL DIMENSIONS, LEVELS, BOUNDARY LINE EXTE	NTS ARE TO BE VER	IFIED BY
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	4. 3D DRAWINGS ARE FOR VISUAL REFERENCE ONLY, 2D DRAWINGS TAKE	SLAB & WALL 'BLOCKOUTS' A	
	PRECEDENCE OVER 3D VISUAL IMAGES	11. REFER TO STRUCTURAL ENG	INEER'S DRAV
	5. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT	SIZES AND DETAILS. REFER T	
	CONTRACTS, SPECIFICATIONS AND DRAWINGS. 6. COPYRIGHT OF THIS DRAWING IS VESTED IN COX ARCHITECTURE.	SUB-SOIL AND SHORING DETA 12. REFER TO HYDRAULIC ENGIN	
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		NAME: AURECONNAME: VERIS AUSTRALIAADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626ADDRESS: 16 EASTERN ROAD, P.O. TEL/FAX: +61 396991400MECHANICAL ENGINEERS ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 396626800QUANTITY SURVEYOR NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN S' TEL/FAX: +61 396021313ELECTRICAL ENGINEERS NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 

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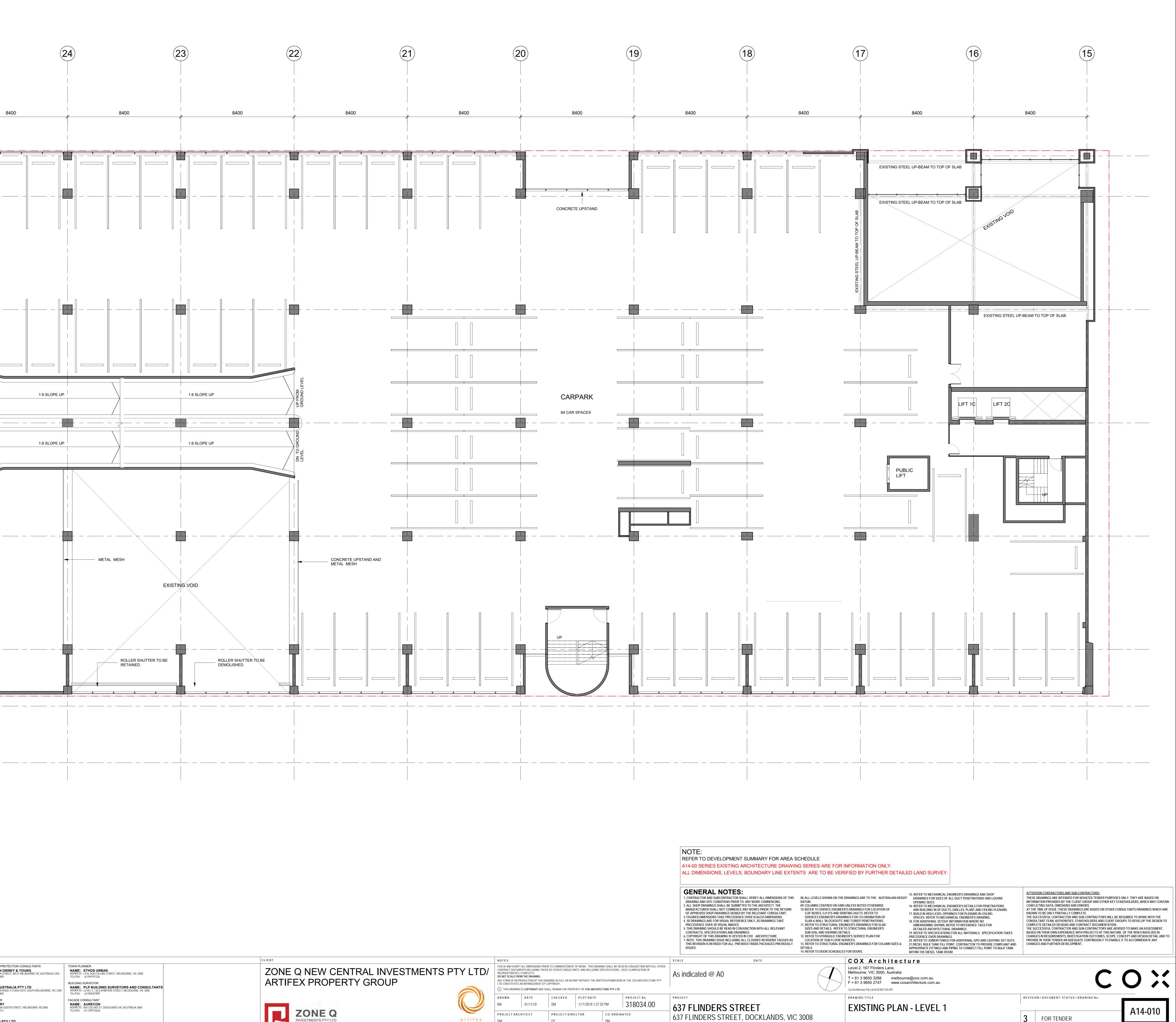
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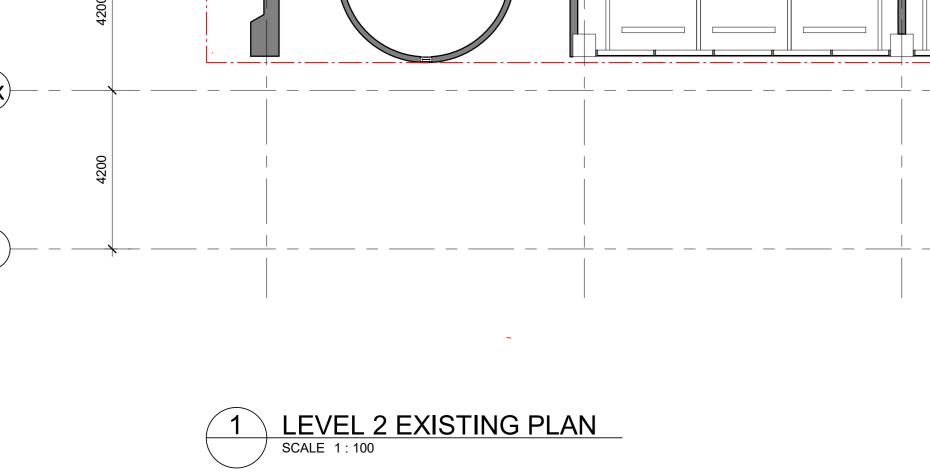


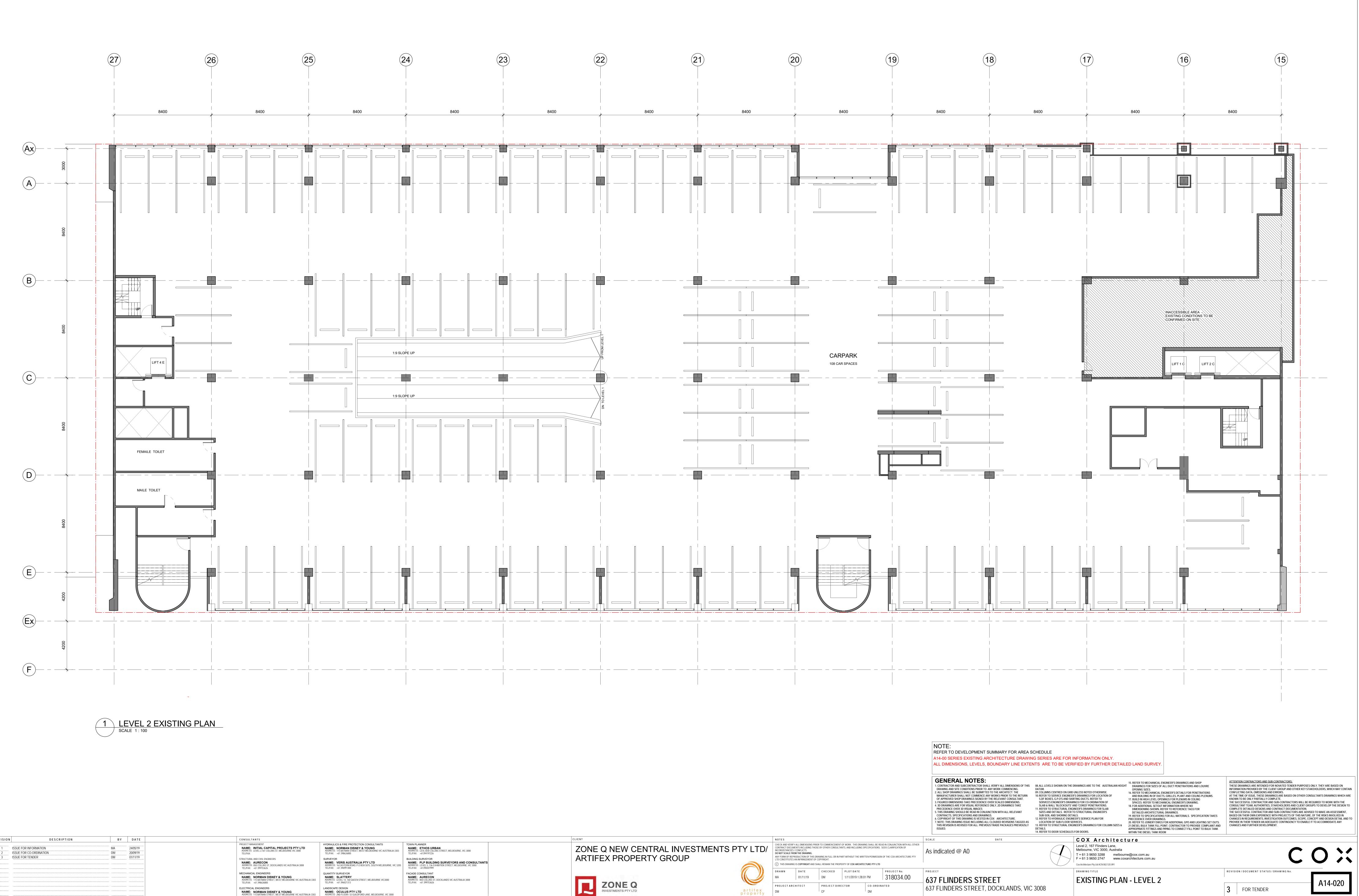
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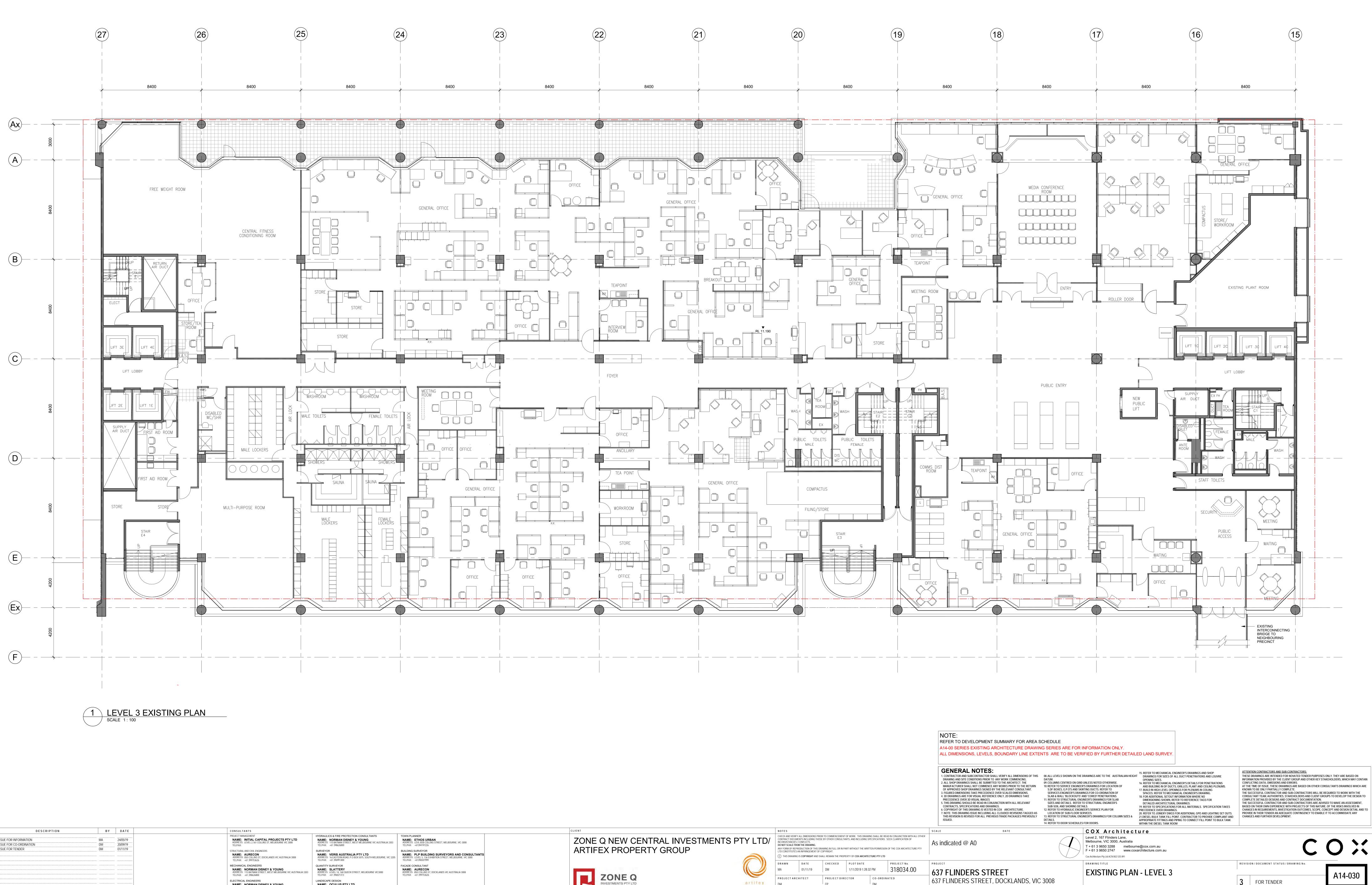
REVISION	DESCRIPTION	ΒY	DATE	CONSULTANTS	
				PROJECT MANAGEMENT	HYDRAULICS & FIRE PROTECTION CONSULTANTS
	ISSUE FOR INFORMATION	MA	24/05/19	NAME: INITIAL CAPITAL PROJECTS PTY LTD	NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303
2	ISSUE FOR CO-ORDINATION	DM	20/09/19	ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX:	ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800
3	ISSUE FOR TENDER	DM	01/11/19	STRUCTURAL AND CIVIL ENGINEERS	SURVEYOR
				NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	NAME: VERIS AUSTRALIA PTY LTD ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205 TEL/FAX: +61 396991400
				MECHANICAL ENGINEERS	QUANTITY SURVEYOR
				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000 TEL/FAX: +61 396021313
				ELECTRICAL ENGINEERS	LANDSCAPE DESIGN
				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699



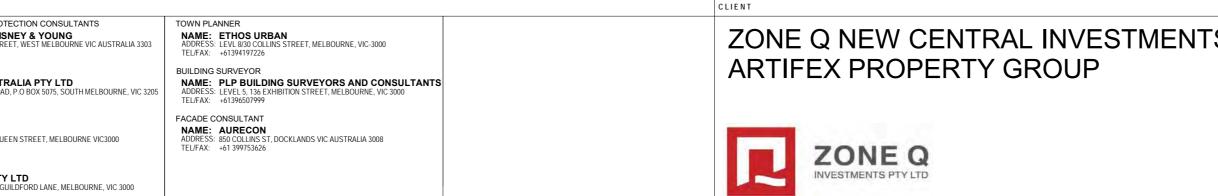




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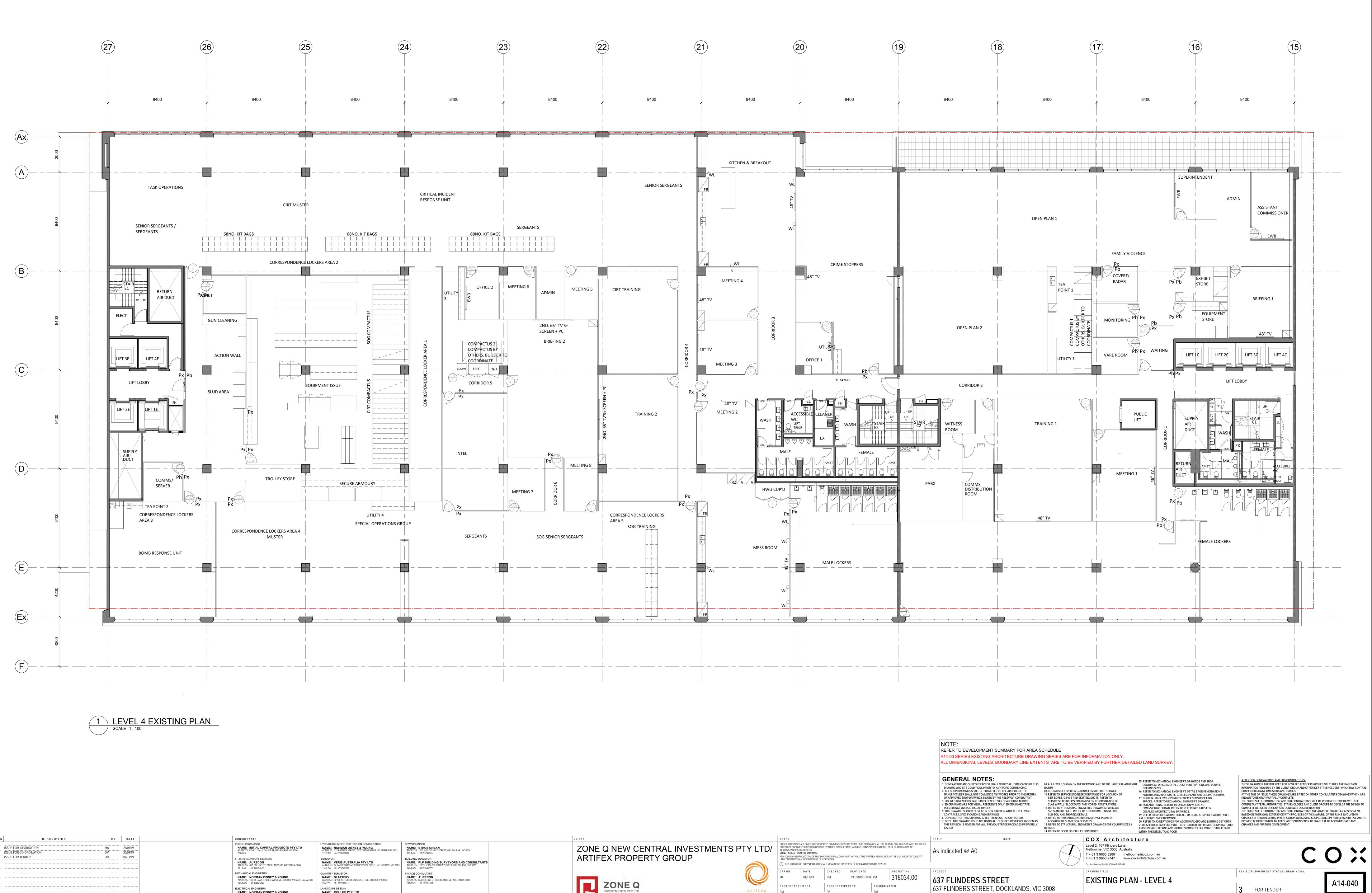
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				NAME: NORMAN DISNEY & YOUNG	NAME: OC
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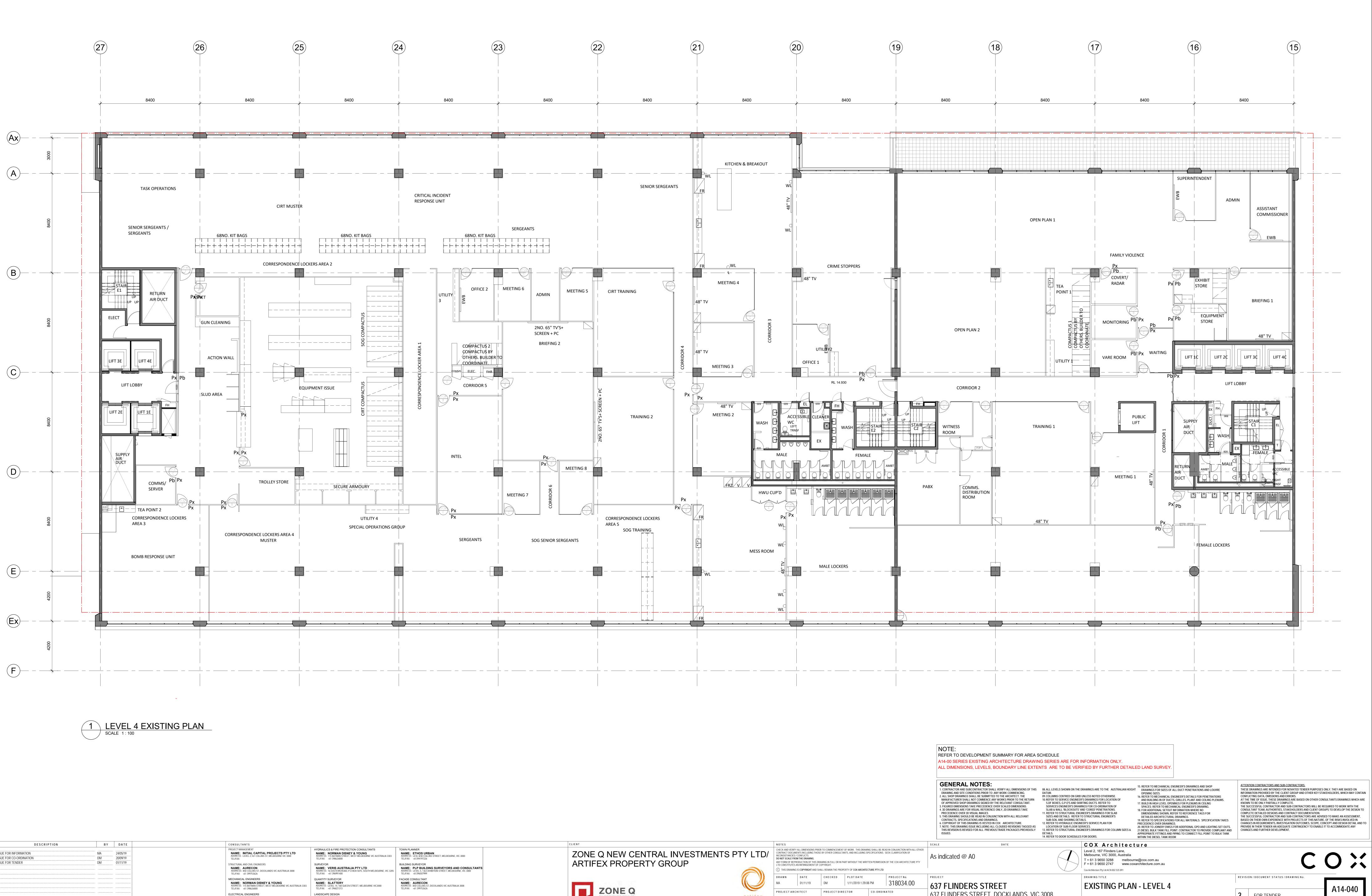


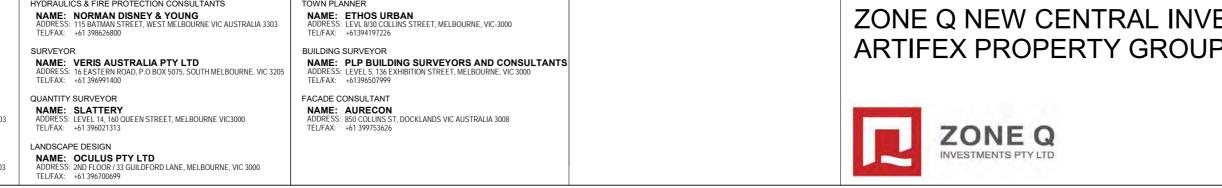
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1 2 3	ISSUE FOR INFORMATION ISSUE FOR CO-ORDINATION ISSUE FOR TENDER	MA DM DM	24/05/19 20/09/19 01/11/19	PROJECT MANAGEMENT NAME: INITIAL CAPITAL PROJECTS PTY LTD ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX: STRUCTURAL AND CIVIL ENGINEERS NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	HYDRAULICS & FIRE PROTEC NAME: NORMAN DISNI ADDRESS: 115 BATMAN STREET TEL/FAX: +61 398626800 SURVEYOR NAME: VERIS AUSTRA ADDRESS: 16 EASTERN ROAD, F TEL/FAX: +61 396991400
	· · · · · · · · · · · · · · · · · · ·			MECHANICAL ENGINEERS NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800 ELECTRICAL ENGINEERS	QUANTITY SURVEYOR NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN TEL/FAX: +61 396021313 LANDSCAPE DESIGN
				ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	NAME: OCULUS PTY L ADDRESS: 2ND FLOOR / 33 GUILI TEL/FAX: +61 396700699







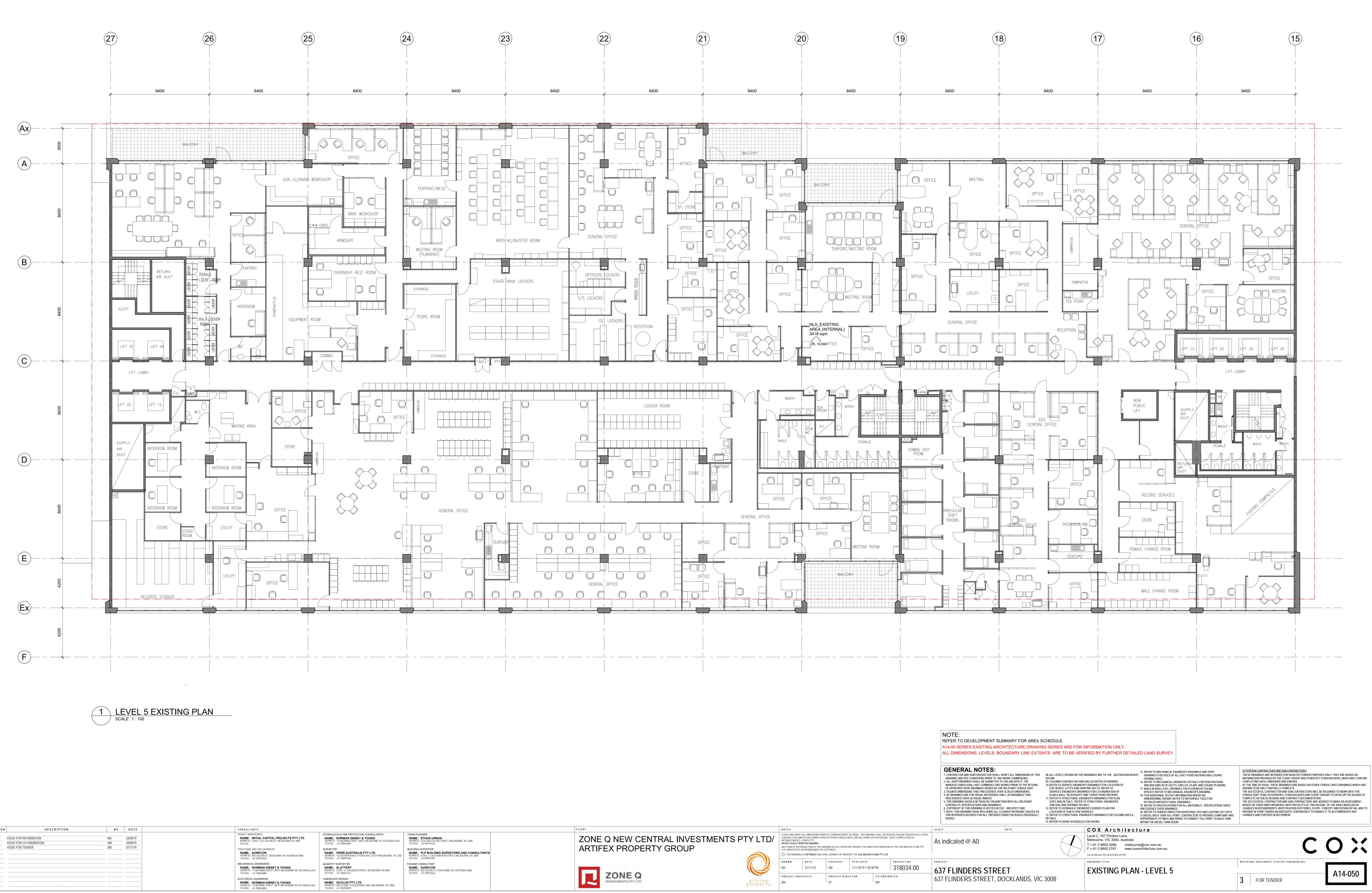
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	MANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOR TO THE RETURN OF APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT CONSULTANT	10. REFER TO SERVICE ENGINEER'S DRAW S.DF BOXES, G.P.O'S AND SKIRTING DU
	3. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.	SERVICES ENGINEER'S DRAWINGS FOR
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	7. NOTE: THIS DRAWING ISSUE INCLUDING ALL CLOUDED REVISIONS TAGGED AS	LOCATION OF SUB-FLOOR SERVICES.
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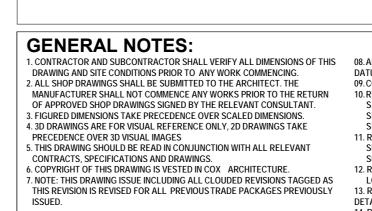
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3	ISSUE FOR TENDER	DM	01/11/19

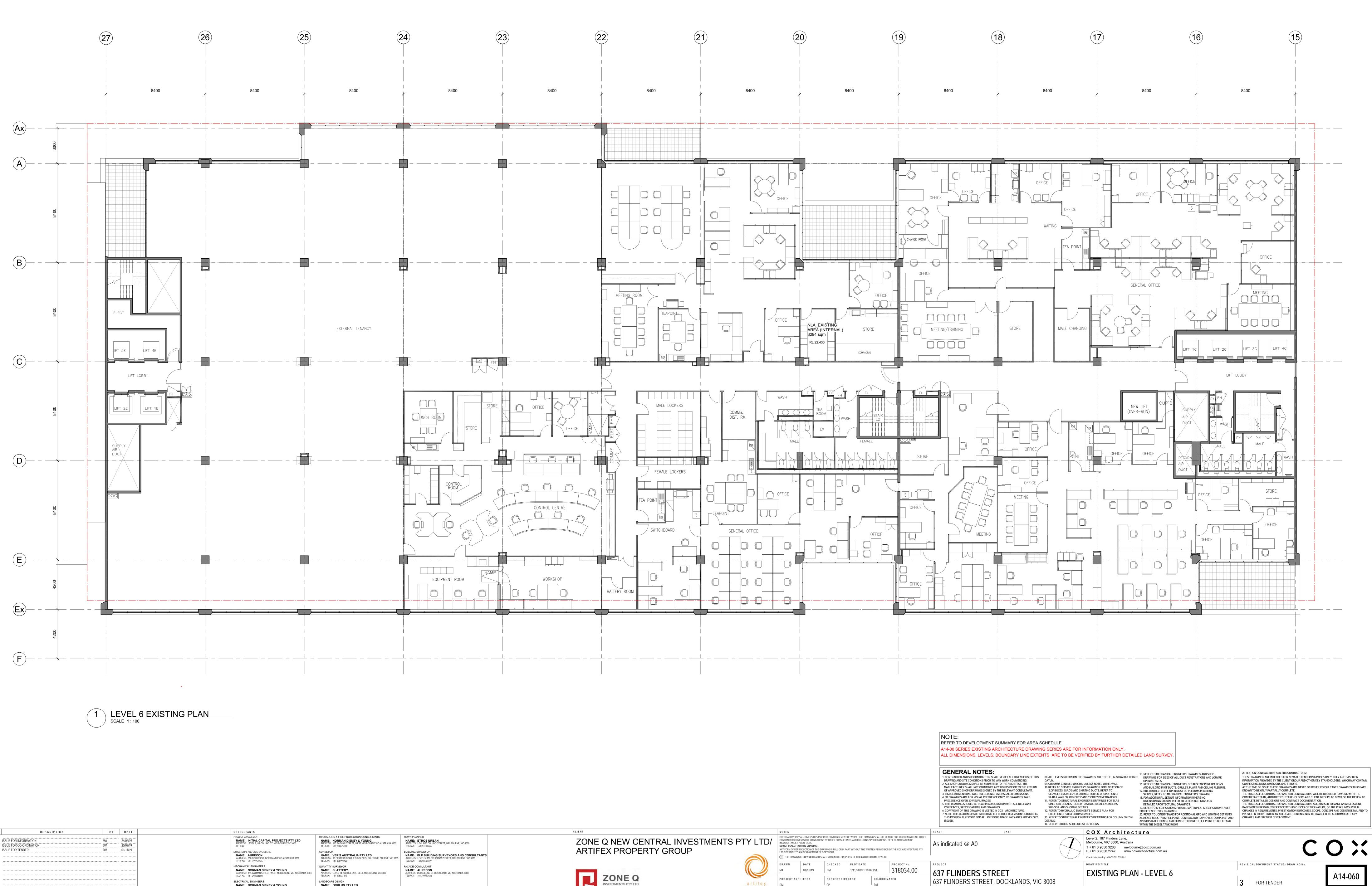








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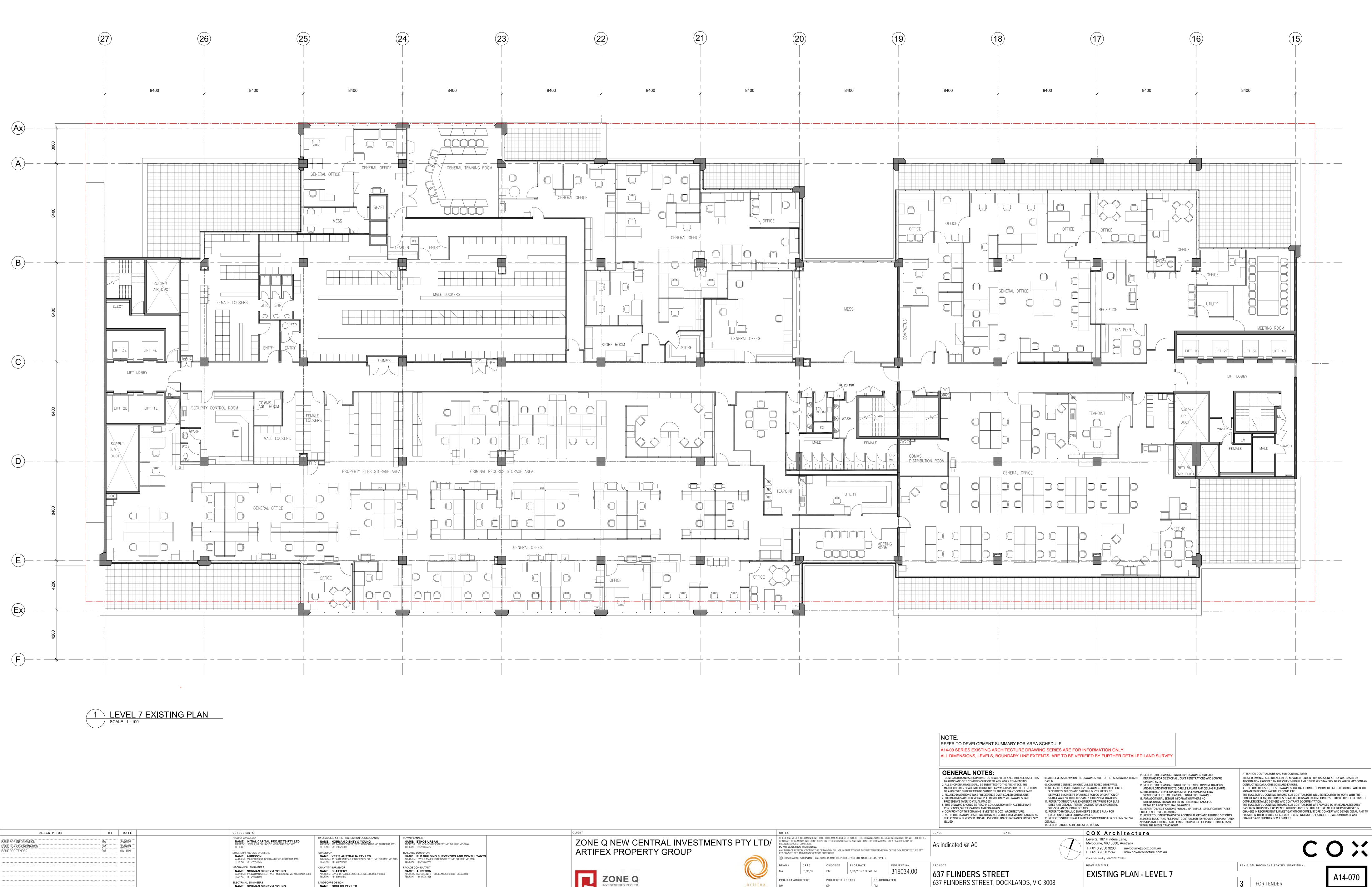
REVISION	DESCRIPTION	ВY	DATE	CONSULTANTS	
				PROJECT MANAGEMENT	HYDRAULICS & FIRE PROTECTION CONSULTANTS
	ISSUE FOR INFORMATION	MA	24/05/19	NAME: INITIAL CAPITAL PROJECTS PTY LTD	NAME: NORMAN DISNEY & YOUNG
2	ISSUE FOR CO-ORDINATION	DM	20/09/19	ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX:	ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800
3	ISSUE FOR TENDER	DM	01/11/19	STRUCTURAL AND CIVIL ENGINEERS	SURVEYOR
				NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	NAME: VERIS AUSTRALIA PTY LTD ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205 TEL/FAX: +61 396991400
				MECHANICAL ENGINEERS	QUANTITY SURVEYOR
				NAME: NORMAN DISNEY & YOUNG	NAME: SLATTERY
				ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000 TEL/FAX: +61 396021313
				ELECTRICAL ENGINEERS	LANDSCAPE DESIGN
				NAME: NORMAN DISNEY & YOUNG	NAME: OCULUS PTY LTD
				ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699
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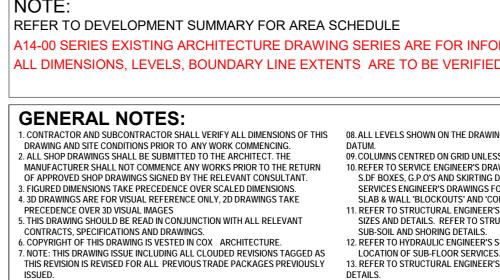
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	<ul> <li>A14-00 SERIES EXISTING ARCHITECTURE DRAWIN ALL DIMENSIONS, LEVELS, BOUNDARY LINE EXTEN ALL DIMENSIONS, LEVELS, BOUNDARY LINE EXTEN CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY ALL DIMENSIONS OF THIS DRAWING AND SITE CONDITIONS PRIOR TO ANY WORK COMMENCING.</li> <li>ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHITECT. THE MANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOR TO THE RETURN OF APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT CONSULTANT.</li> <li>FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.</li> <li>JD DRAWINGS ARE FOR VISUAL REFERENCE ONLY, 2D DRAWINGS TAKE PRECEDENCE OVER 3D VISUAL IMAGES</li> <li>STHIS DRAWING SHOLD BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS AND DRAWINGS.</li> <li>COPYRIGHT OF THIS DRAWING IS VESTED IN COX_ARCHITECTURE.</li> <li>NOTE: THIS DRAWING ISSUE INCLUDING ALL CLOUDED REVISIONS TAGGED AS THIS REVISION IS REVISED FOR ALL PREVIOUS TRADE PACKAGES PREVIOUSLY ISSUED.</li> </ul>	CINERS CONTROL
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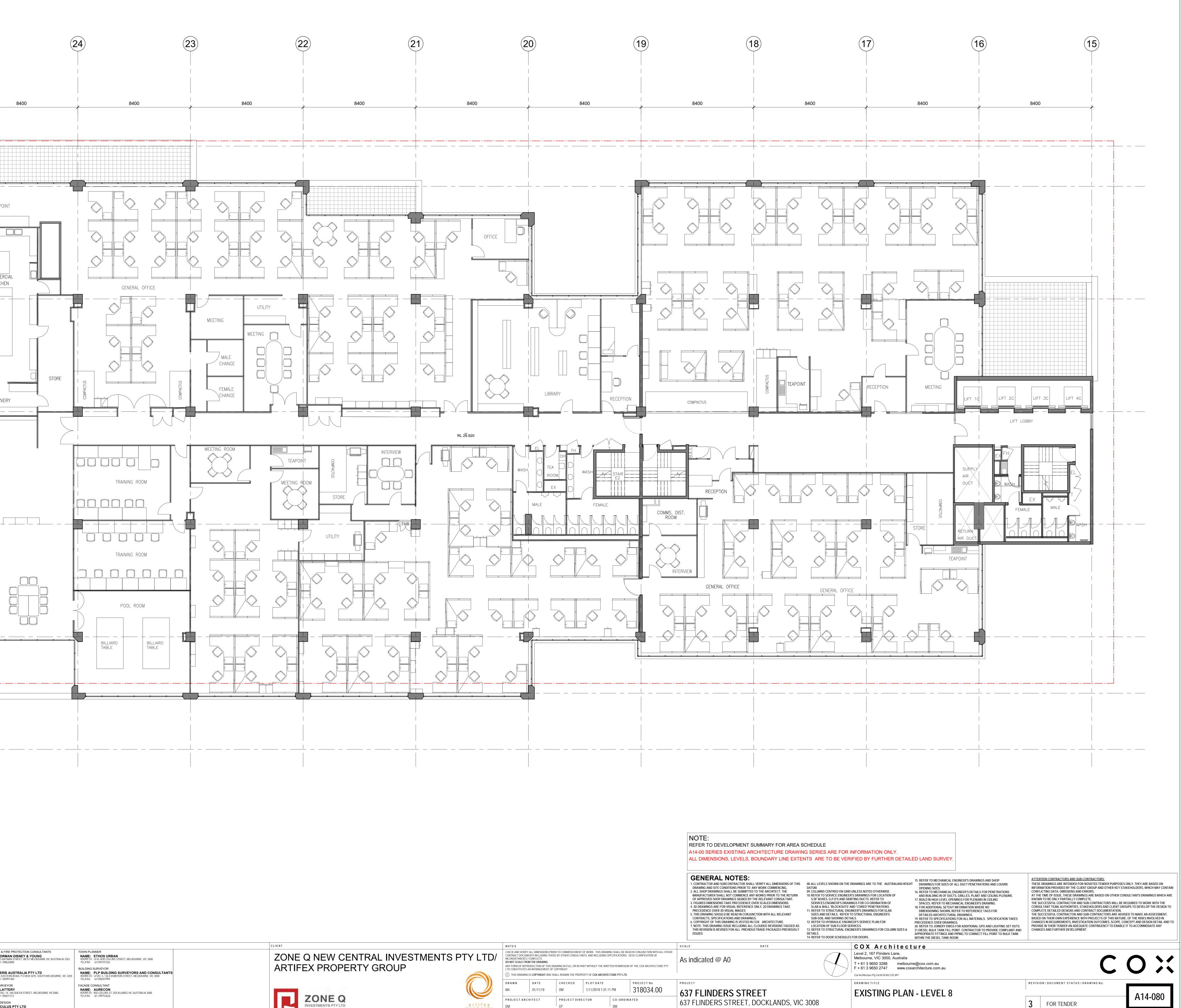
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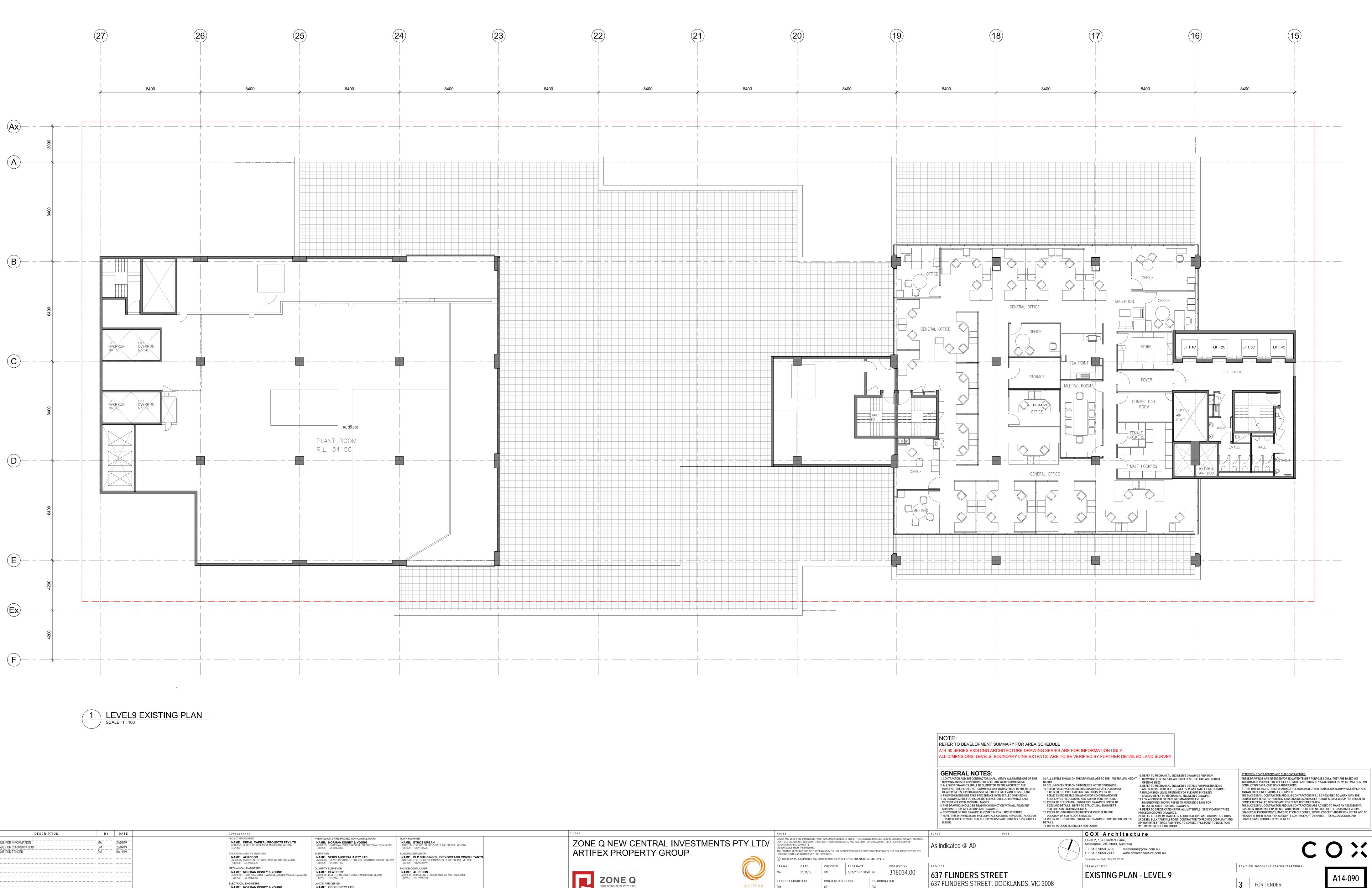
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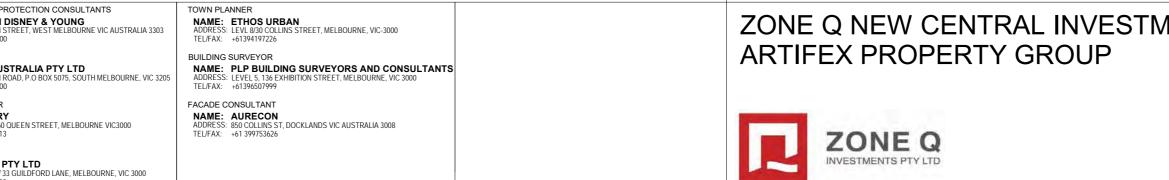


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	<ul> <li>GENERAL NOTES:</li> <li>1. CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY ALL DIMENSIONS OF THIS DRAWING AND SITE CONDITIONS PRIOR TO ANY WORK COMMENCING.</li> <li>2. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHITECT. THE MANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOR TO THE RETURN OF APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT CONSULTANT.</li> <li>3. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.</li> <li>4. 3D DRAWINGS ARE FOR VISUAL REFERENCE ONLY, 2D DRAWINGS TAKE PRECEDENCE OVER 3D VISUAL IMAGES</li> <li>5. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS AND DRAWINGS.</li> <li>6. COPYRIGHT OF THIS DRAWING ISVES INCLUDING ALL CLOUDED REVISIONS TAGGED AS THIS REVISION IS REVISED FOR ALL PREVIOUS TRADE PACKAGES PREVIOUSLY ISSUED.</li> </ul>	08. ALL LEVELS SHOWN ON THE I DATUM. 09. COLUMNS CENTRED ON GRID 10. REFER TO SERVICE ENGINEEF S.D.F BOXES, G.P.O'S AND SKI SERVICES ENGINEER'S DRAW SLAB & WALL 'BLOCKOUTS' A 11. REFER TO STRUCTURAL ENG SIZES AND DETAILS. REFER T SUB-SOIL AND SHORING DETA 12. REFER TO HYDRAULIC ENGIN LOCATION OF SUB-FLOOR SE 13. REFER TO STRUCTURAL ENG DETAILS. 14. REFER TO DOOR SCHEDULES	UNLESS R'S DRAV RTING DI INGS FO AND 'COF INEER'S O STRU' AILS. EER'S SI RVICES. INEER'S
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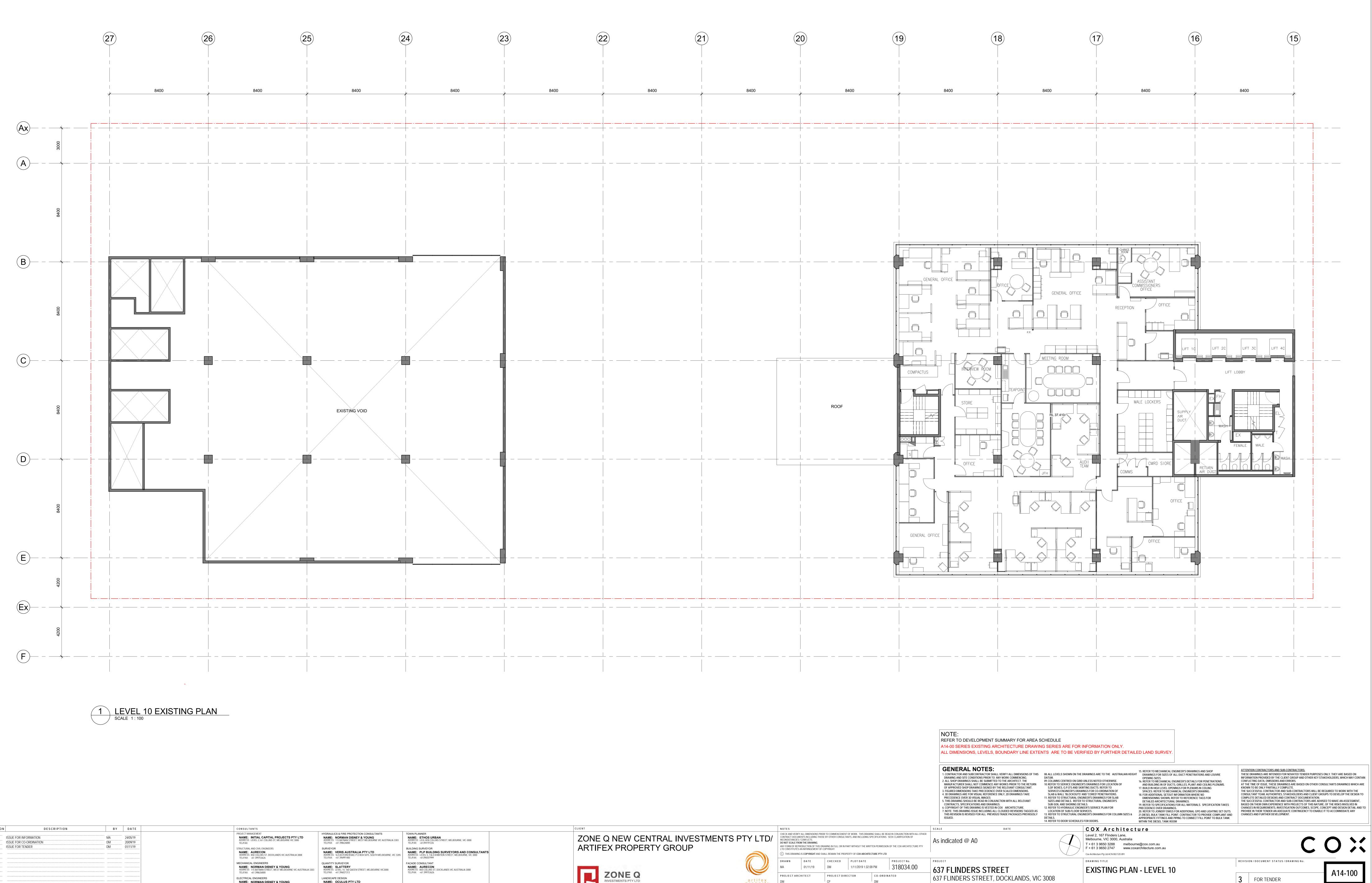


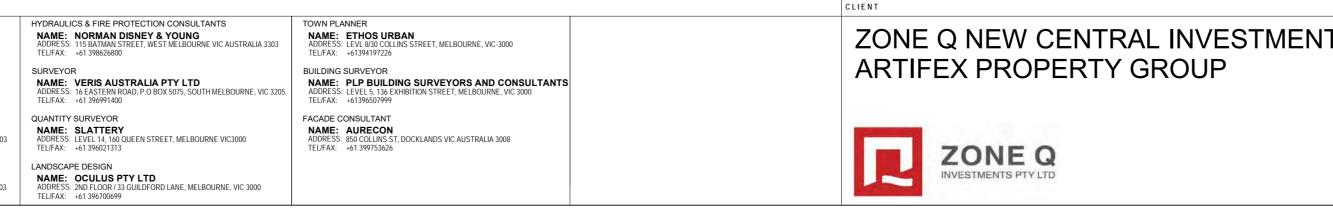
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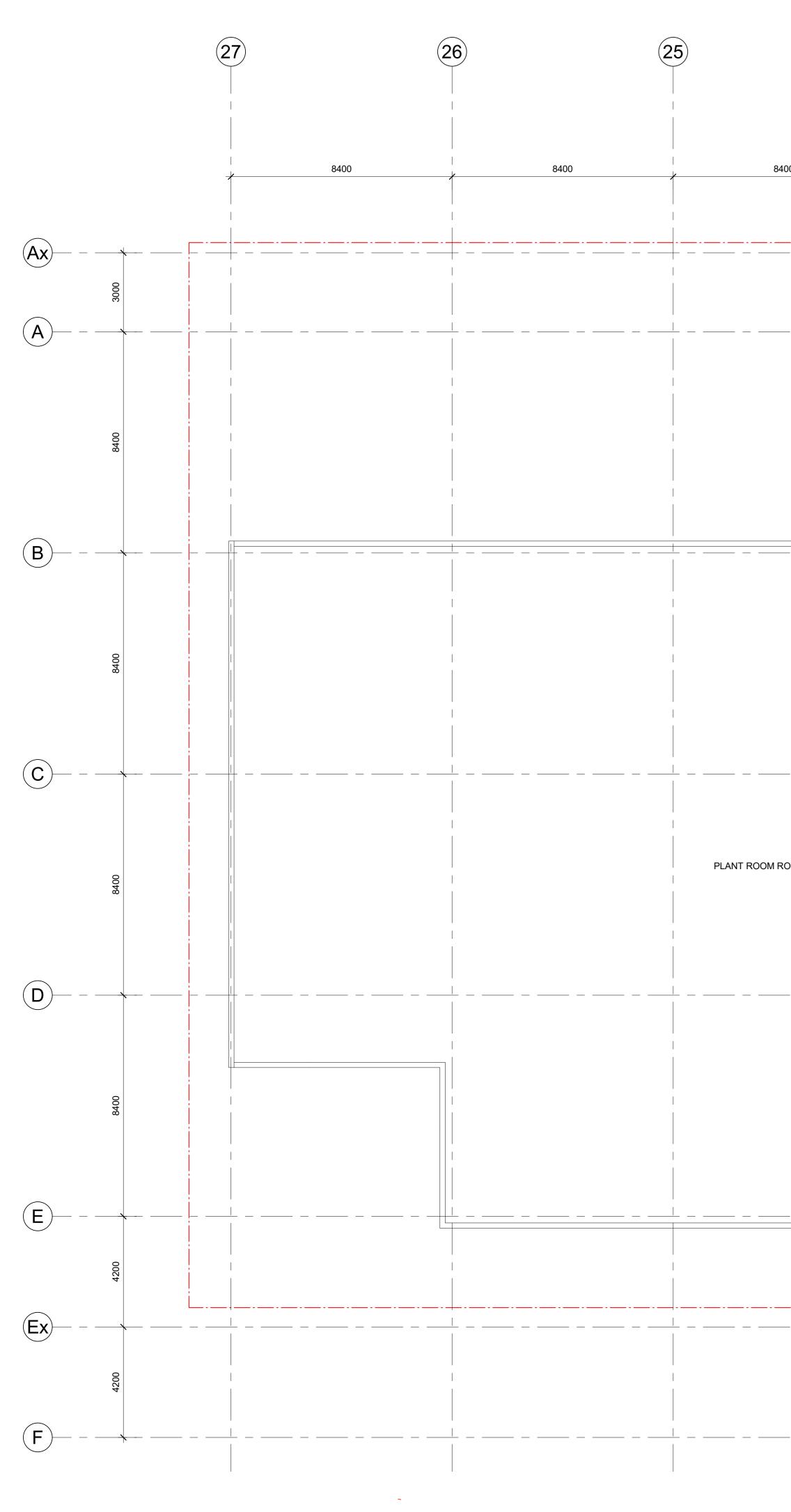


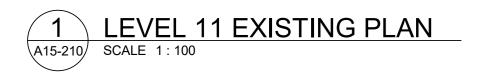


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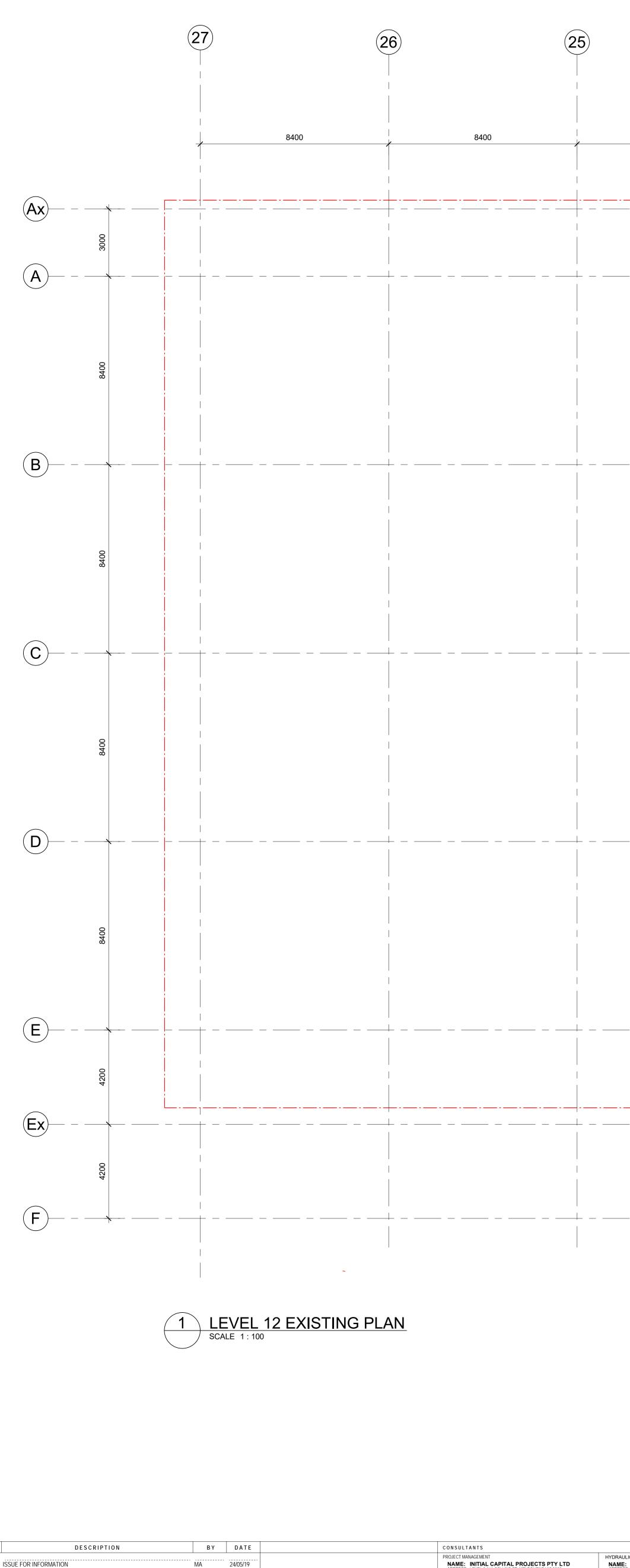
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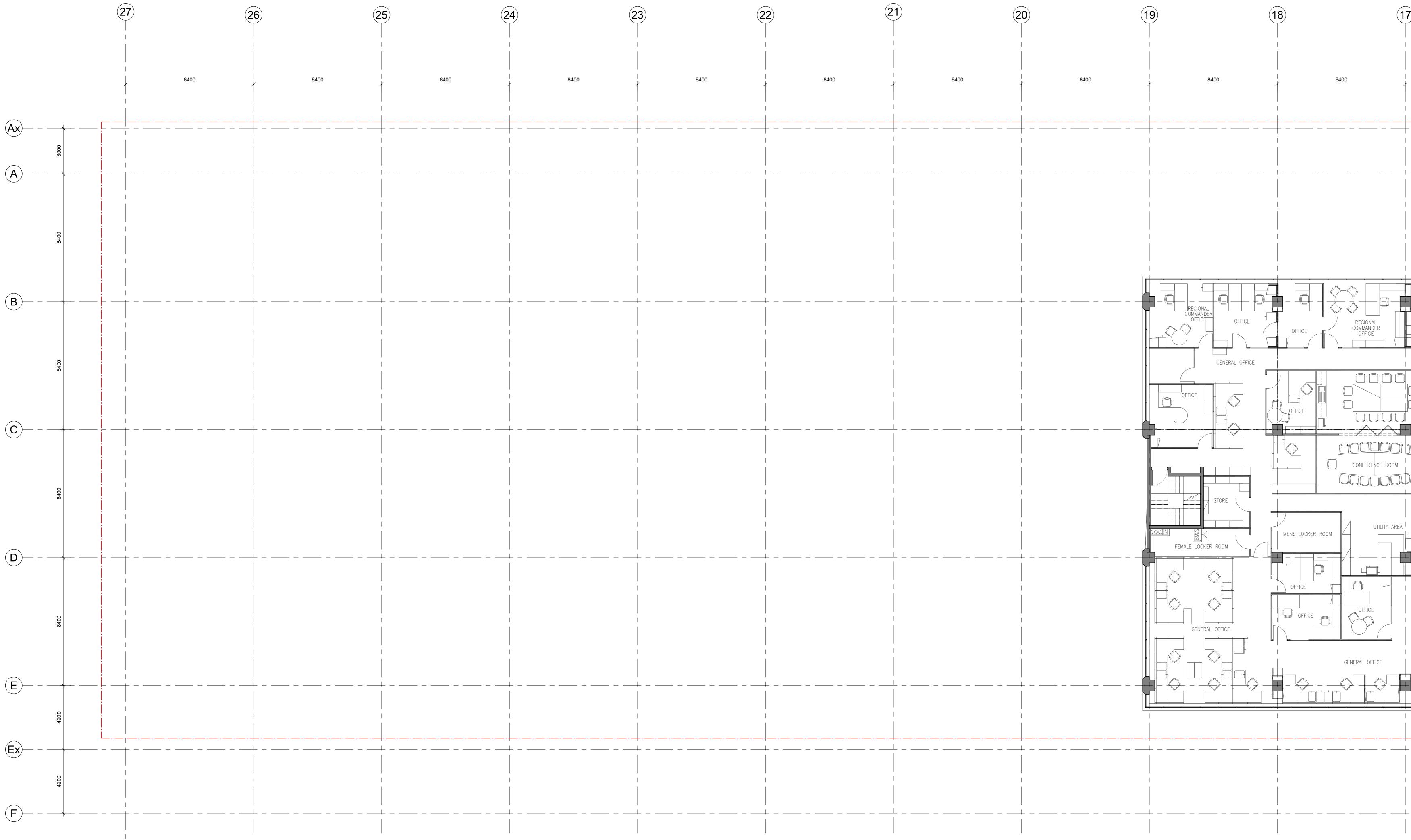
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 ELECTRICAL ENGINEERS
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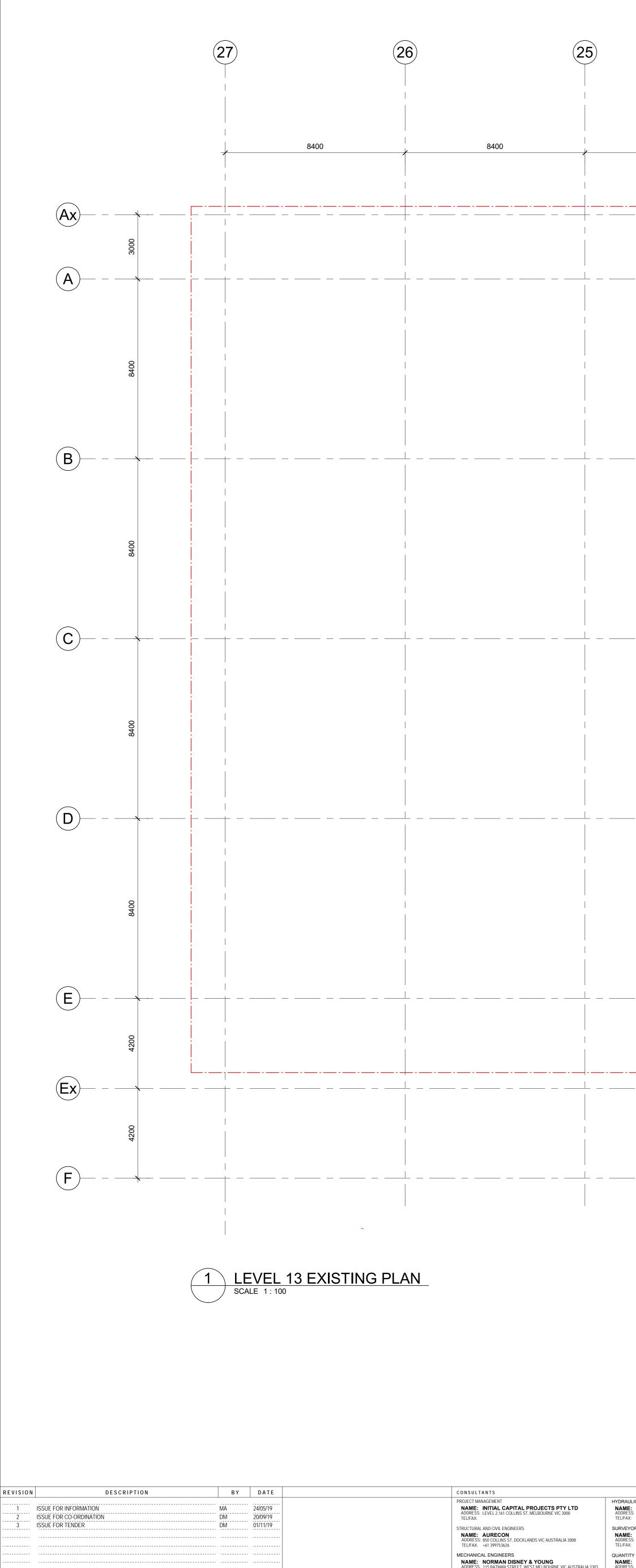
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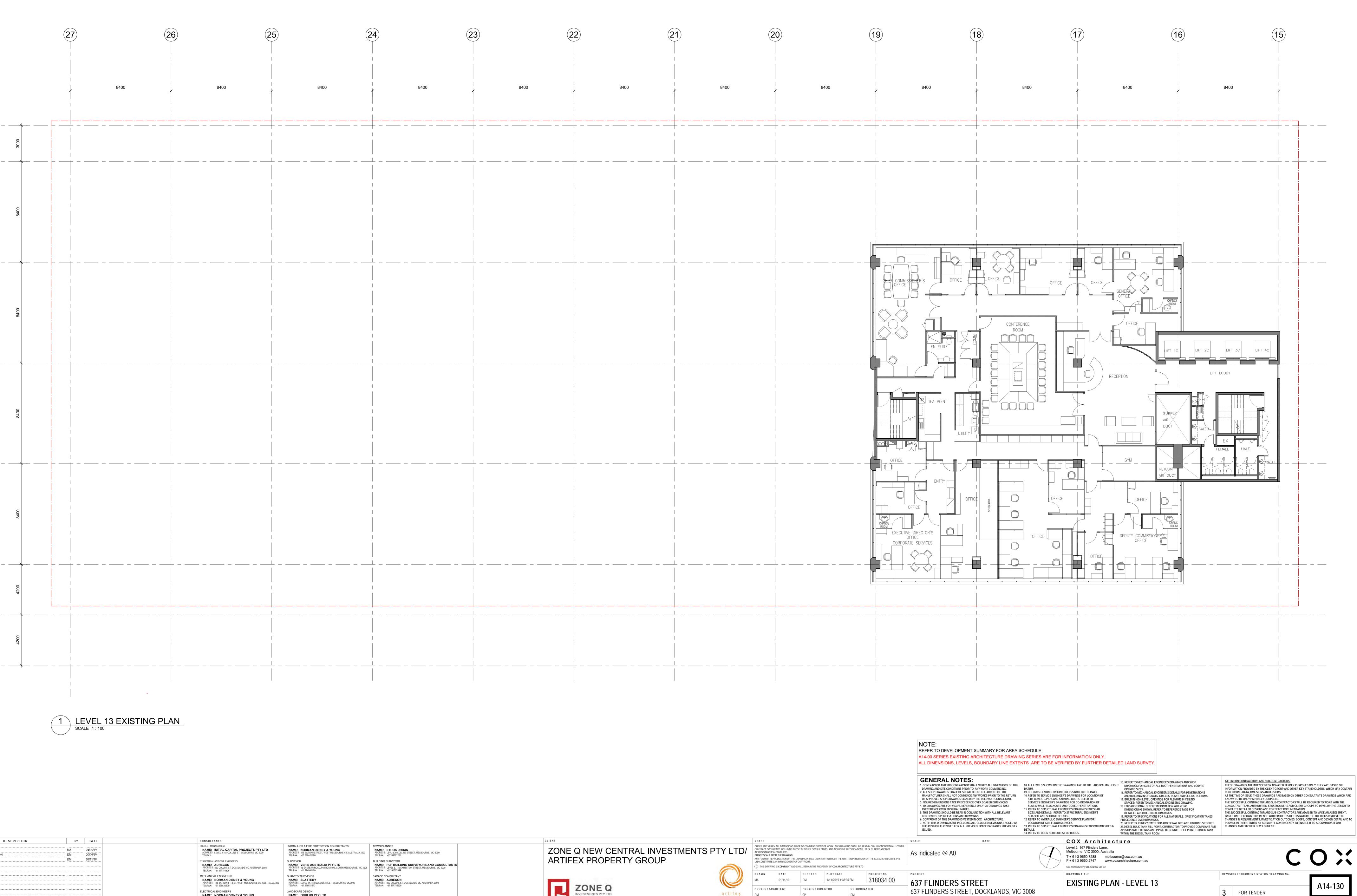




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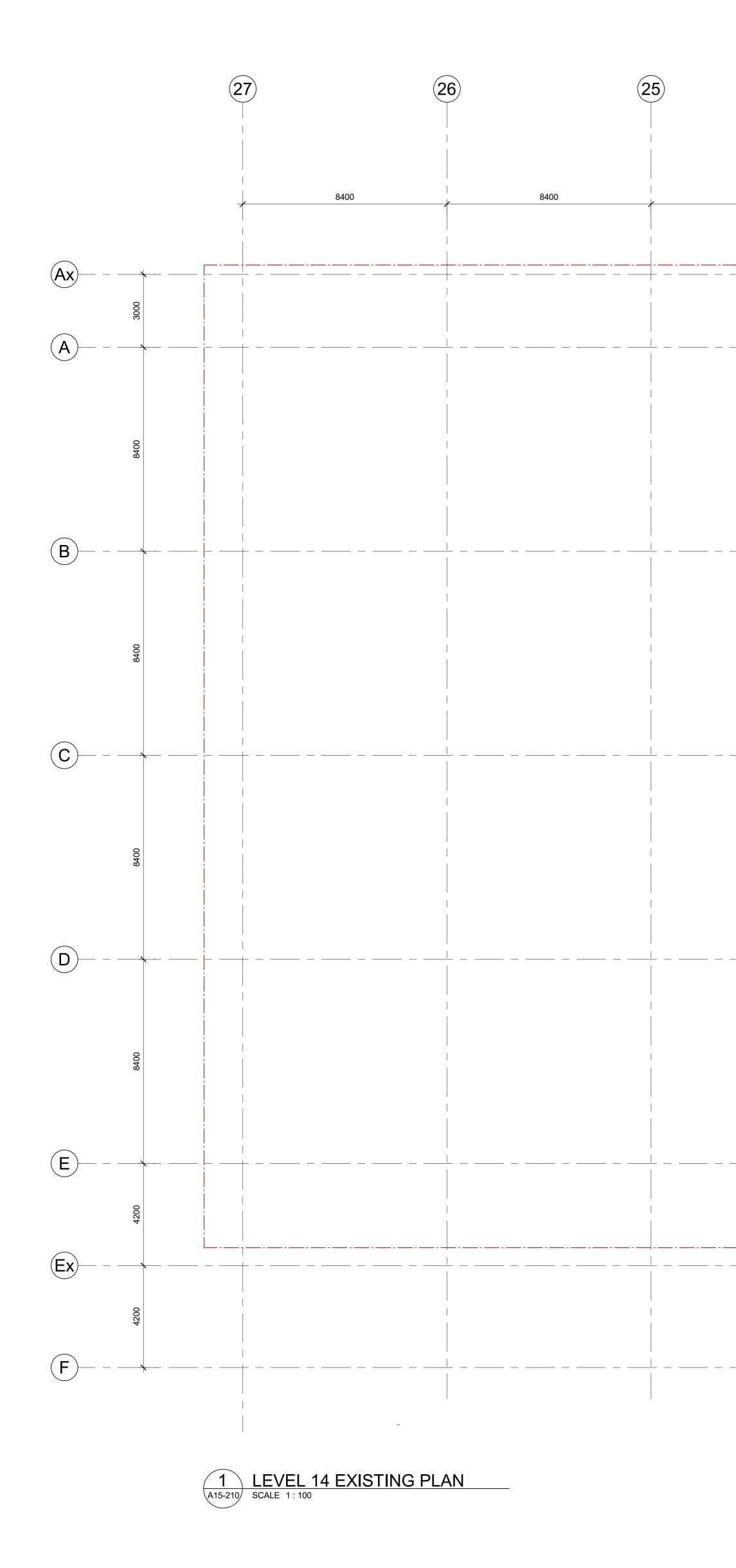
SURVEYOR NAME: VERIS AUSTRALIA PTY LTD ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUT QUANTITY SURVEYOR NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800 NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000 TEL/FAX: +61 396021313 ELECTRICAL ENGINEERS LANDSCAPE DESIGN NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800 NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TELIFAX: +61 396700699



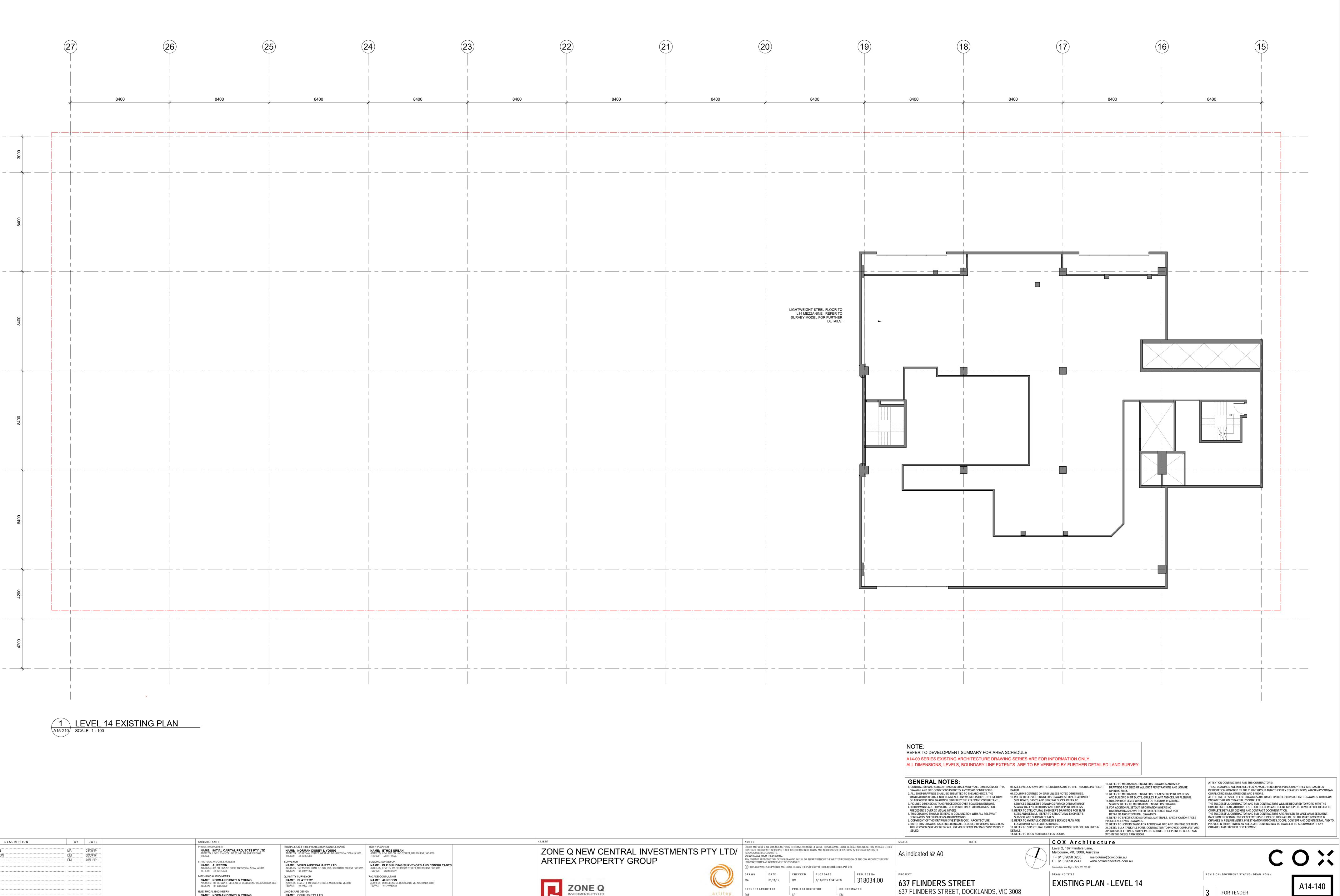


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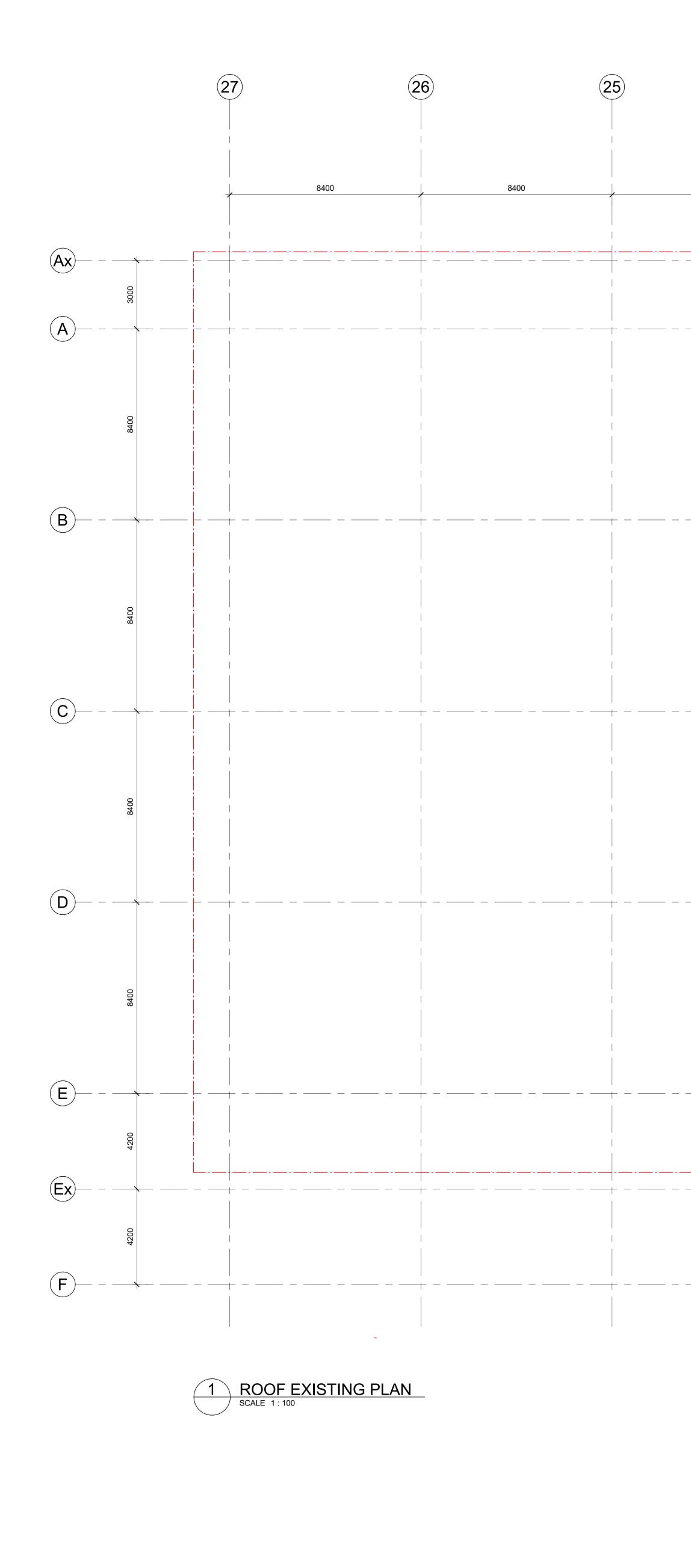
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3	ISSUE FOR TENDER	DM	01/11/19	STRUCTURAL AND CIVIL ENGINEERS	SURVEYOR
				NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	NAME: VERI ADDRESS: 16 EAS TEL/FAX: +61 396
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				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	ADDRESS: LEVEL TEL/FAX: +61 39
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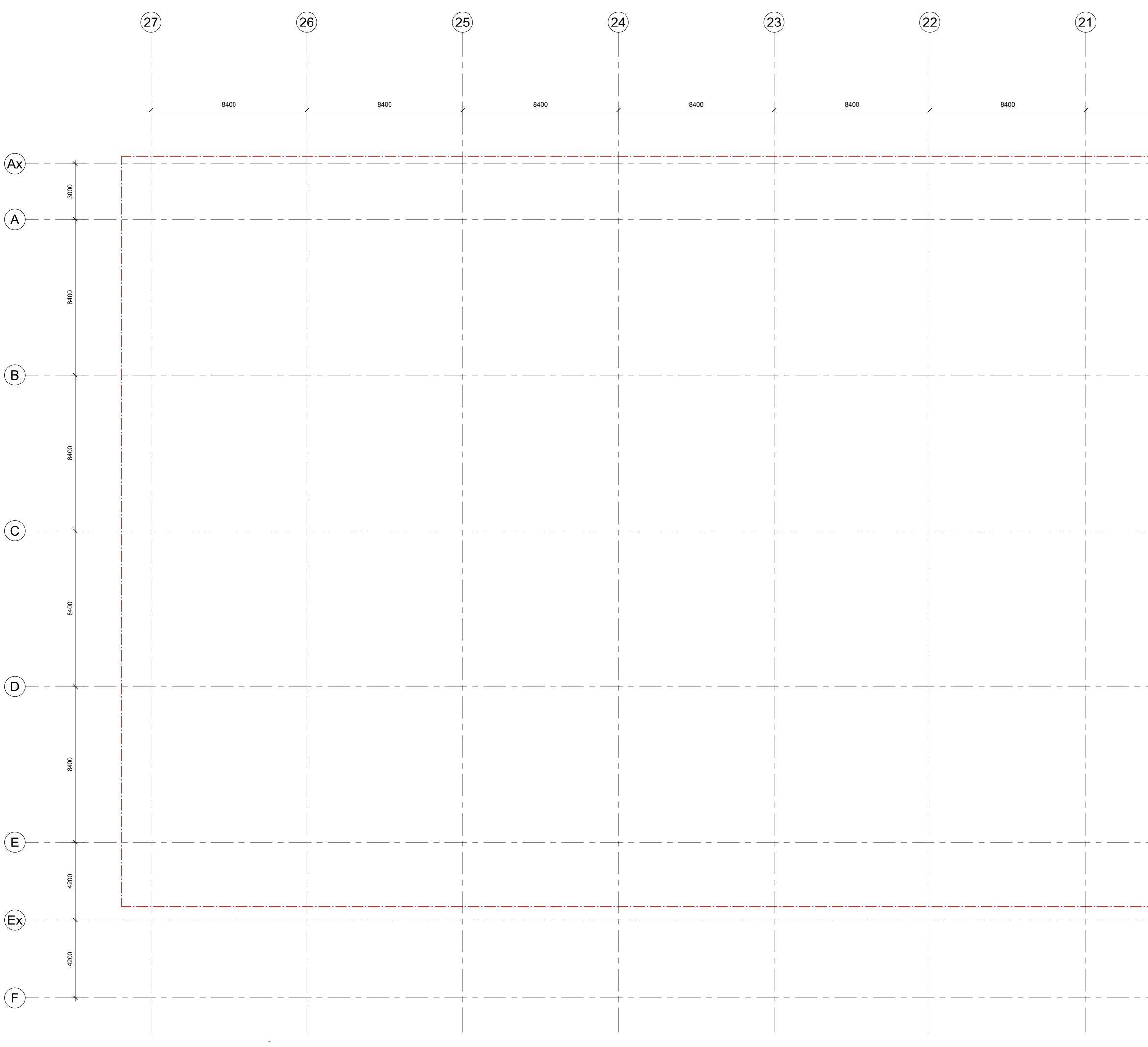


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3 1	ISSUE FOR TENDER	DM	01/11/19	STRUCTURAL AND CIVIL ENGINEERS
				ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626
				MECHANICAL ENGINEERS
				NAME: NORMAN DISNEY & YOUNG
				ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800
				ELECTRICAL ENGINEERS
				NAME: NORMAN DISNEY & YOUNG
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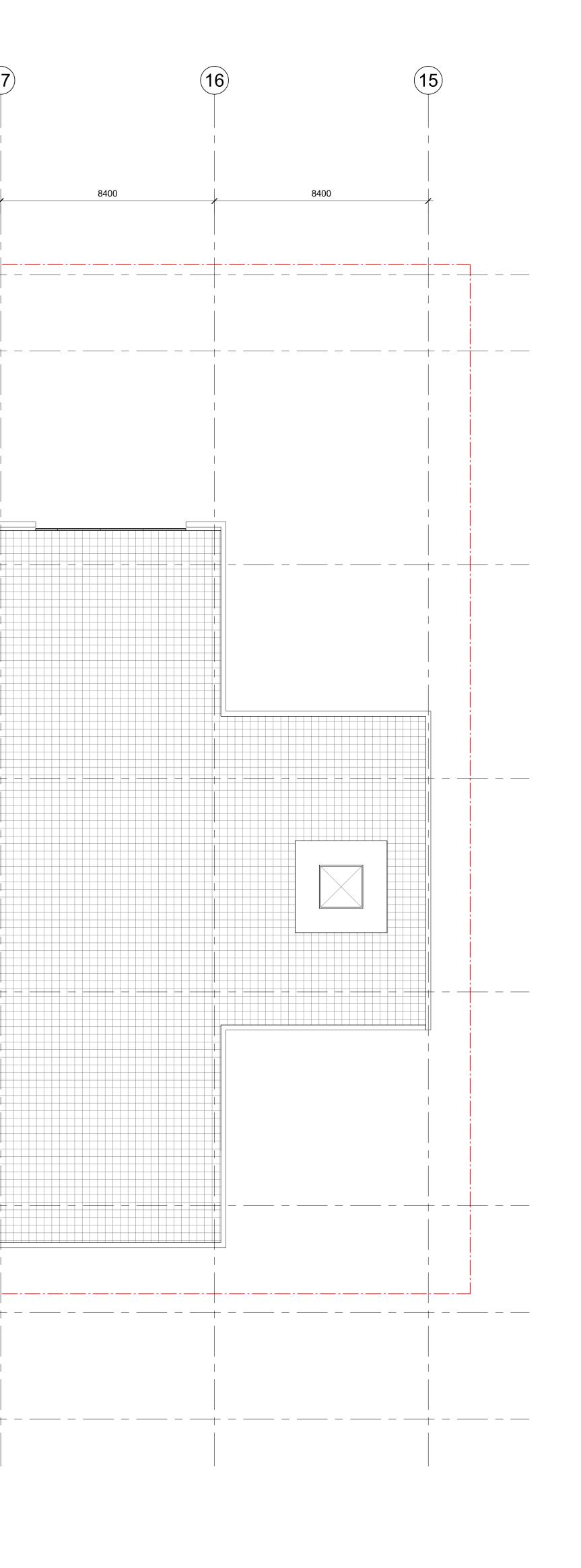


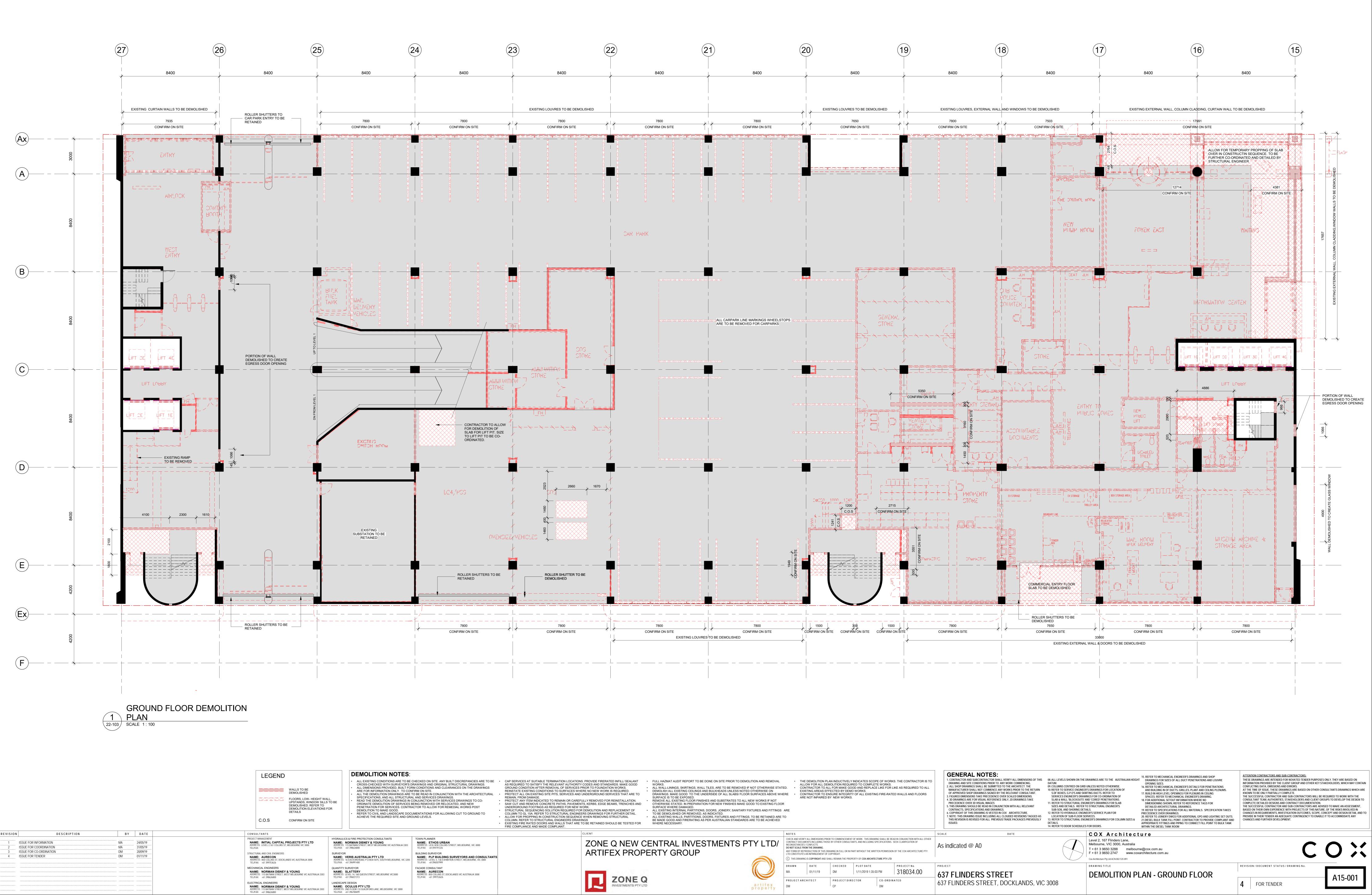


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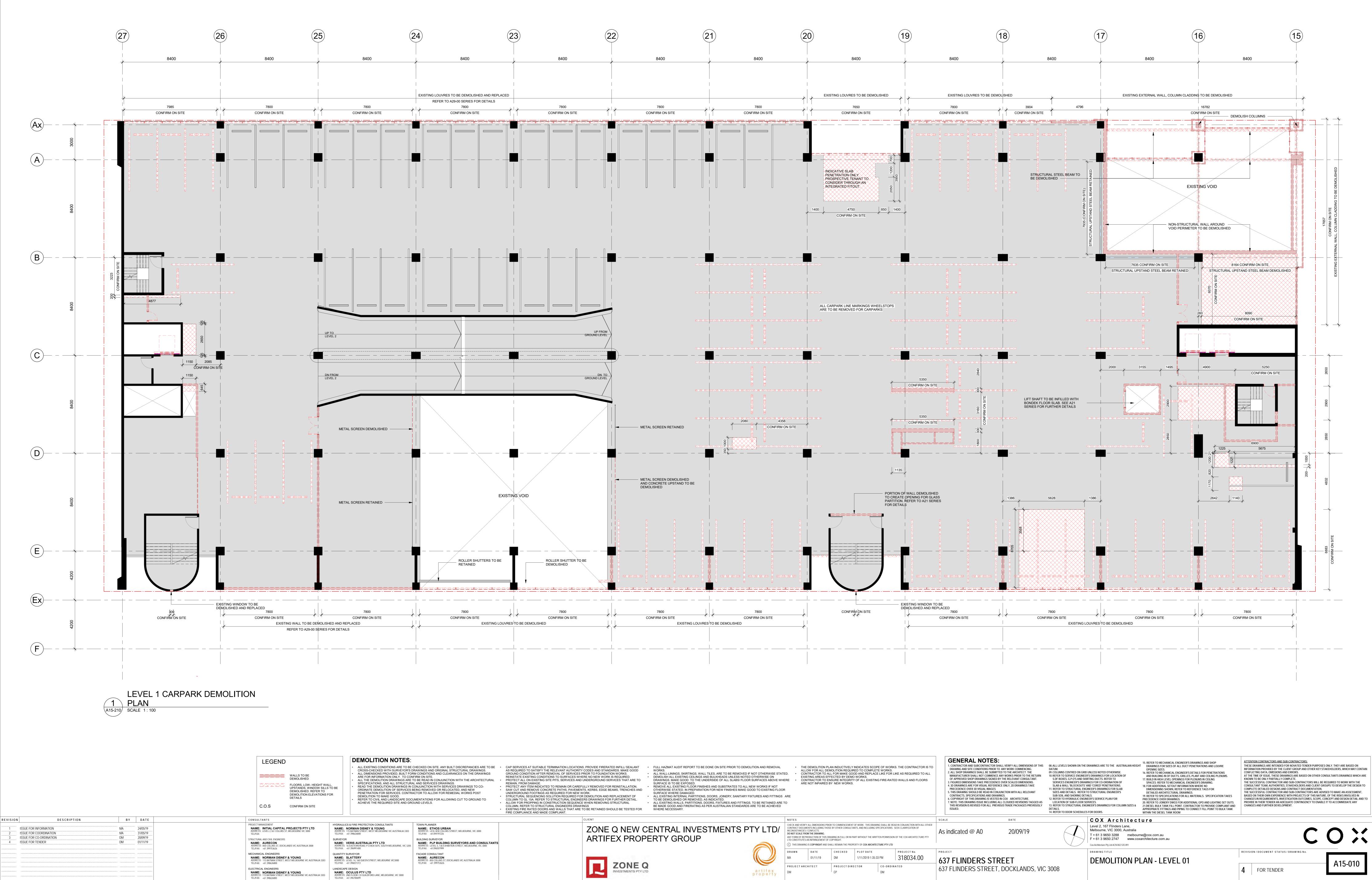
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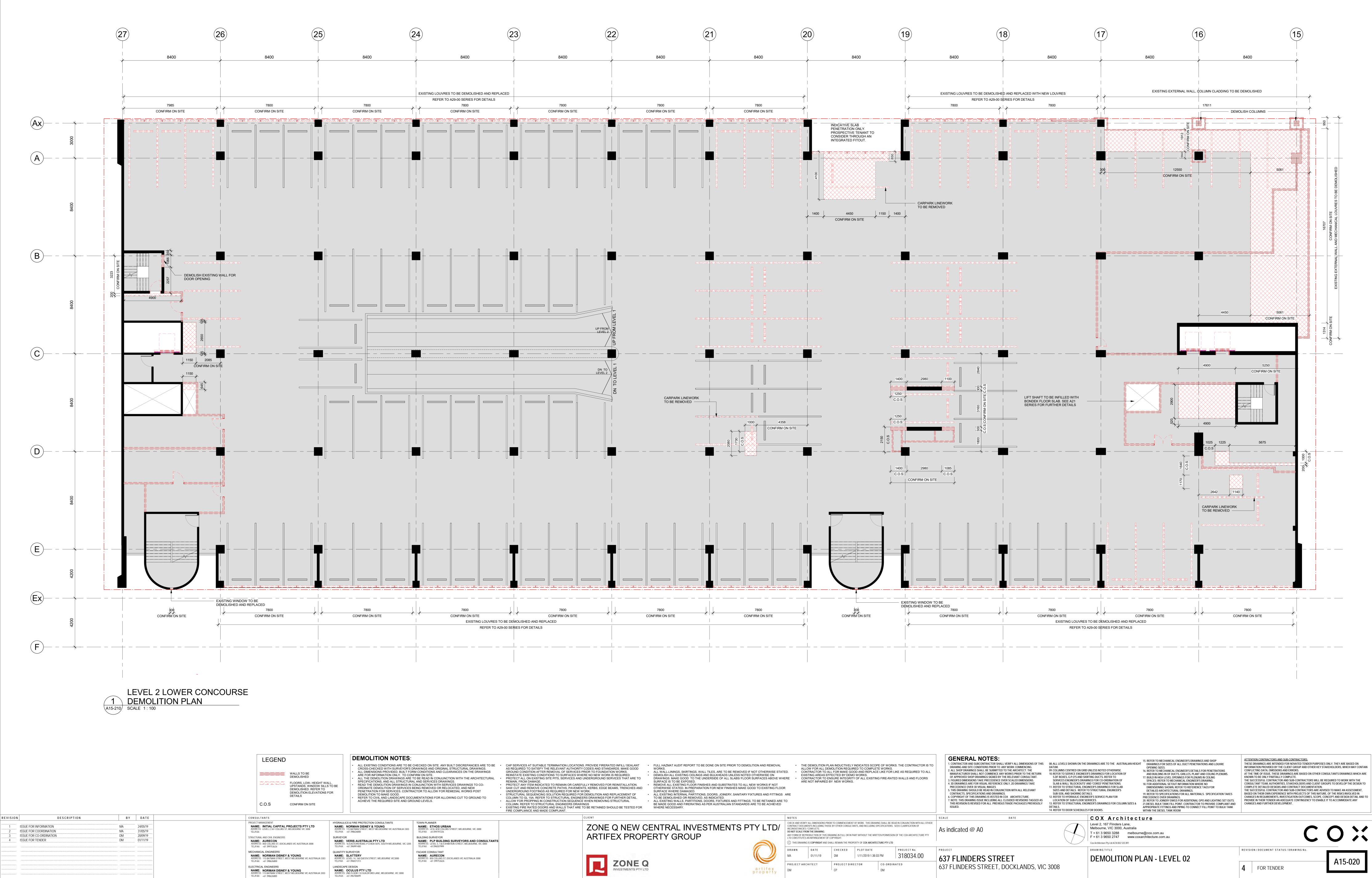




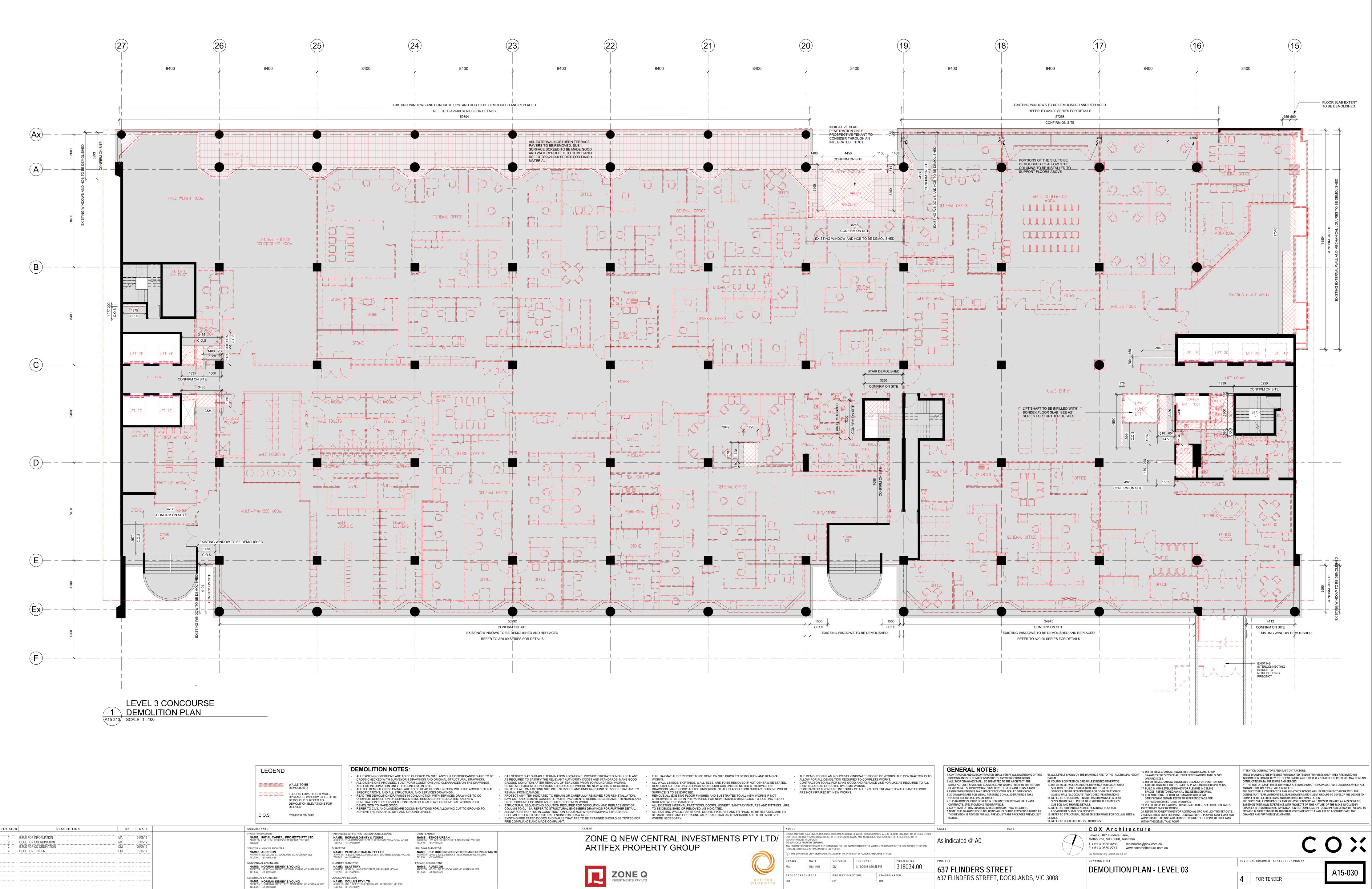
<ul> <li>TO DEMOLITION AND REMOVAL</li> <li>THE DEMOLITION PLAN INDUCTIVELY INDICATES SCOPE OF WORKS. THE CONTRACTOR IS ALLOW FOR ALL DEMOLITION REQUIRED TO COMPLETE WORKS.</li> <li>CONTRACTOR TO ALL FOR MAKE GOOD AND REPLACE LIKE FOR LIKE AS REQUIRED TO AL EXISTING AREAS EFFECTED BY DEMO WORKS.</li> <li>CONTRACTOR TO ENSURE INTEGRITY OF ALL EXISTING FIRE-RATED WALLS AND FLOORS ARE NOT INPAIRED BY NEW WORKS.</li> <li>CONTRACTOR TO ENSURE INTEGRITY OF ALL EXISTING FIRE-RATED WALLS AND FLOORS ARE TO BE ACHIEVED</li> </ul>								GENERAL NOTES: 1. CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY ALL DIMENSIONS OF THIS DRAWING AND SITE CONDITIONS PRIOR TO ANY WORK COMMENCING. 2. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHITECT. THE MANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOR TO THE RETURN OF APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT CONSULTANT. 3. FIGURED DIMENSIONS TAKE PRECEDENCE OVLY, 2D DRAWINGS TAKE PRECEDENCE OVER 3D VISUAL REFERENCE ONLY, 2D DRAWINGS TAKE PRECEDENCE OVER 3D VISUAL IMAGES 5. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT CONTRACTS, SPECIFICATIONS AND DRAWINGS. 6. COPYRIGHT OF THIS DRAWING IS VESTED IN COX ARCHITECTURE. 7. NOTE: THIS DRAWING ISSUE INCLUDING ALL CLOUDED REVISIONS TAGGED AS THIS REVISION IS REVISED FOR ALL PREVIOUS TRADE PACKAGES PREVIOUSLY ISSUED.	08. ALL LEVELS SHOWN ON THE I DATUM. 09. COLUMNS CENTRED ON GRID 10. REFER TO SERVICE ENGINEEF S.DF BOXES, G.P.O'S AND SKII SERVICES ENGINEER'S DRAW SLAB & WALL 'BLOCKOUTS' A 11. REFER TO STRUCTURAL ENG SIZES AND DETAILS. REFER T SUB-SOIL AND SHORING DET/ 12. REFER TO HYDRAULIC ENGIN LOCATION OF SUB-FLOOR SE 13. REFER TO STRUCTURAL ENG DETAILS. 14. REFER TO DOOR SCHEDULES	UNLESS NOTEI Carling Ducts, RTING DUCTS, INGS FOR CO-C ND 'CORED' PE INEER'S DRAWI 'O STRUCTURA AILS, EER'S SERVICE RVICES, INEER'S DRAWI	
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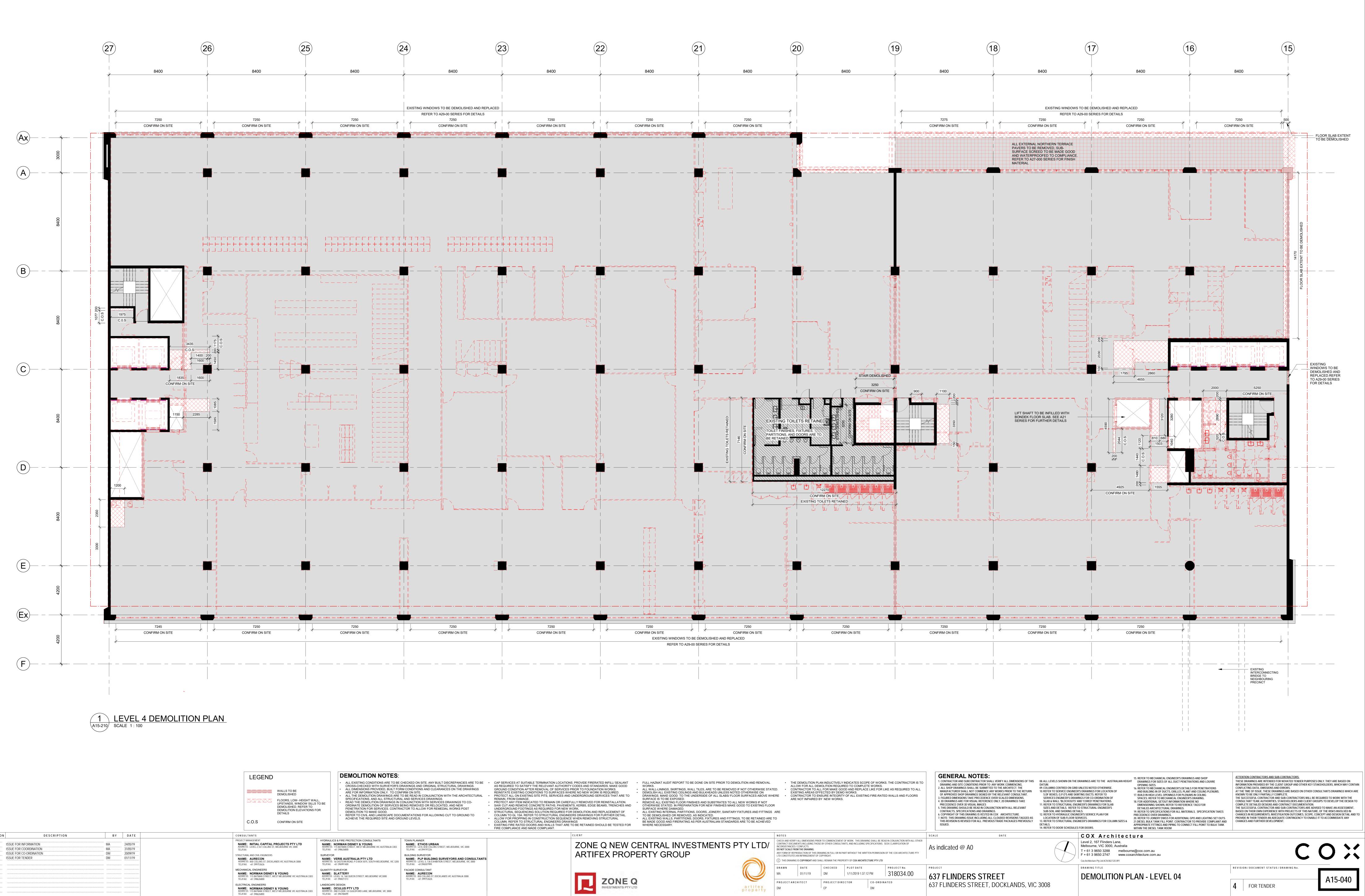
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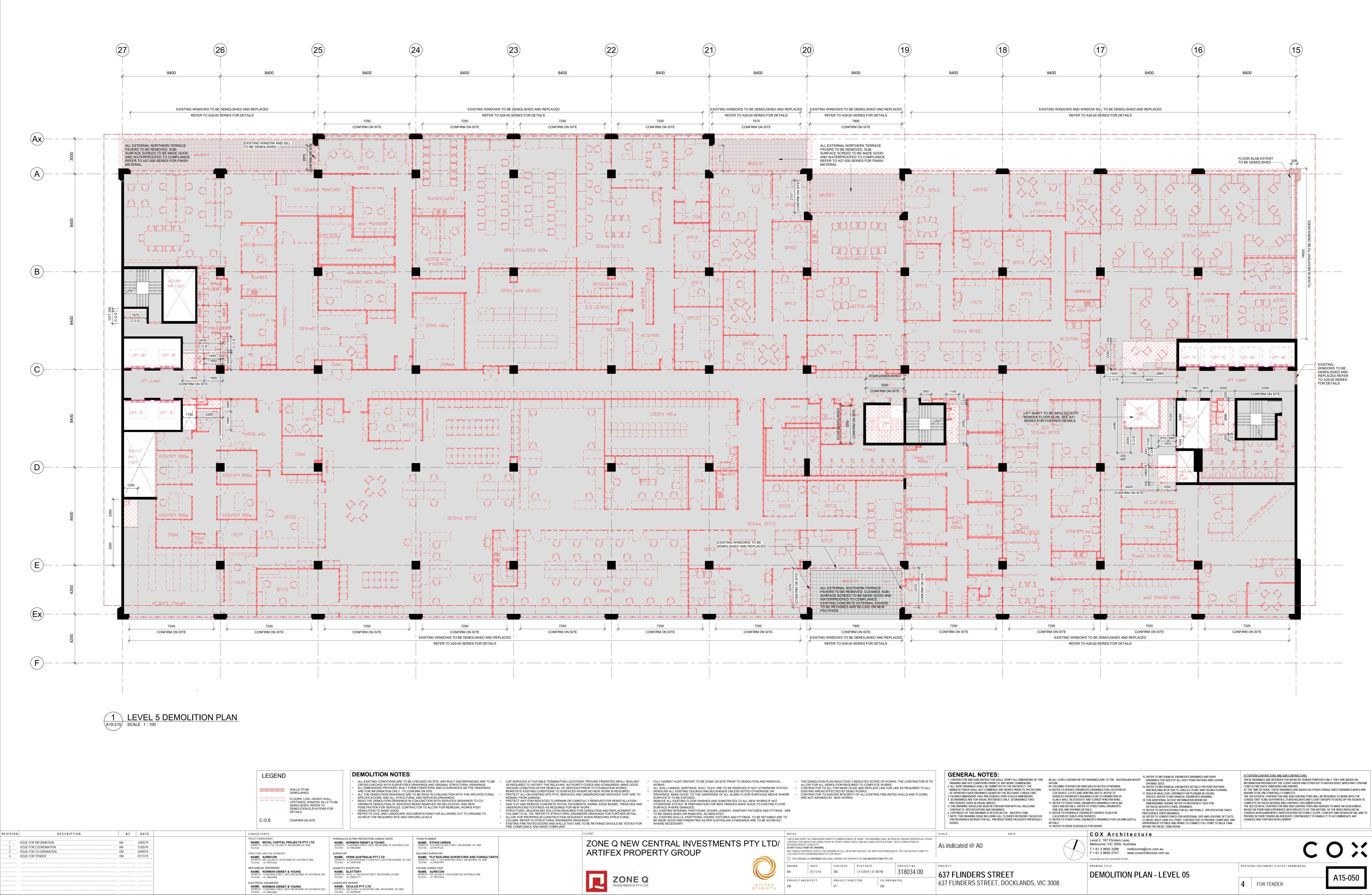


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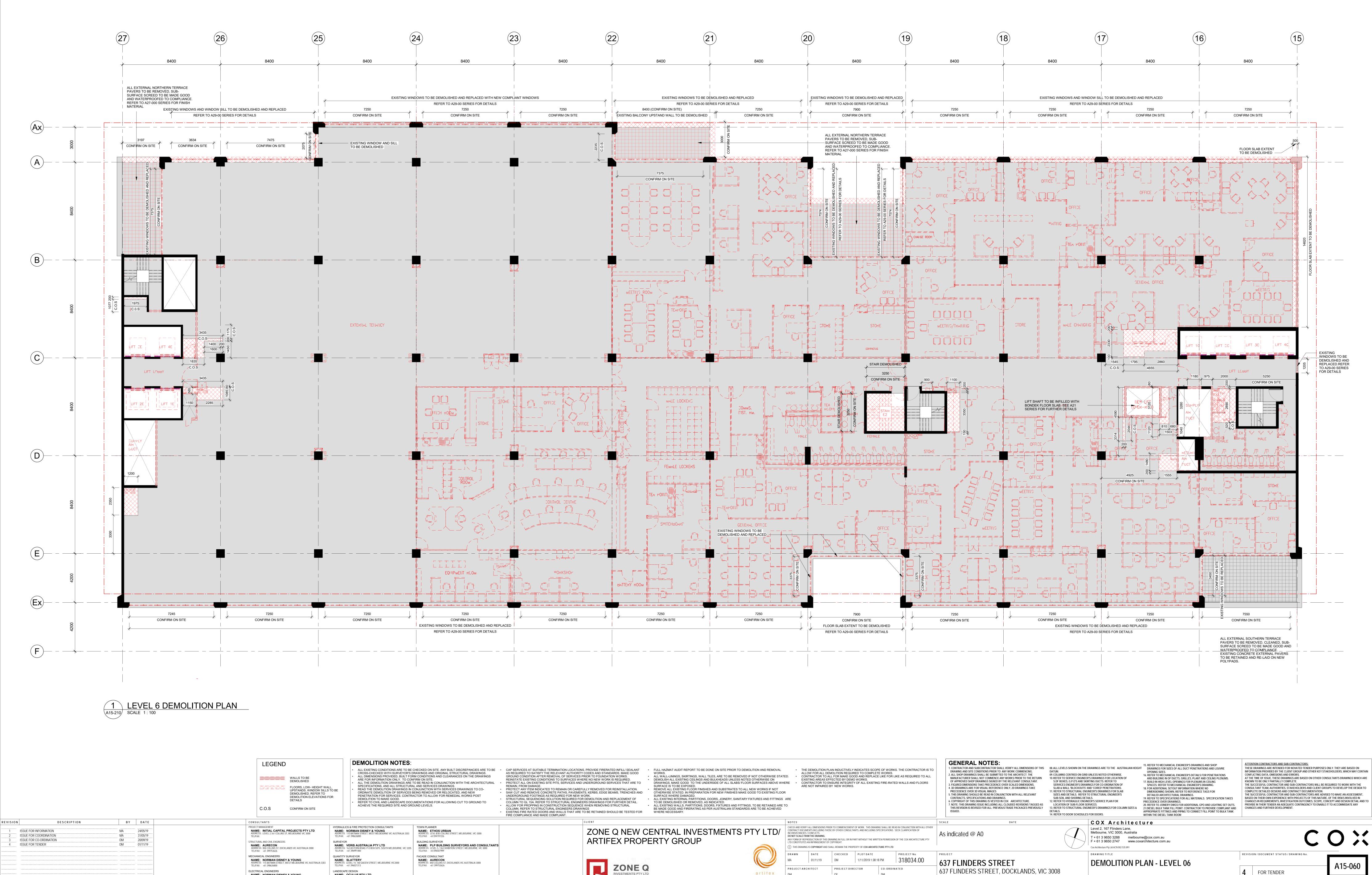


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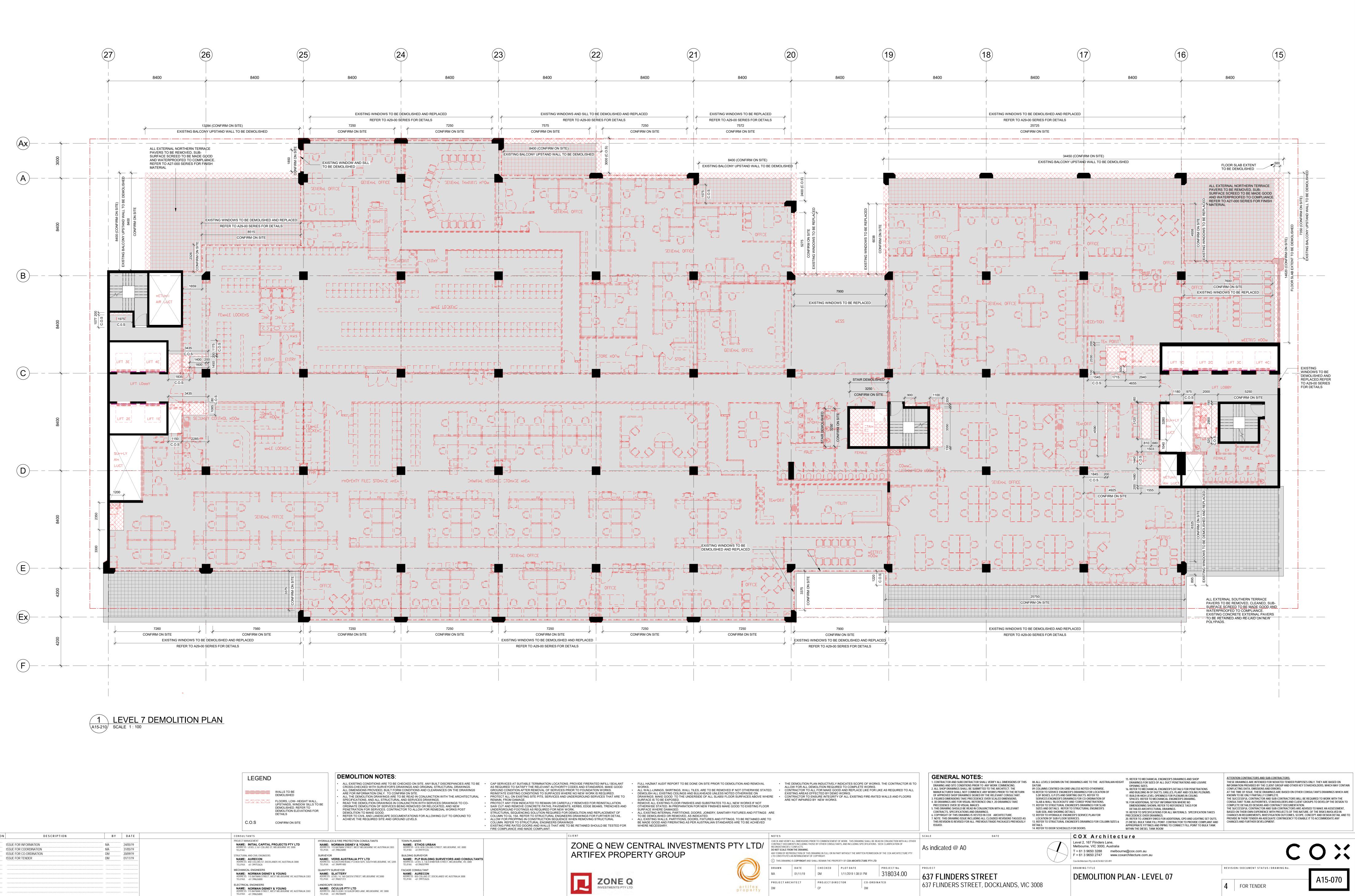


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EEN STREET, MELBOURNE VIC3000 <b>/ LTD</b> UILDFORD LANE, MELBOURNE, VIC 3000	FACADE CONSULTANT NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	ZONE Q INVESTMENTS PTY LTD	

NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800

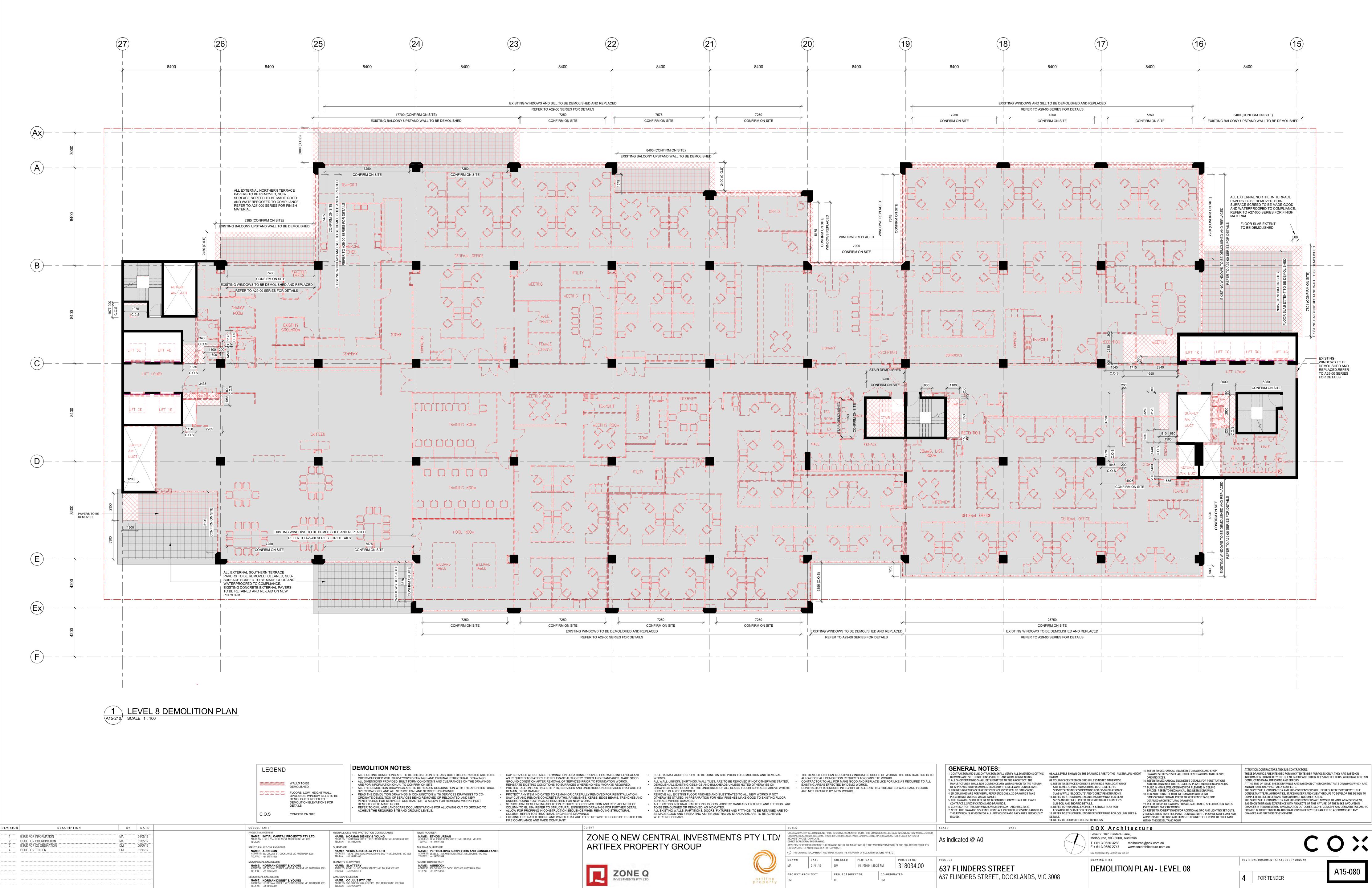
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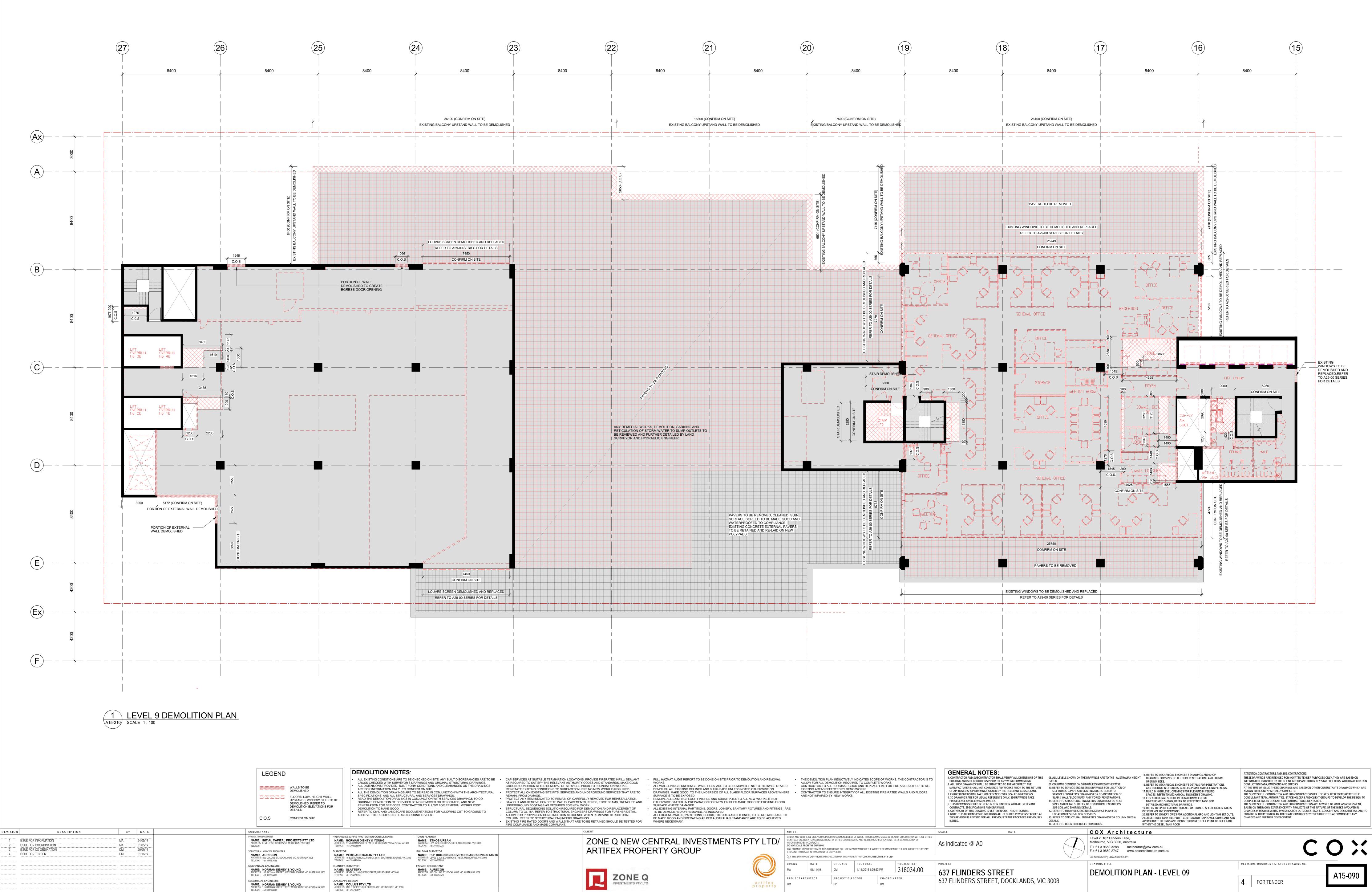


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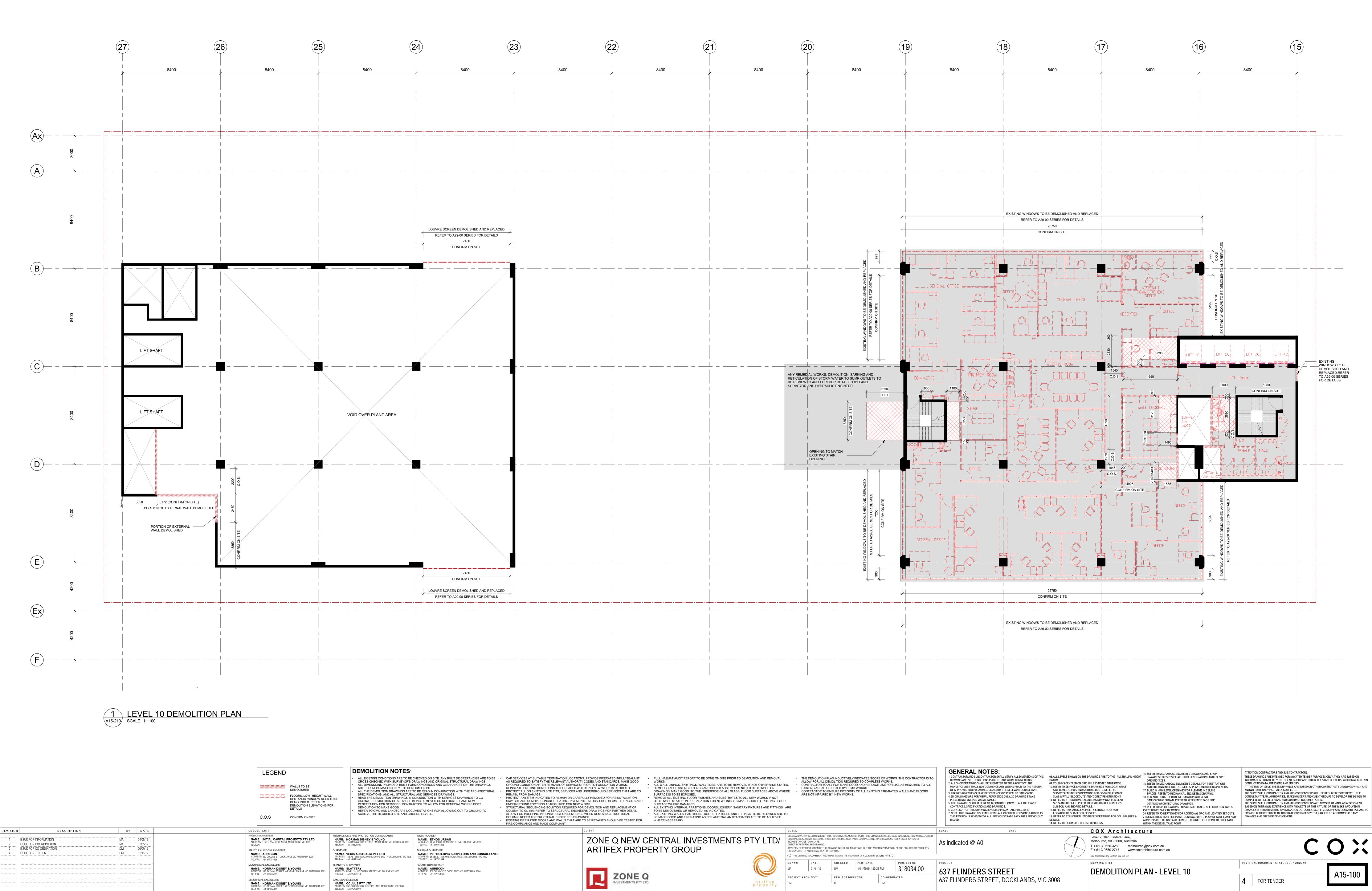
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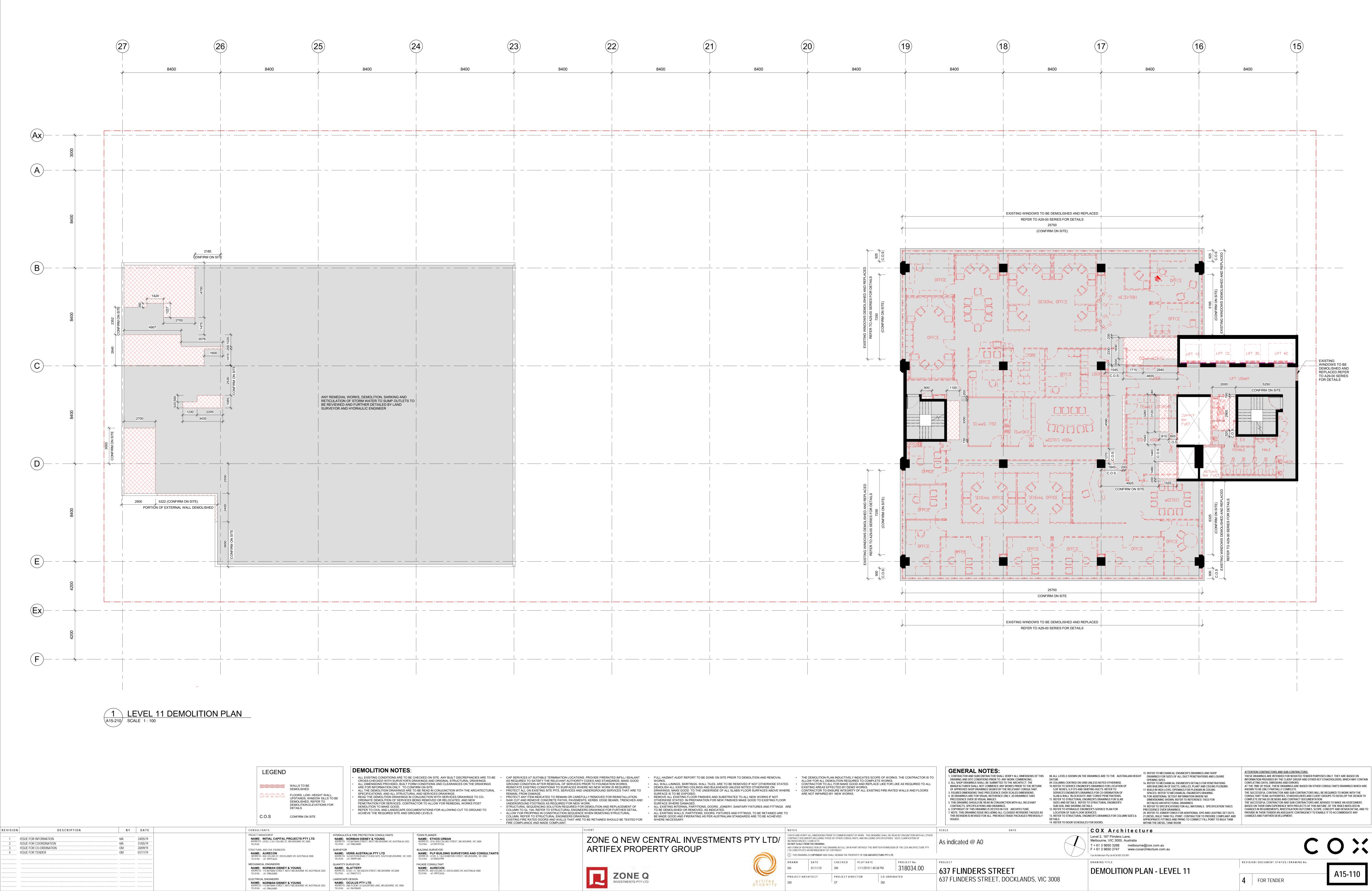
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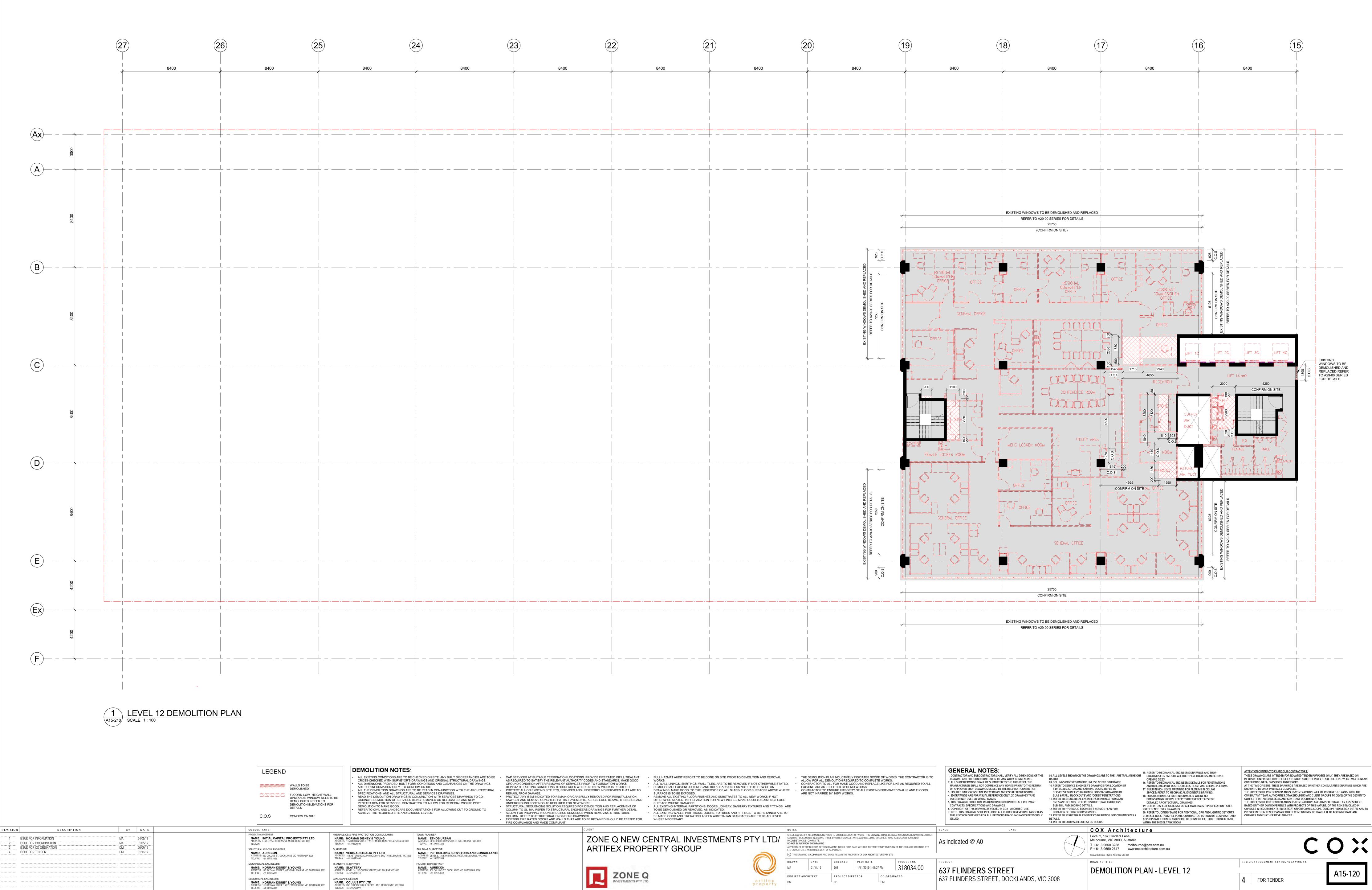


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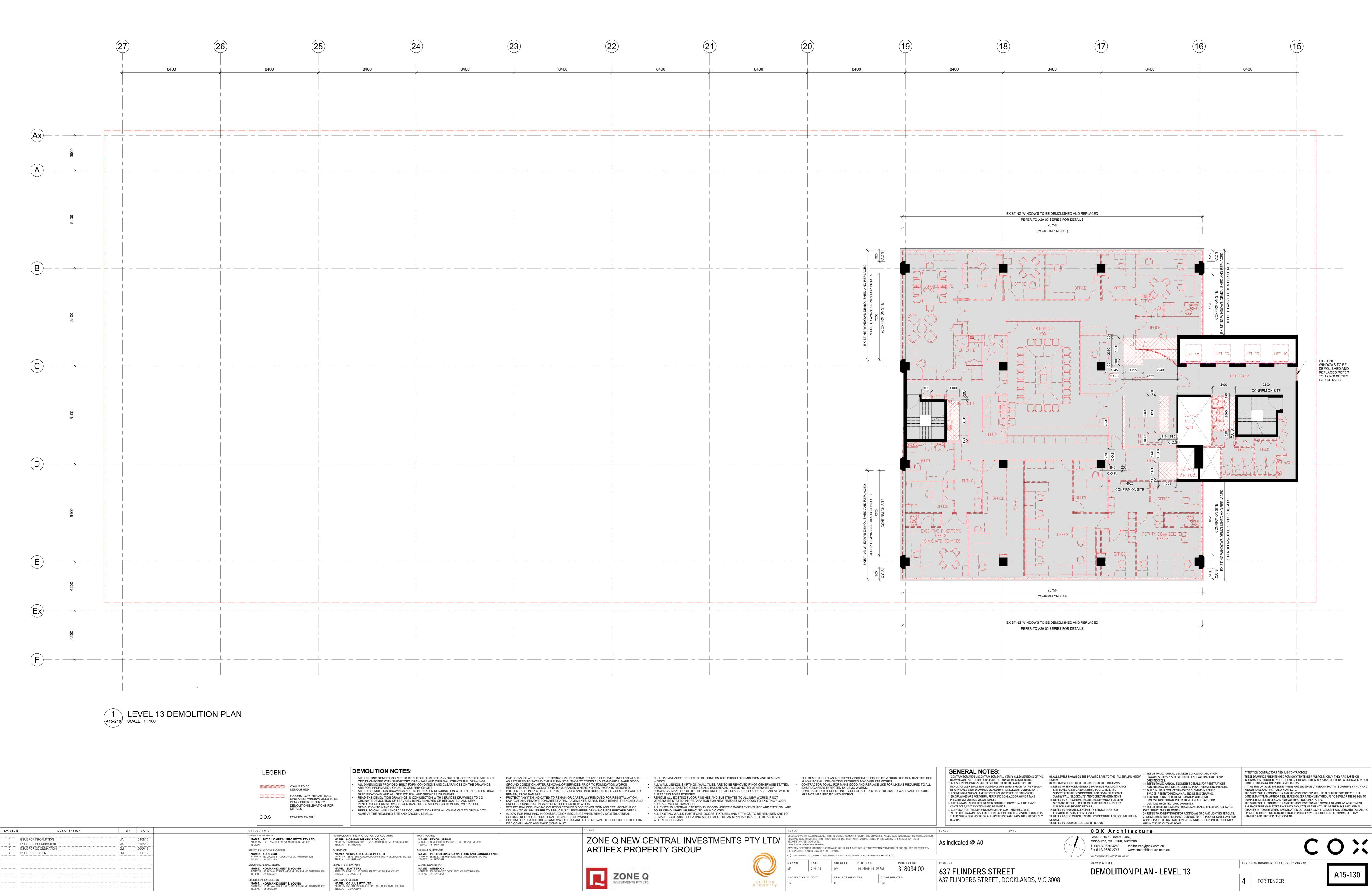
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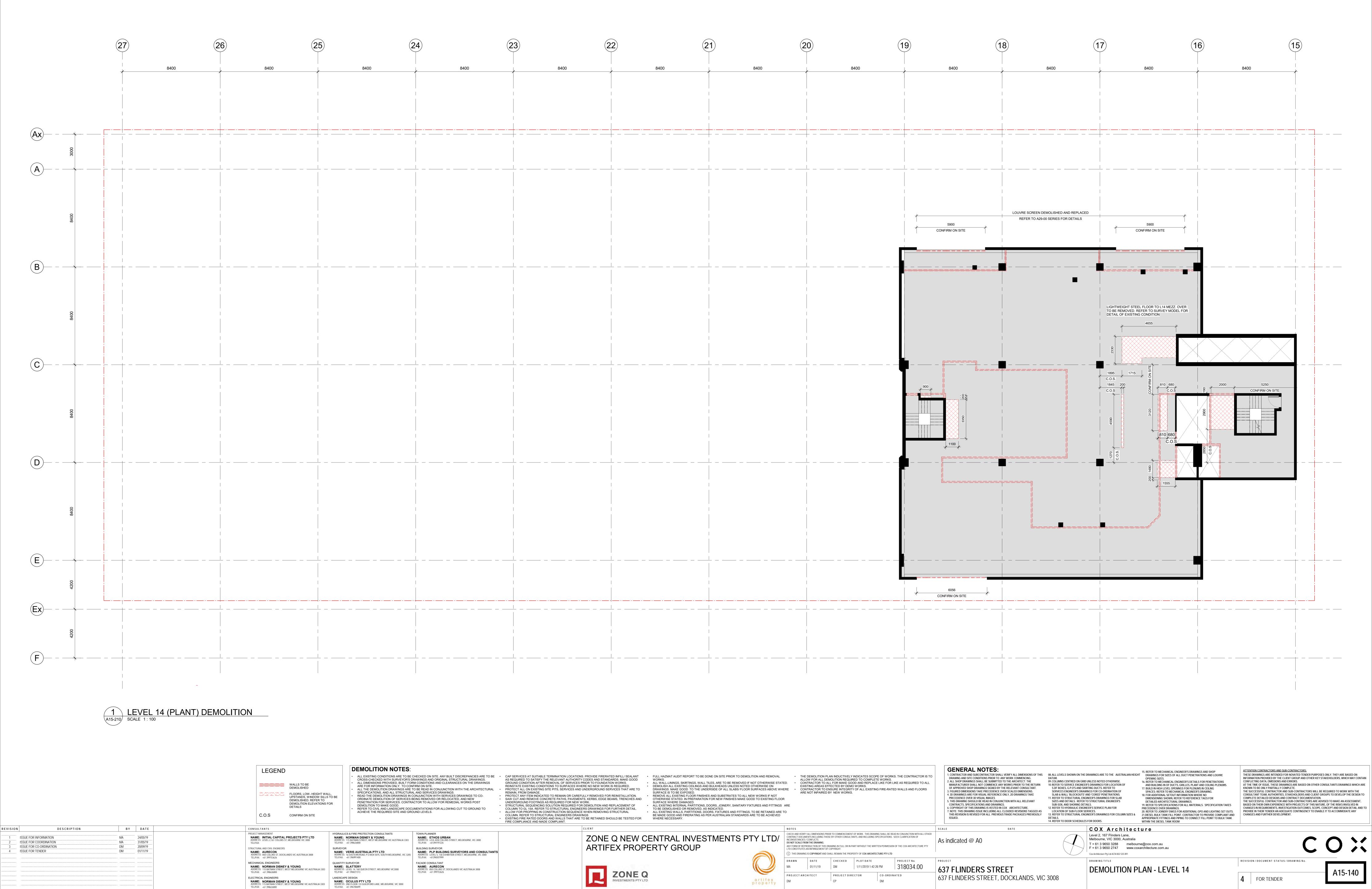
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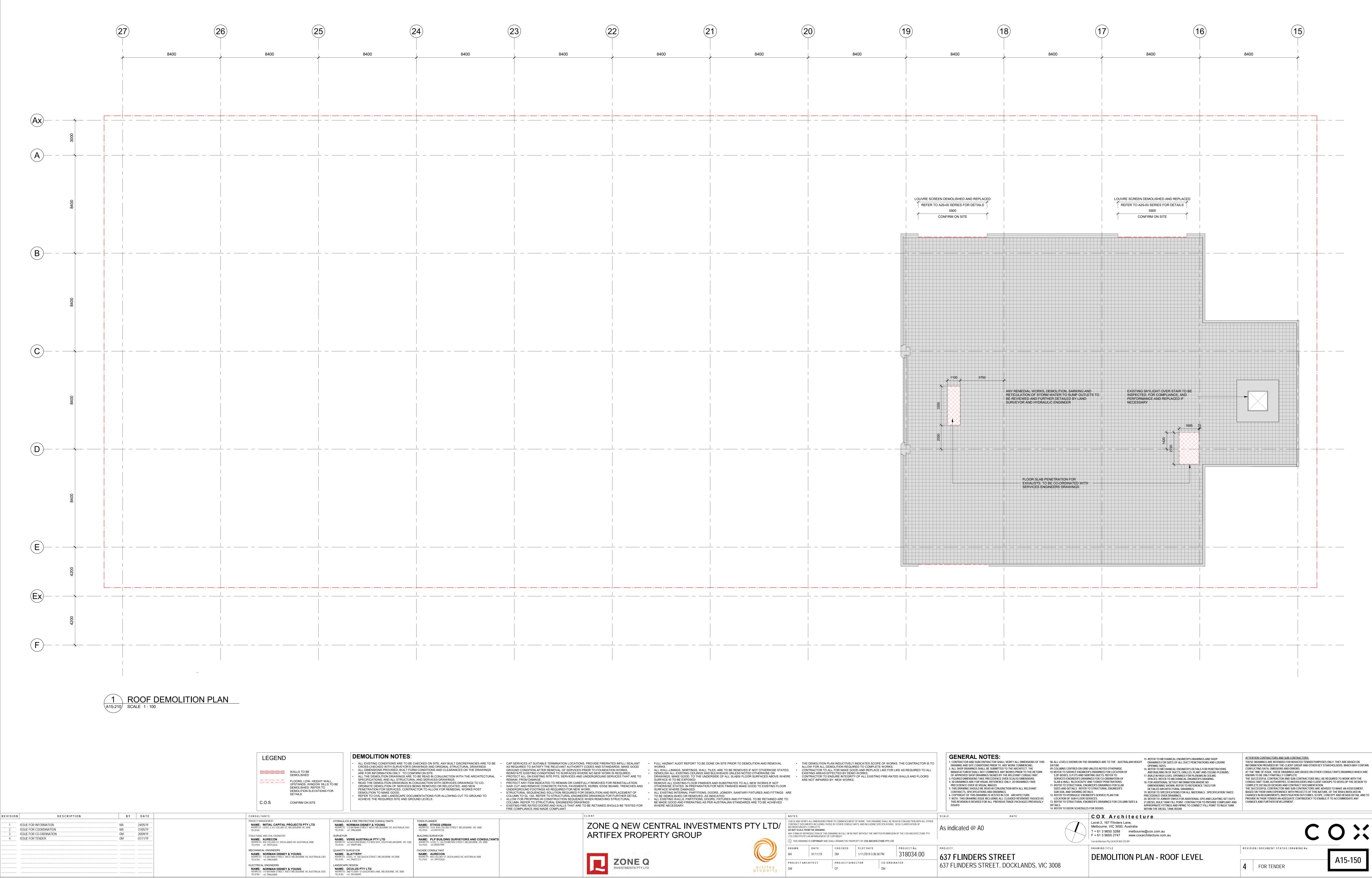
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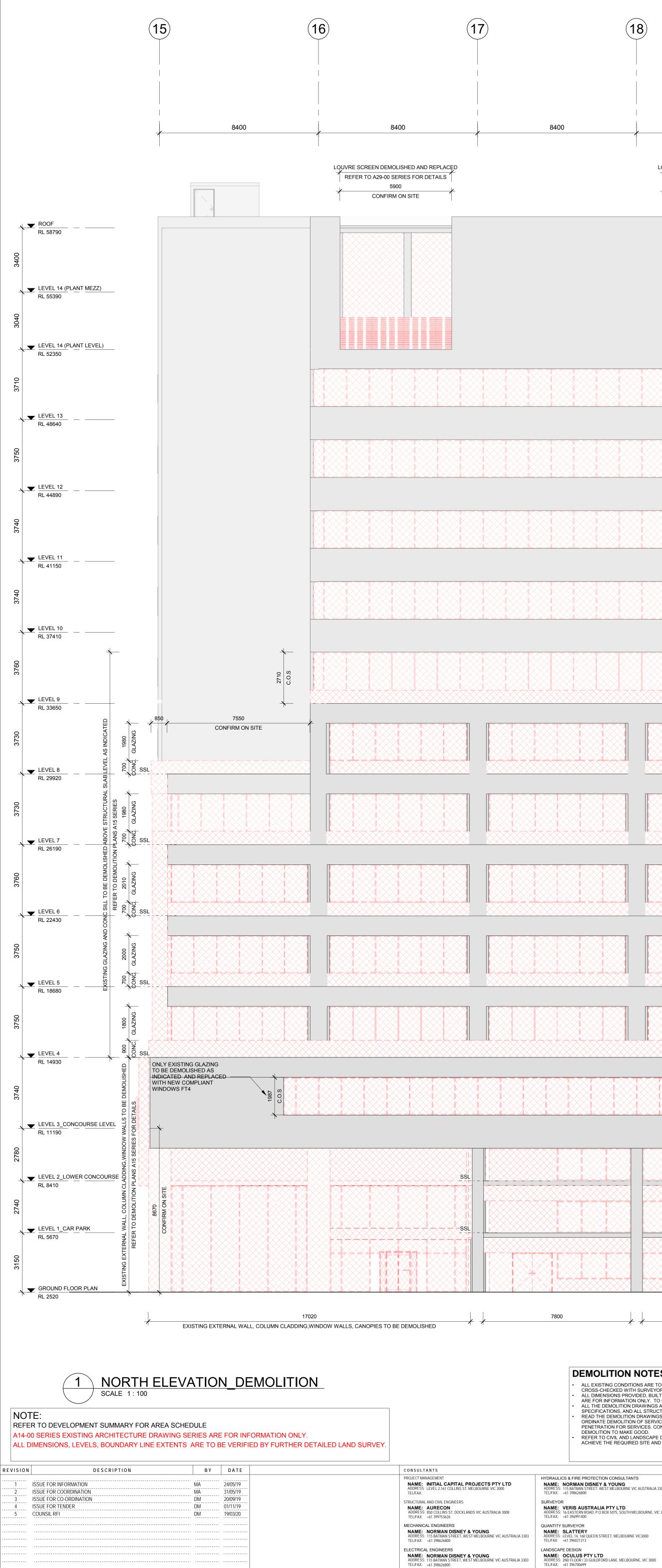
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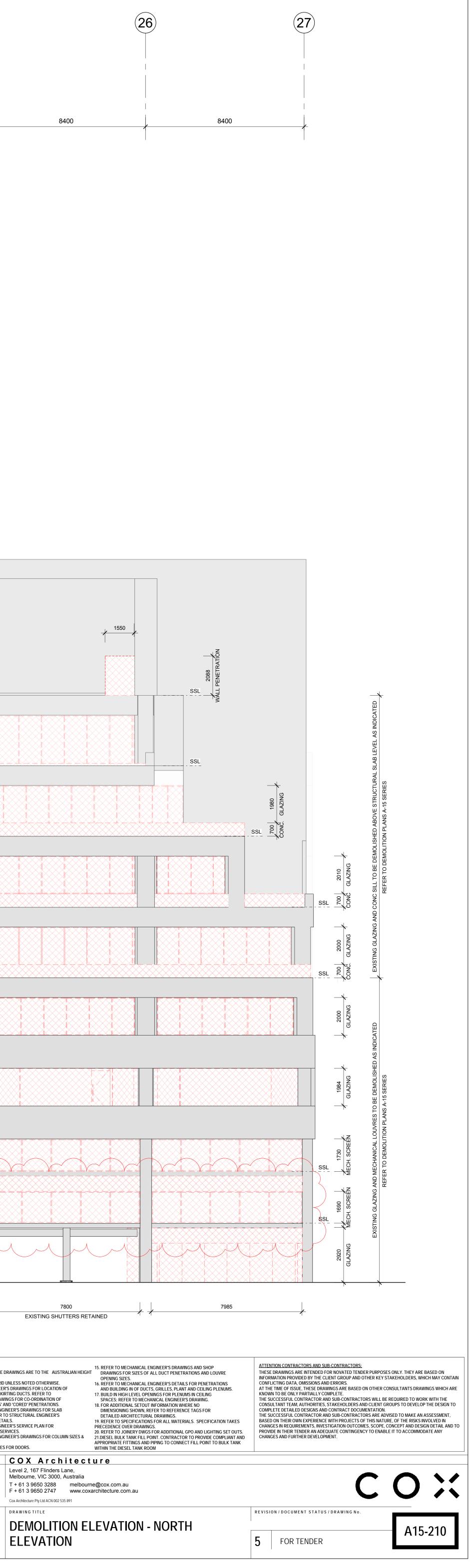
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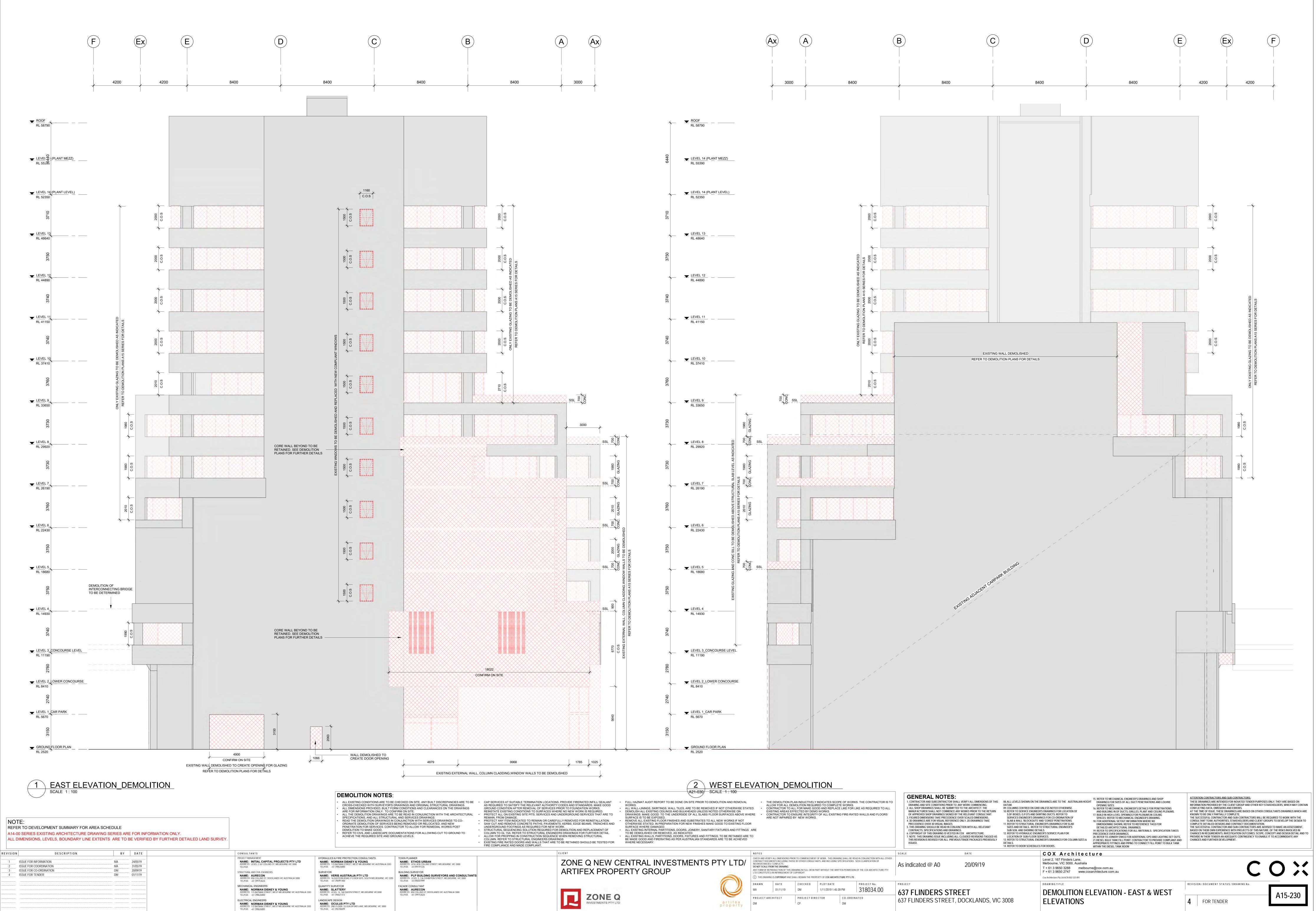
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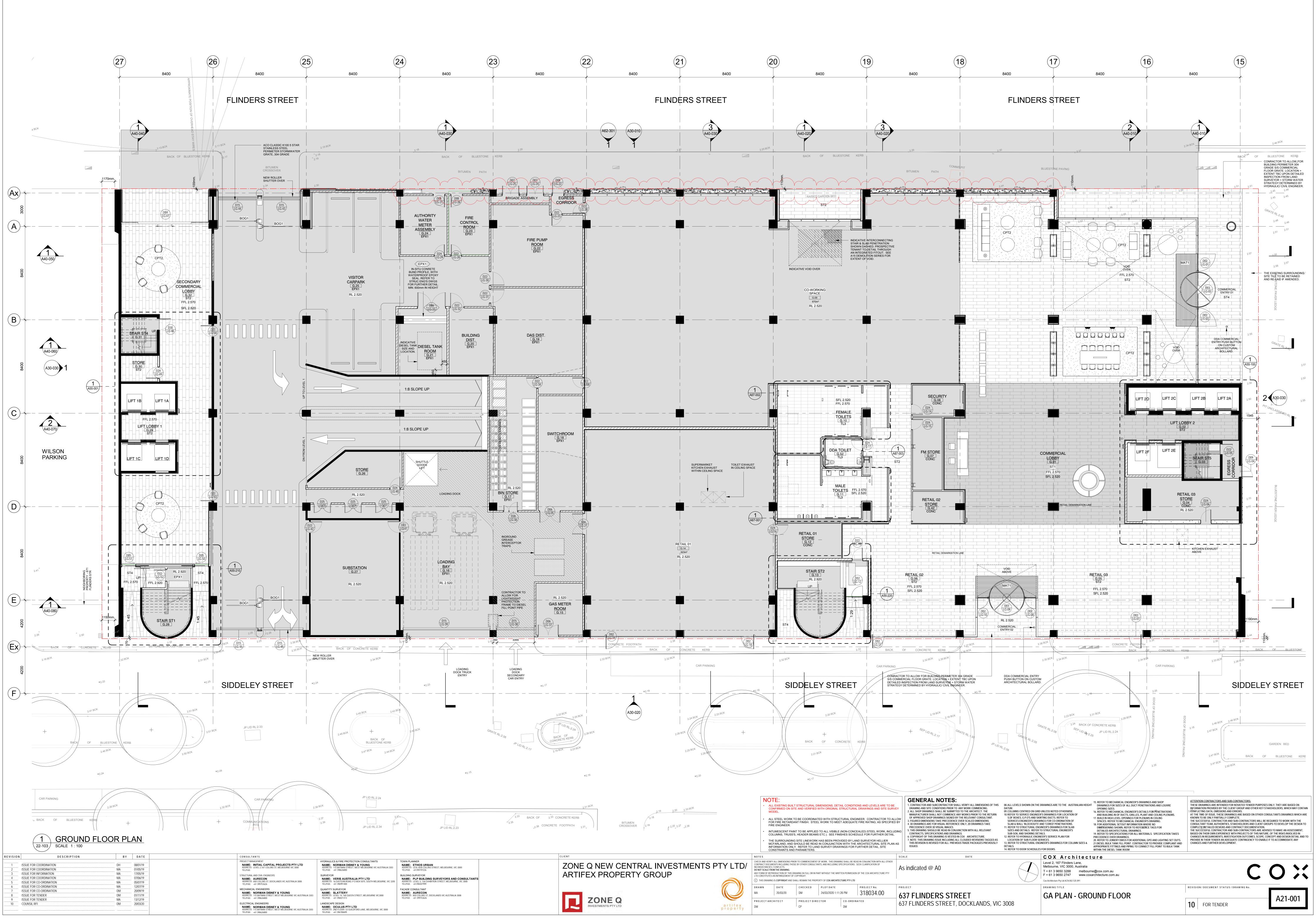
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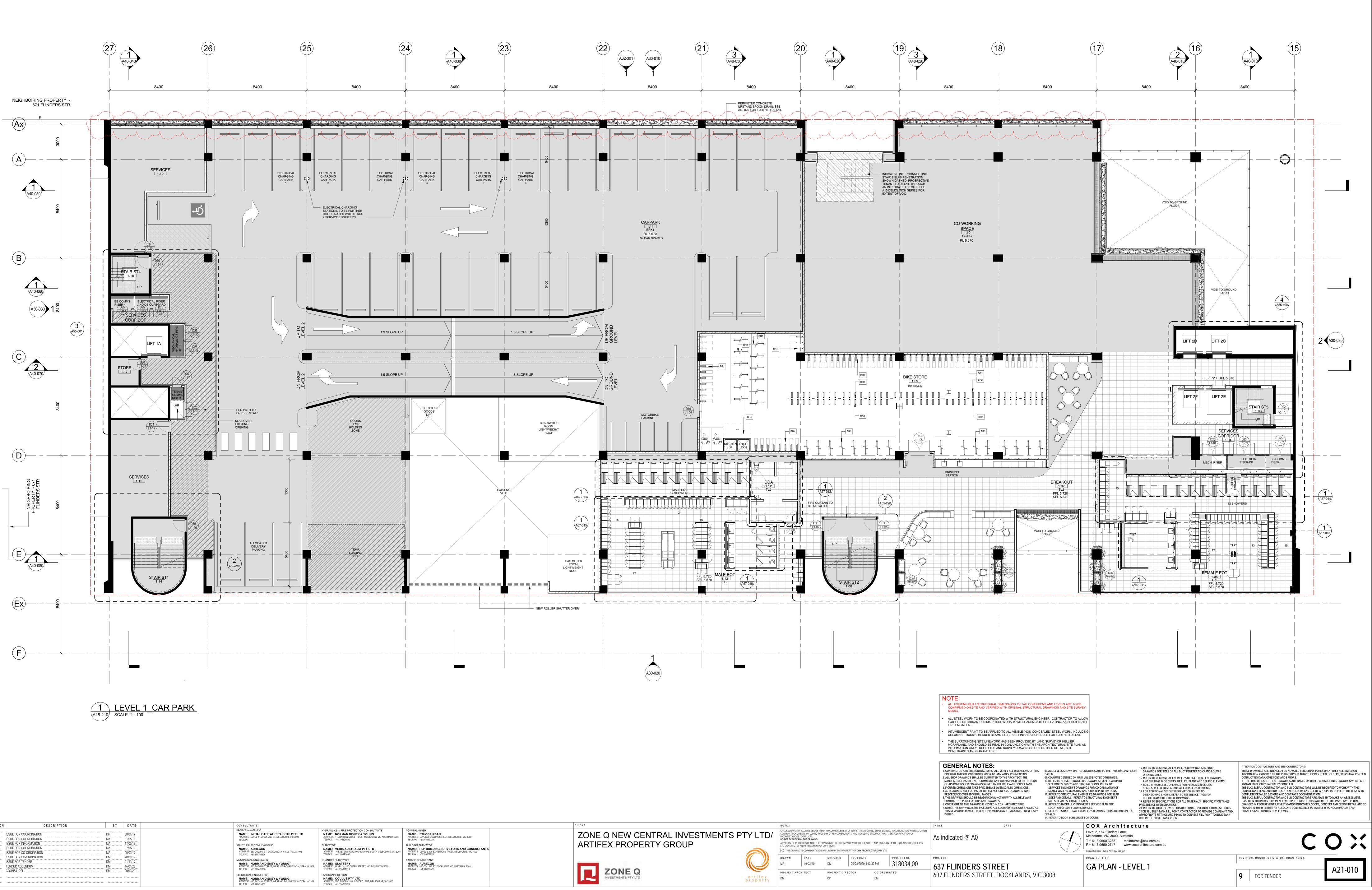
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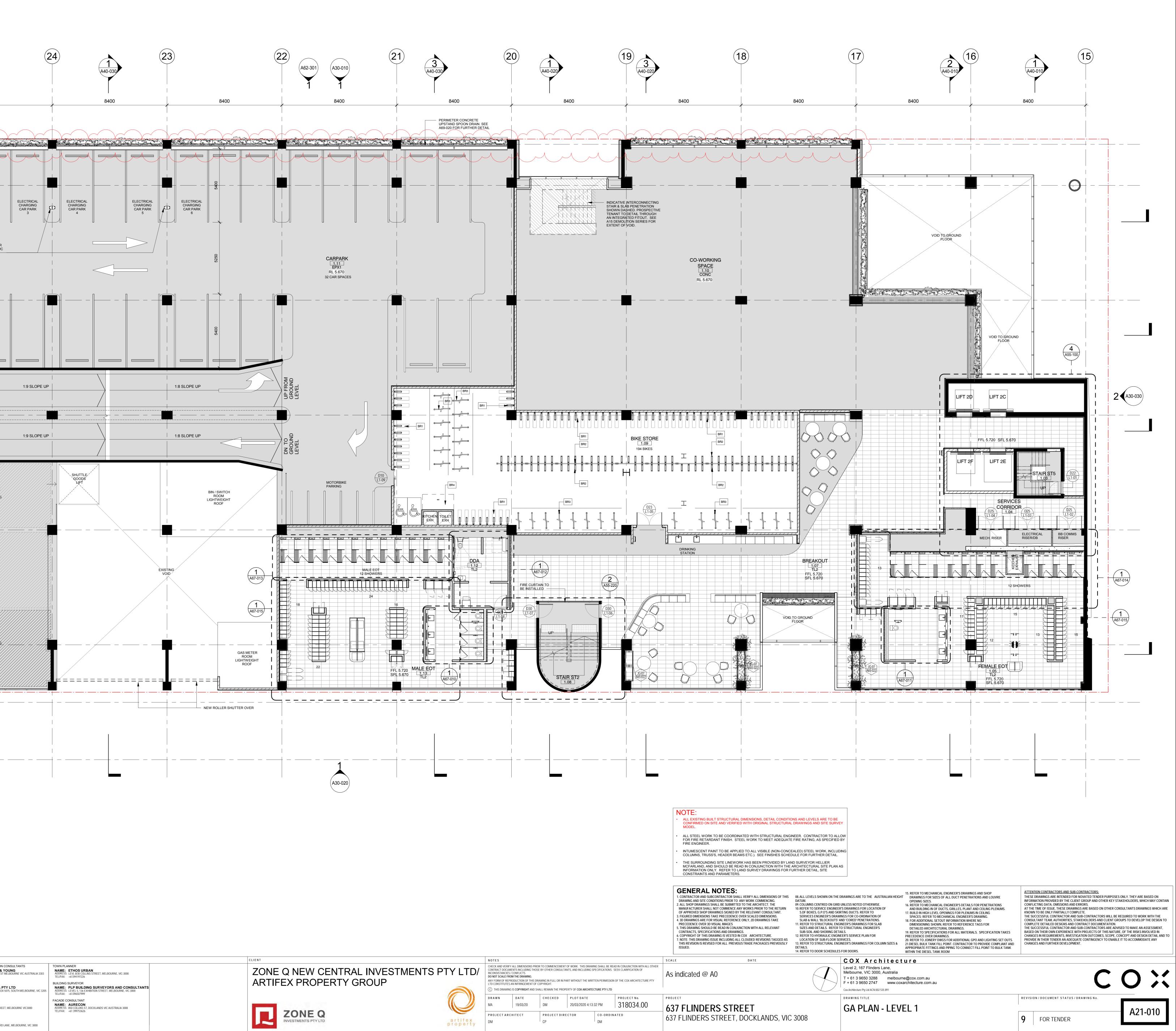


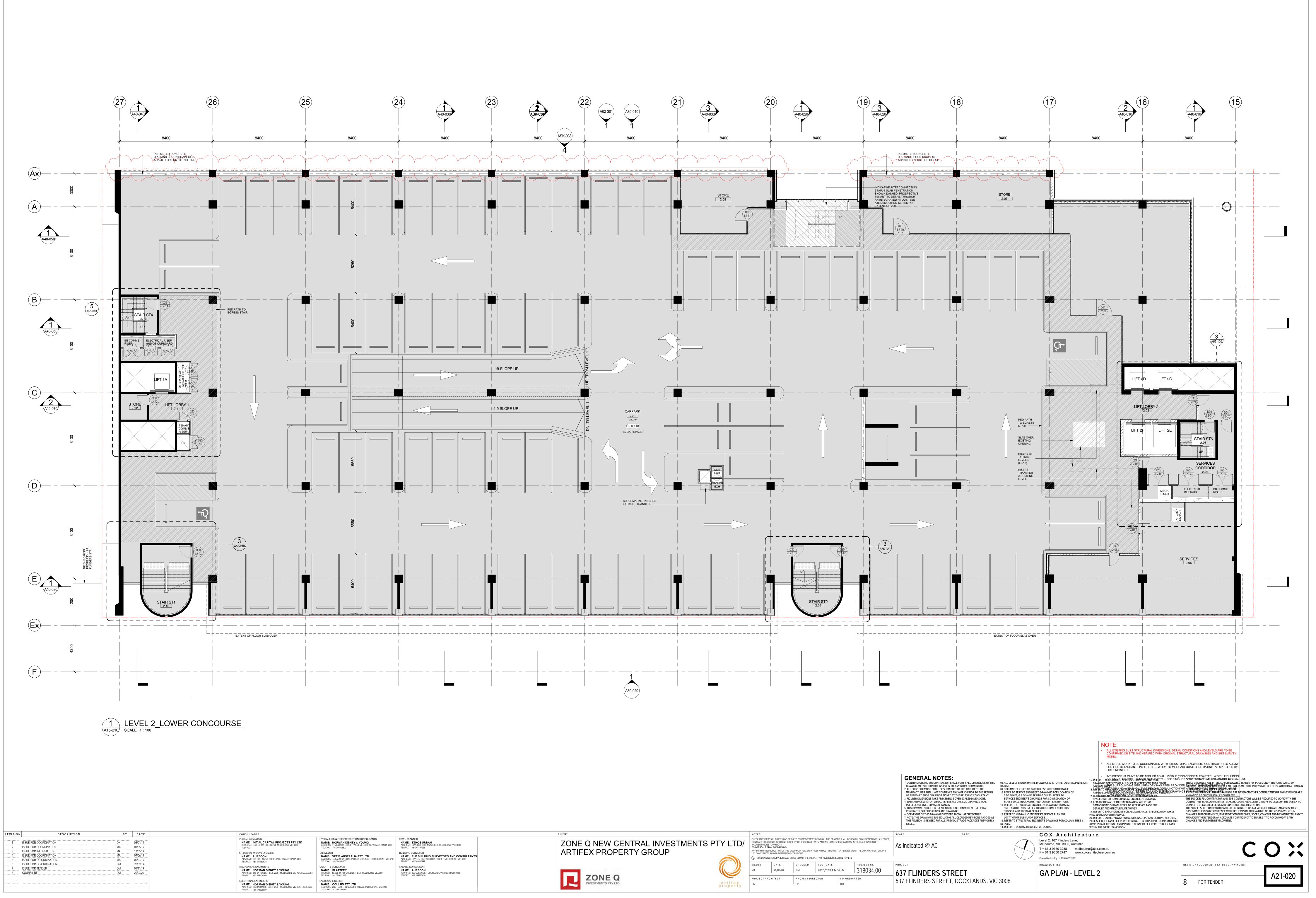
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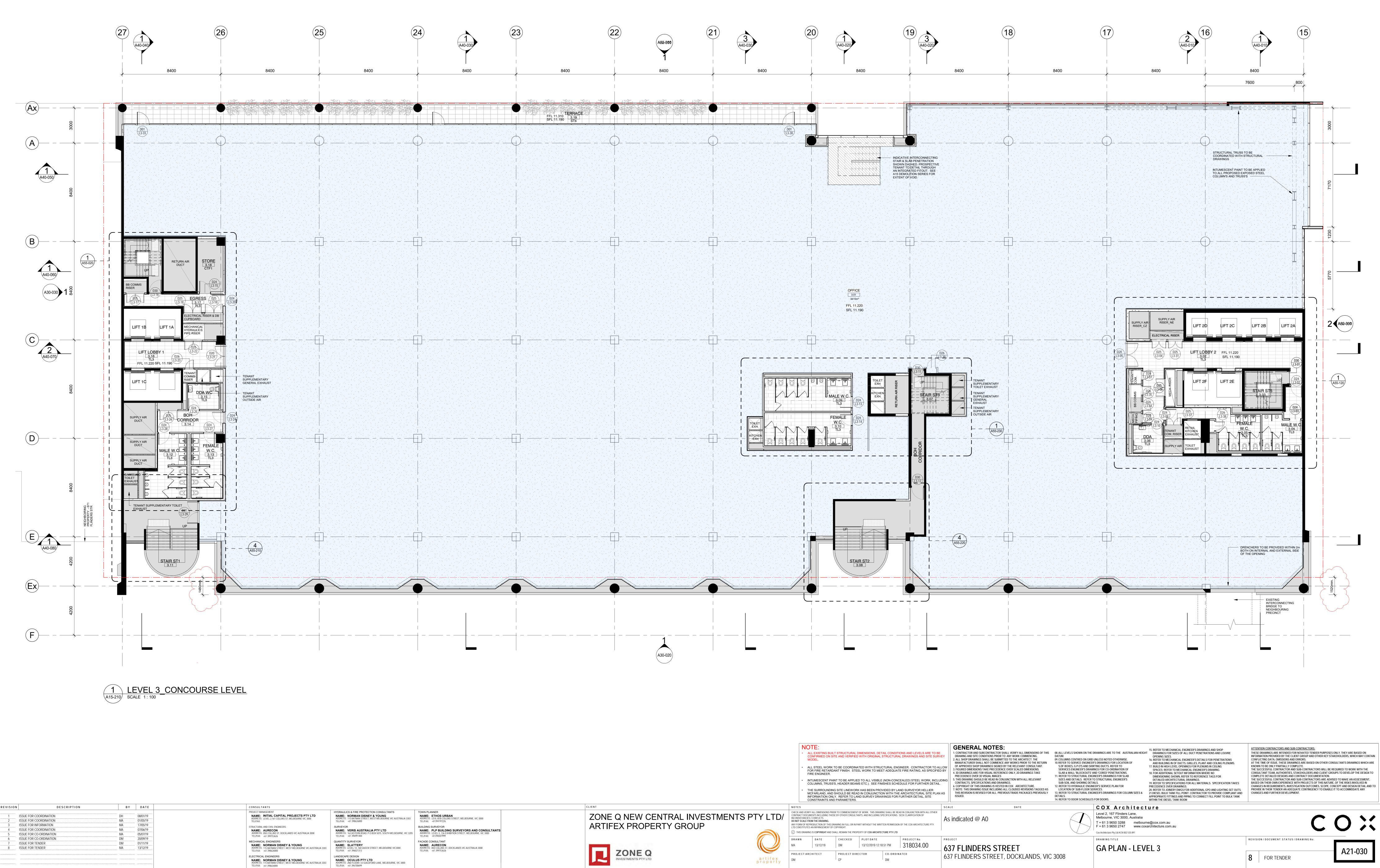


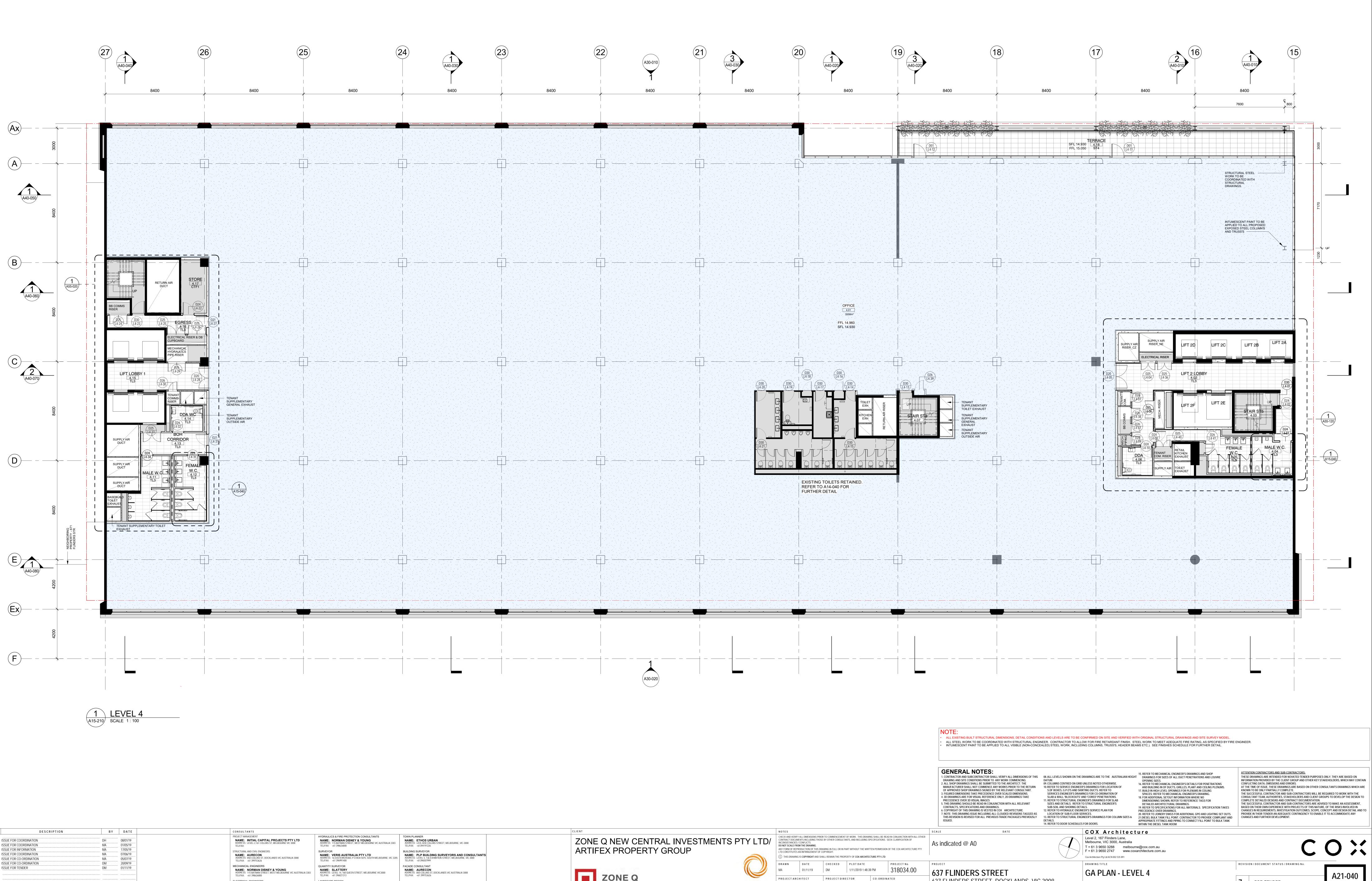


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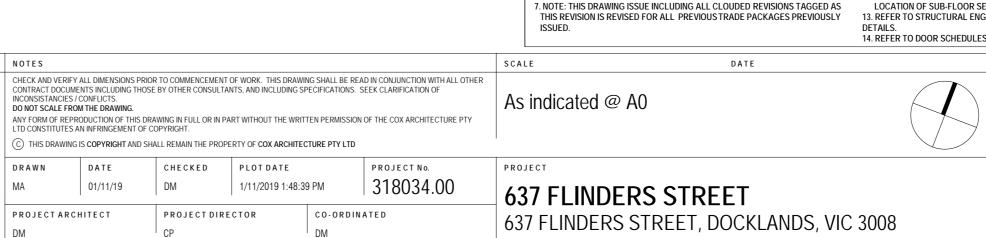




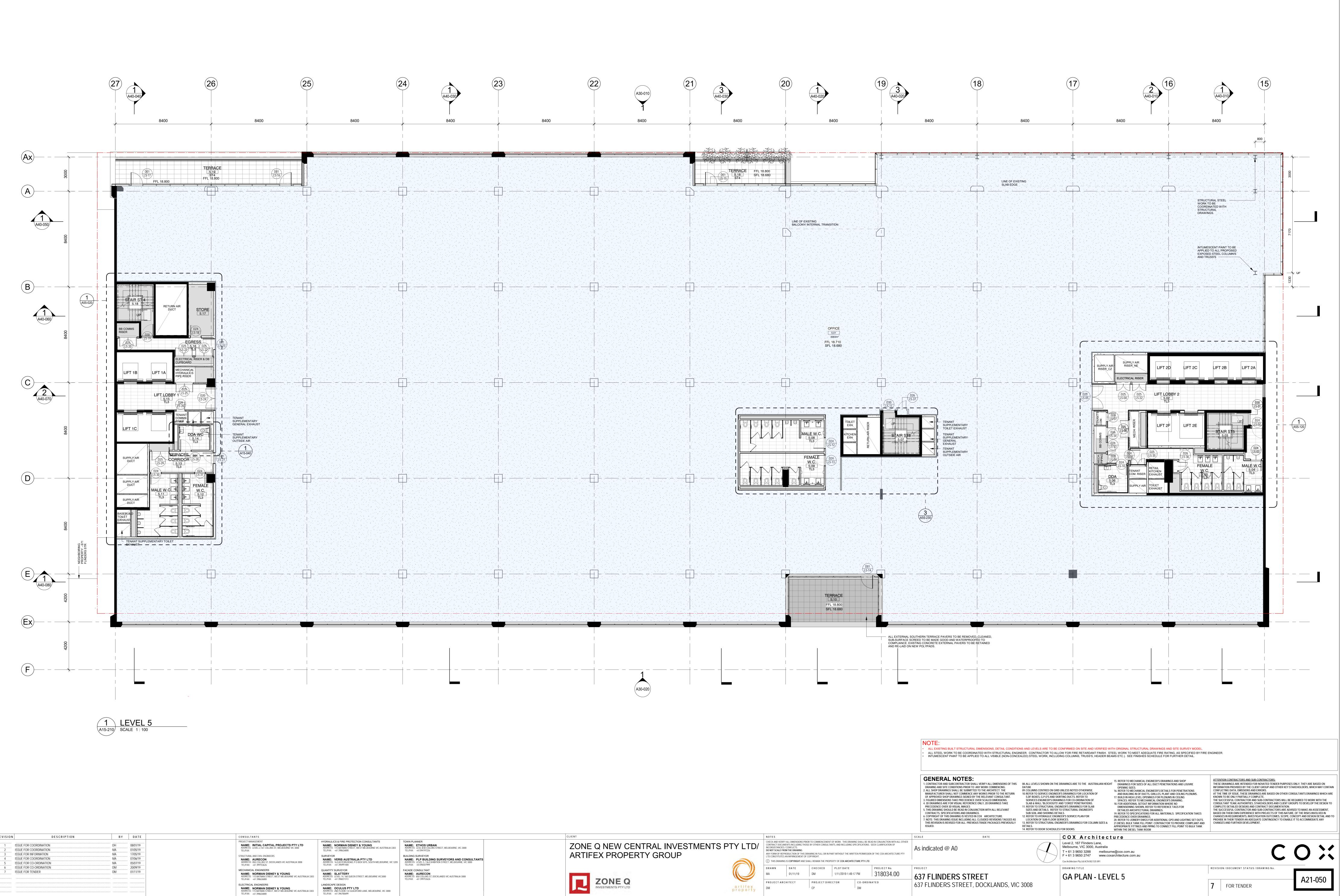
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REVISION	DESCRIPTION	BY	DATE	CONSULTANTS	
				PROJECT MANAGEMENT	HYDRAULICS & FIRE PROTECTION CONSULTANTS
1	ISSUE FOR COORDINATION	DH	08/01/19	NAME: INITIAL CAPITAL PROJECTS PTY LTD	NAME: NORMAN DISNEY & YOUNG
	ISSUE FOR COORDINATION	MA	01/05/19	ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX:	ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800
	ISSUE FOR INFORMATION	MA	17/05/19	STRUCTURAL AND CIVIL ENGINEERS	SURVEYOR
4	ISSUE FOR COORDINATION	MA	07/06/19	NAME: AURECON	NAME: VERIS AUSTRALIA PTY LTD
5	ISSUE FOR CO-ORDINATION	MA	05/07/19	ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 320 TEL/FAX: +61 396991400
6	ISSUE FOR CO-ORDINATION	DM	20/09/19		TEL/FAX. +01 390991400
7	ISSUE FOR TENDER	DM	01/11/19	MECHANICAL ENGINEERS	QUANTITY SURVEYOR
				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000 TEL/FAX: +61 396021313
				ELECTRICAL ENGINEERS	LANDSCAPE DESIGN
				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699



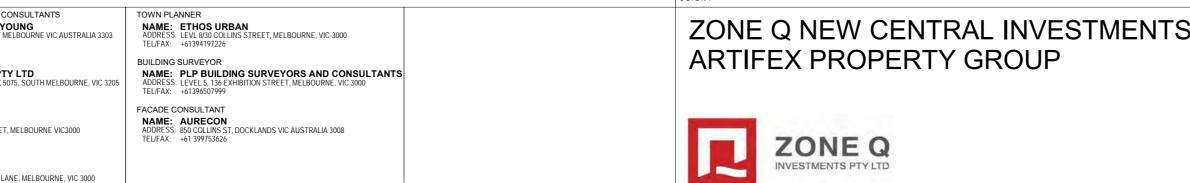


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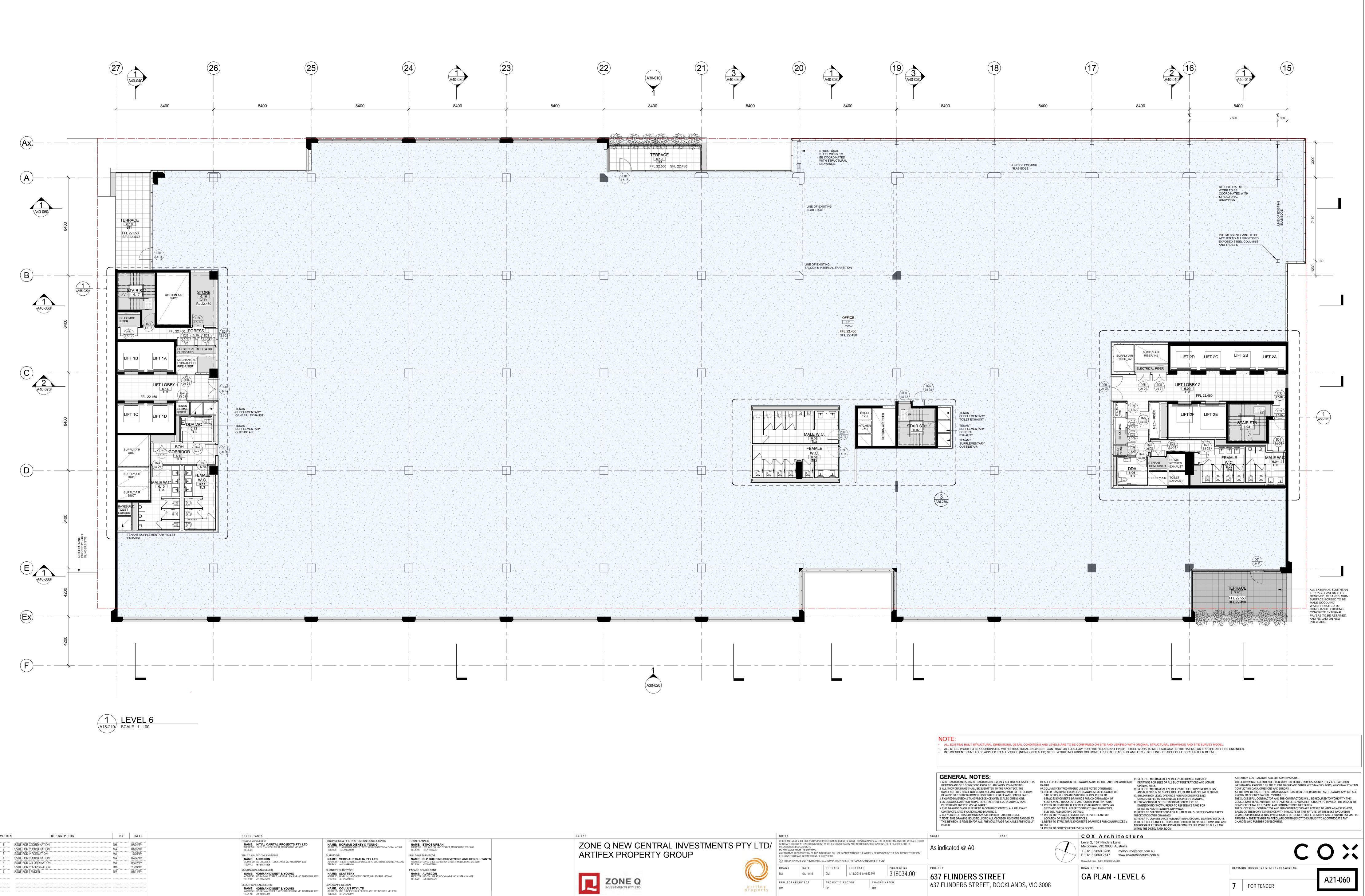
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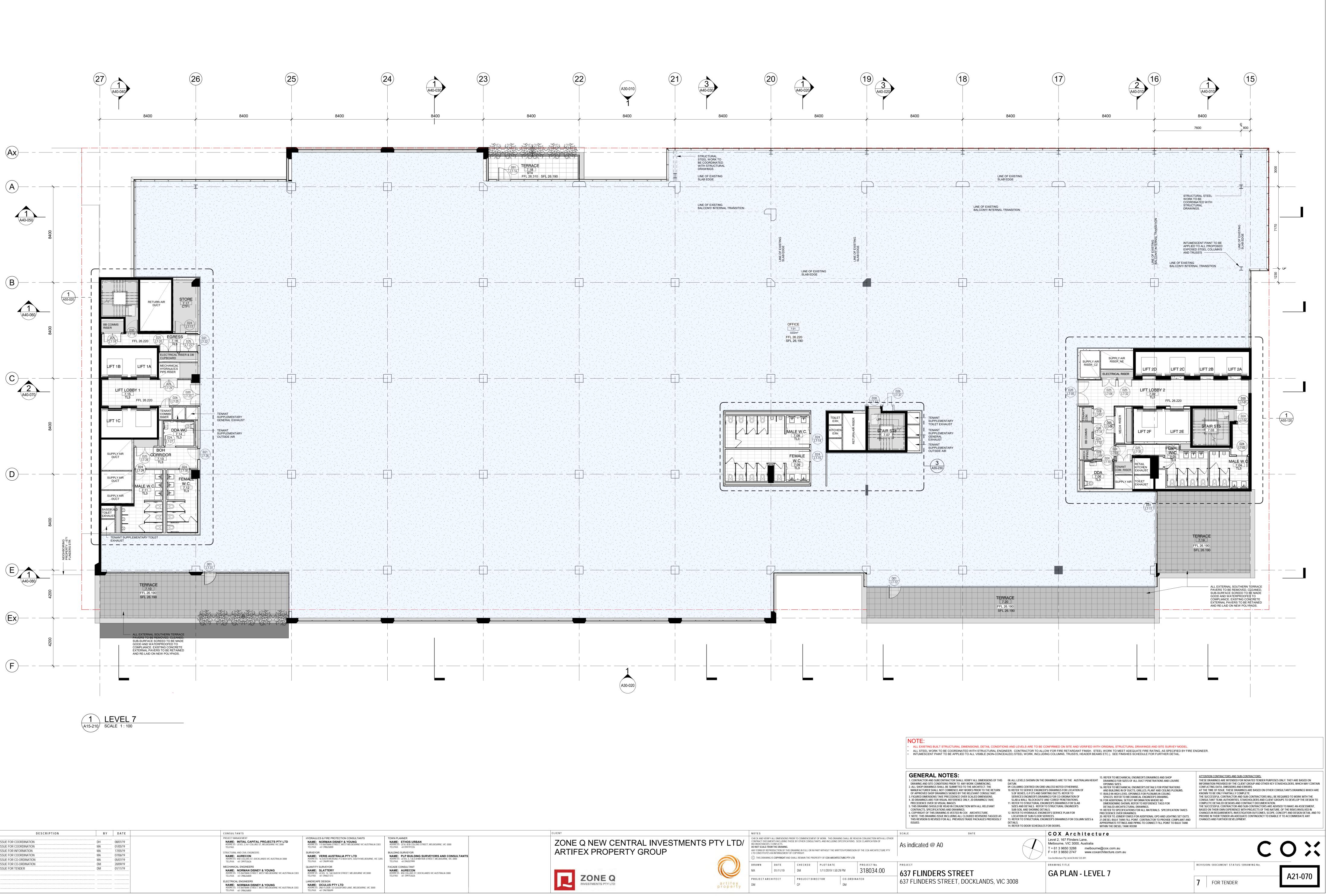


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NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699

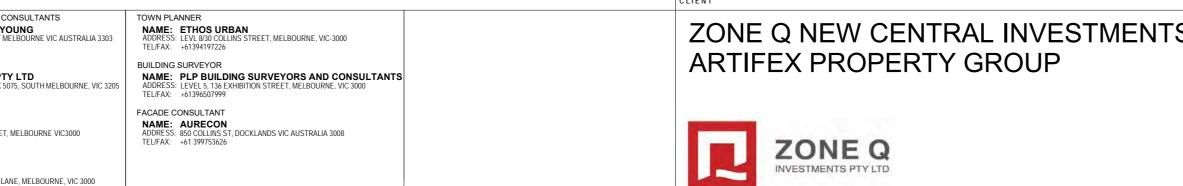


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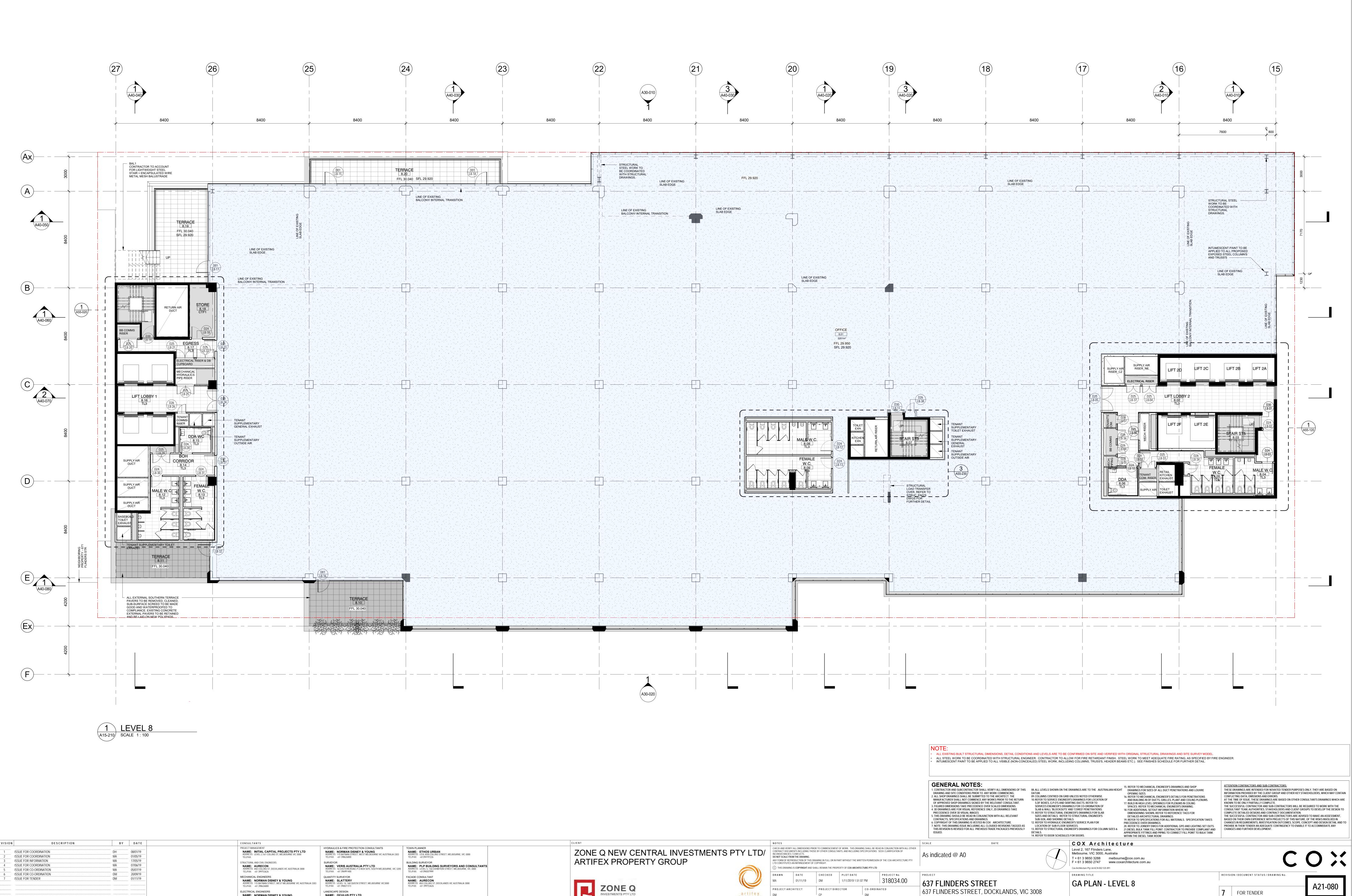
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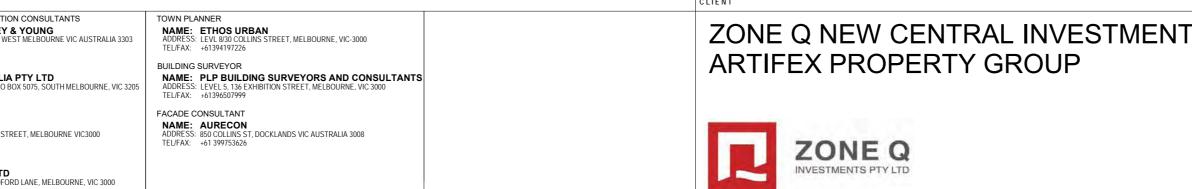




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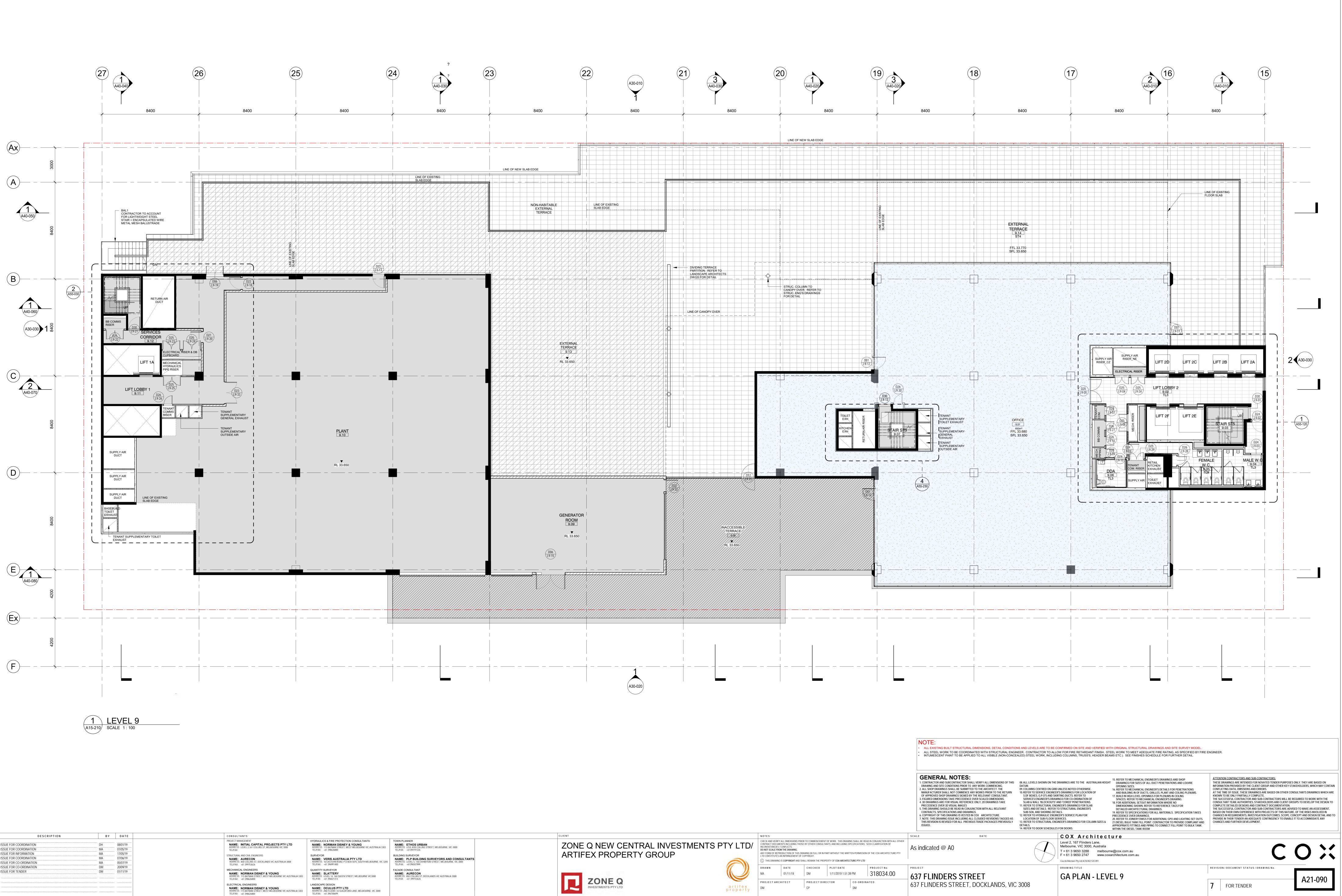


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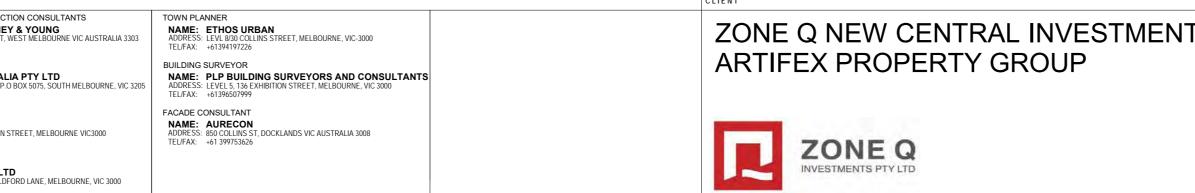




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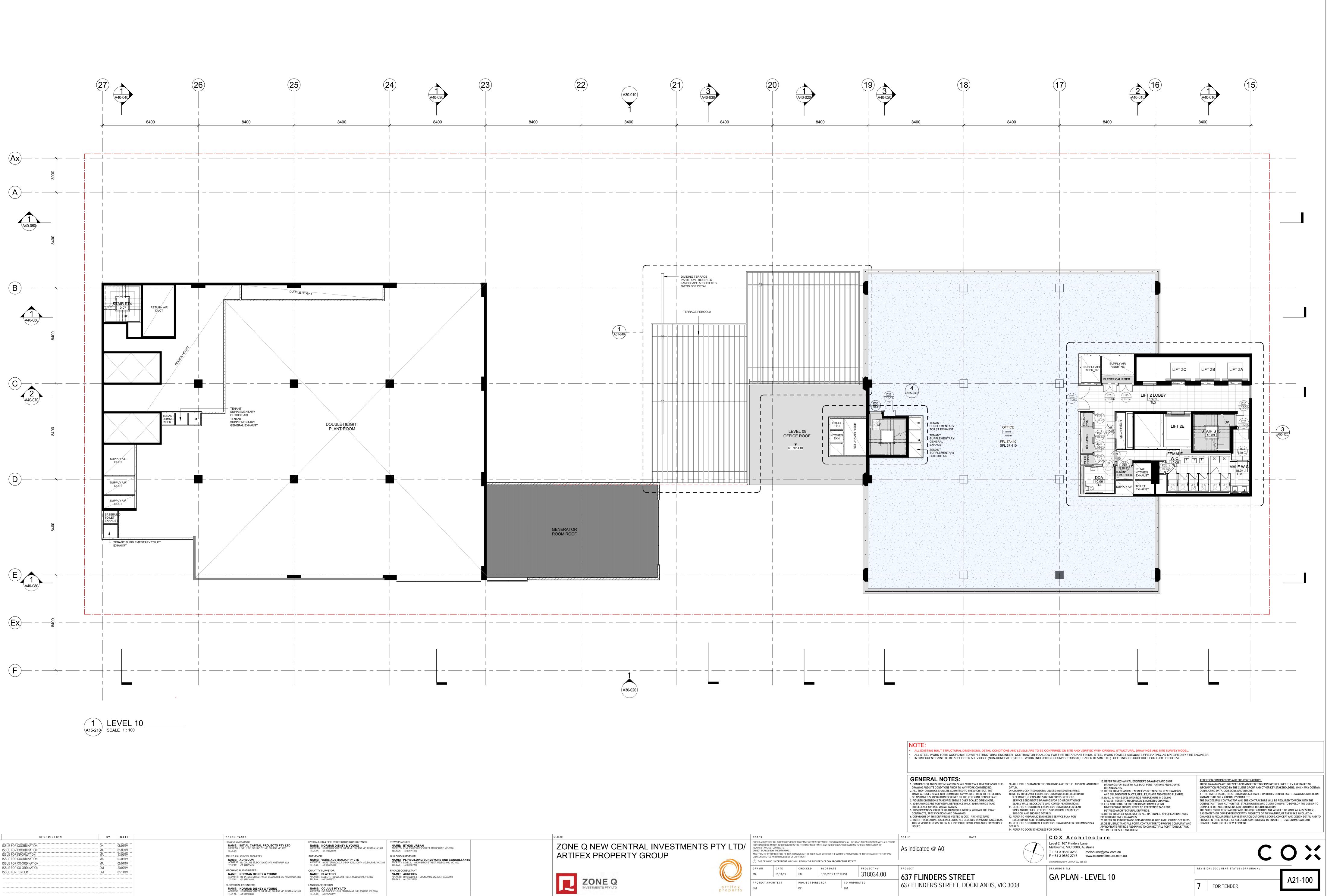


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				TEL/FAX: +61 398626800	TEL/FAX
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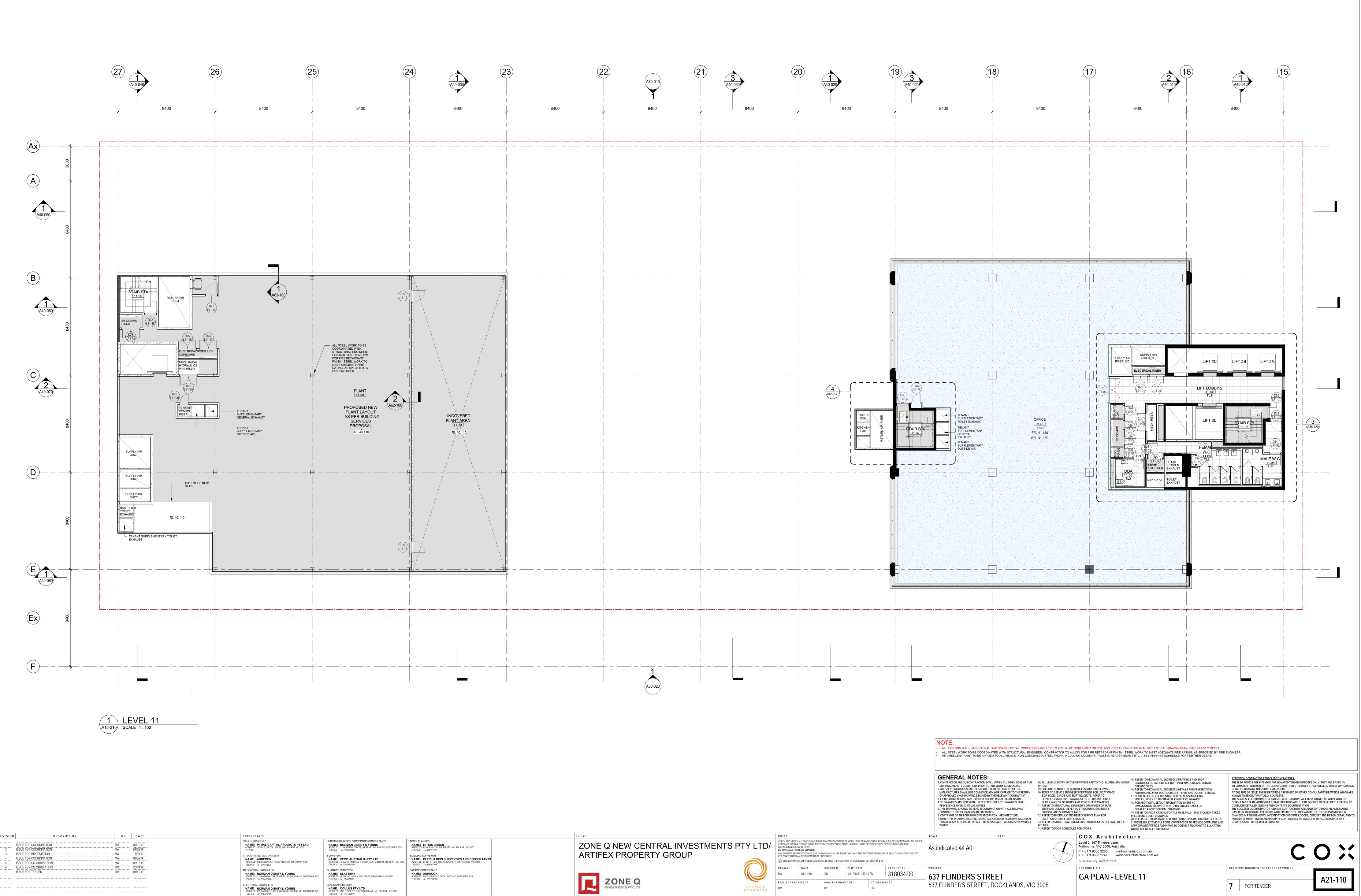
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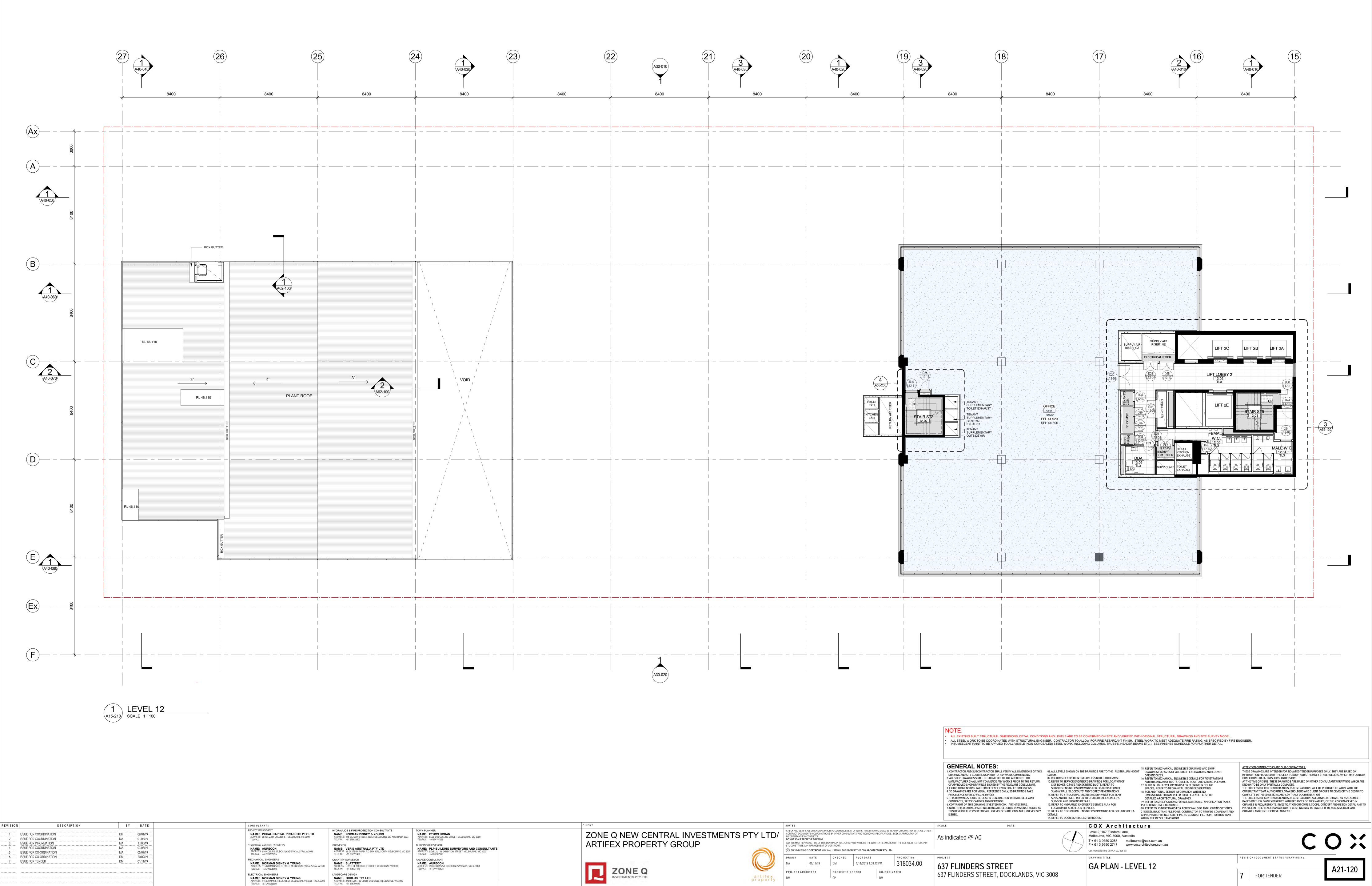
REVISION

NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800

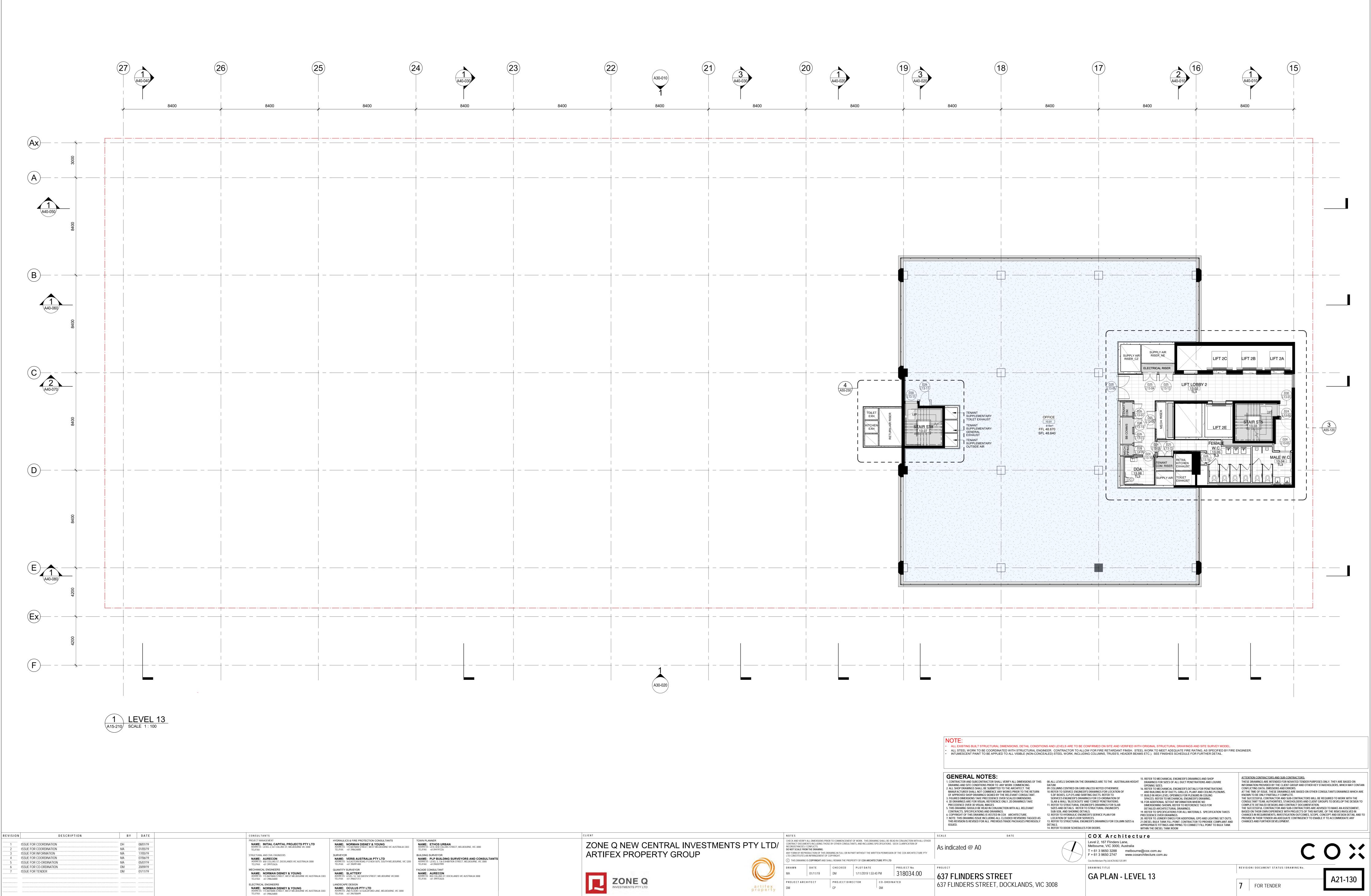




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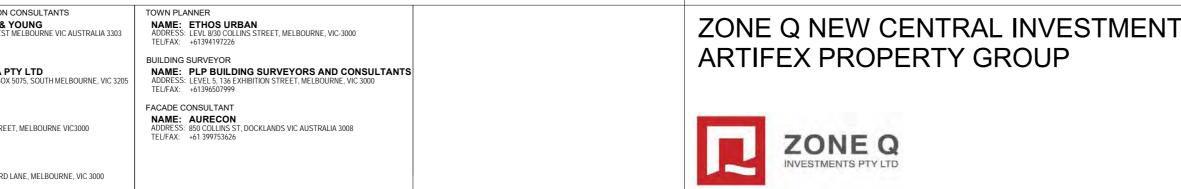
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6	ISSUE FOR CO-ORDINATION	DM	20/09/19
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NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800

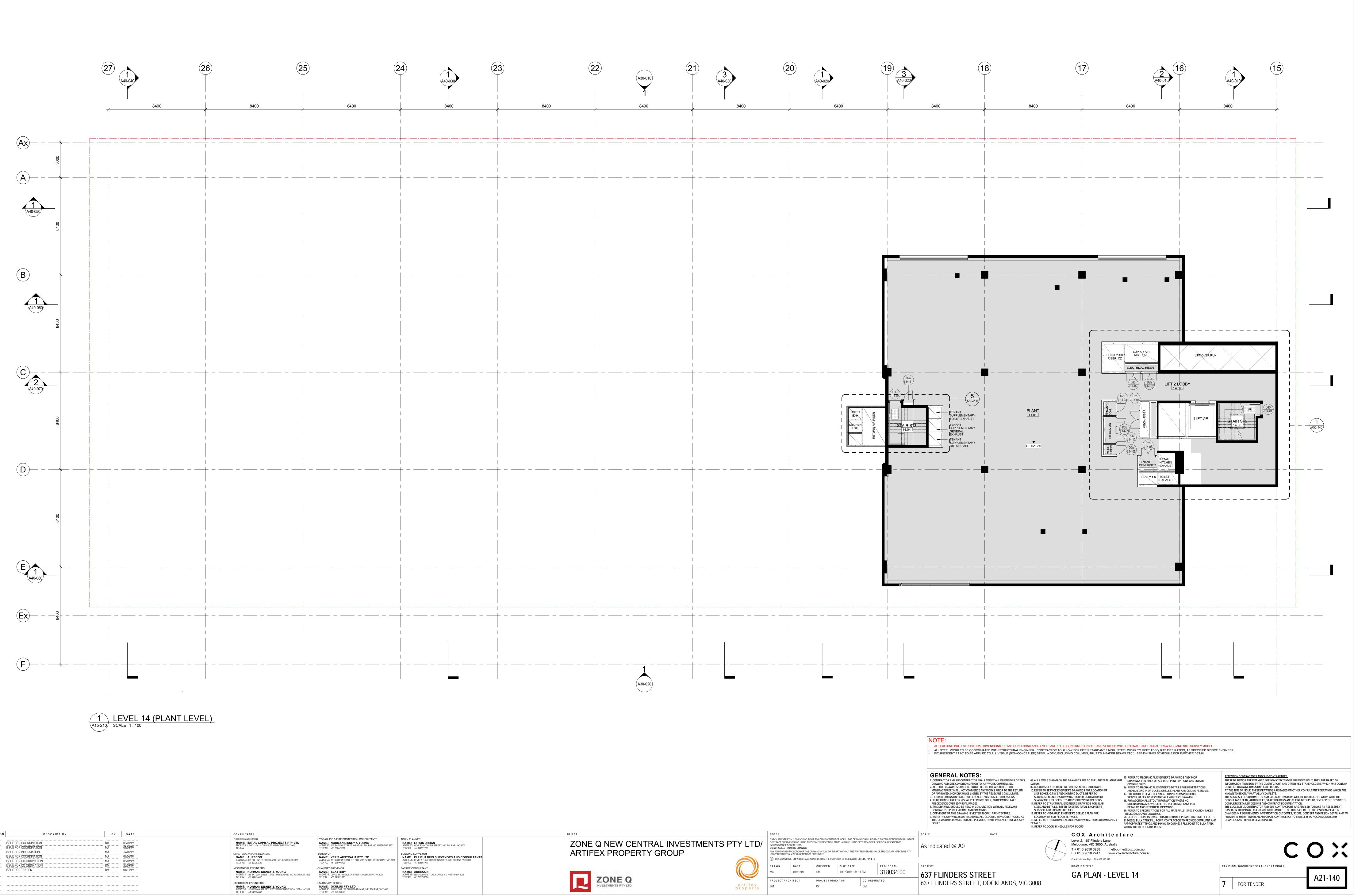




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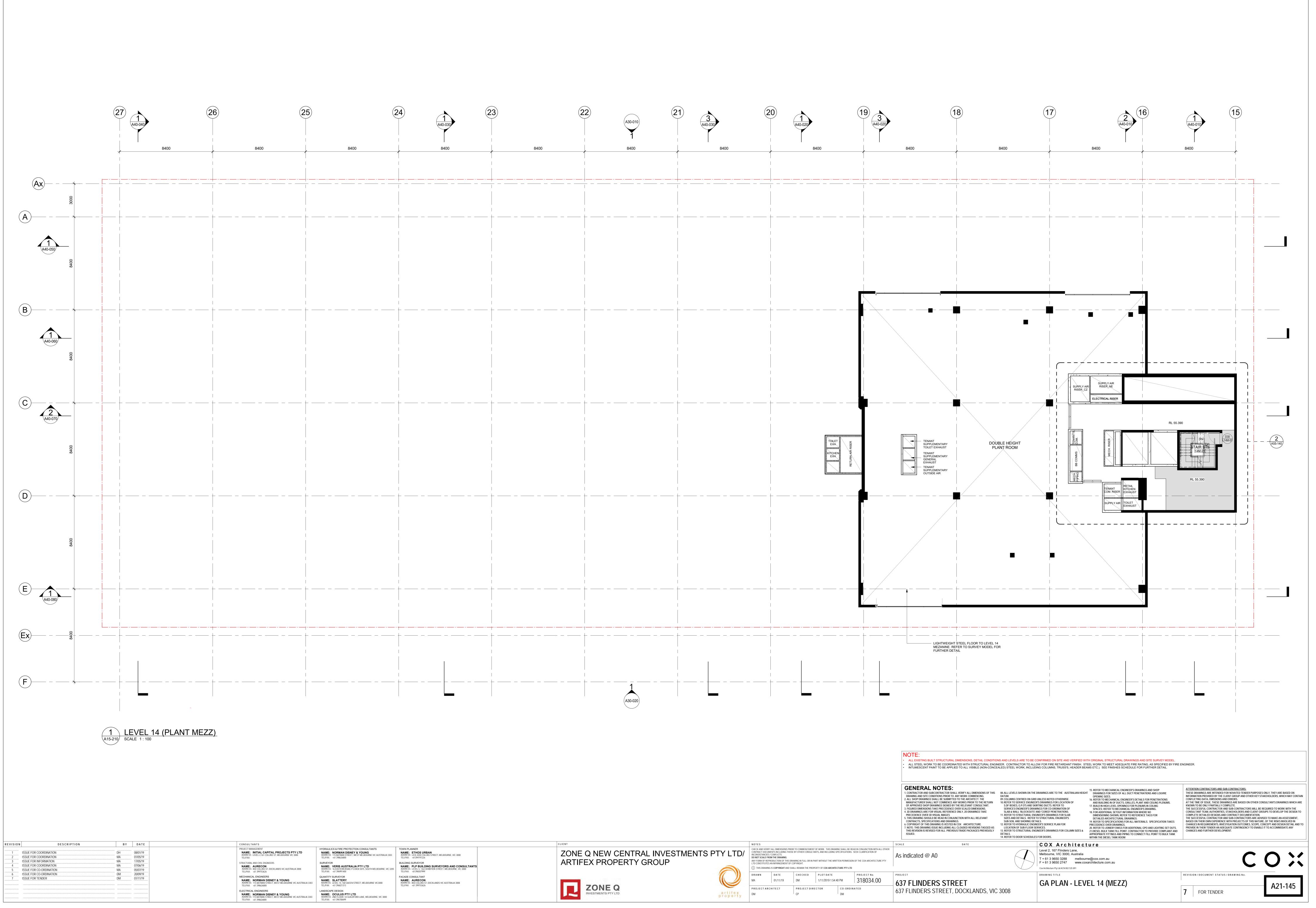


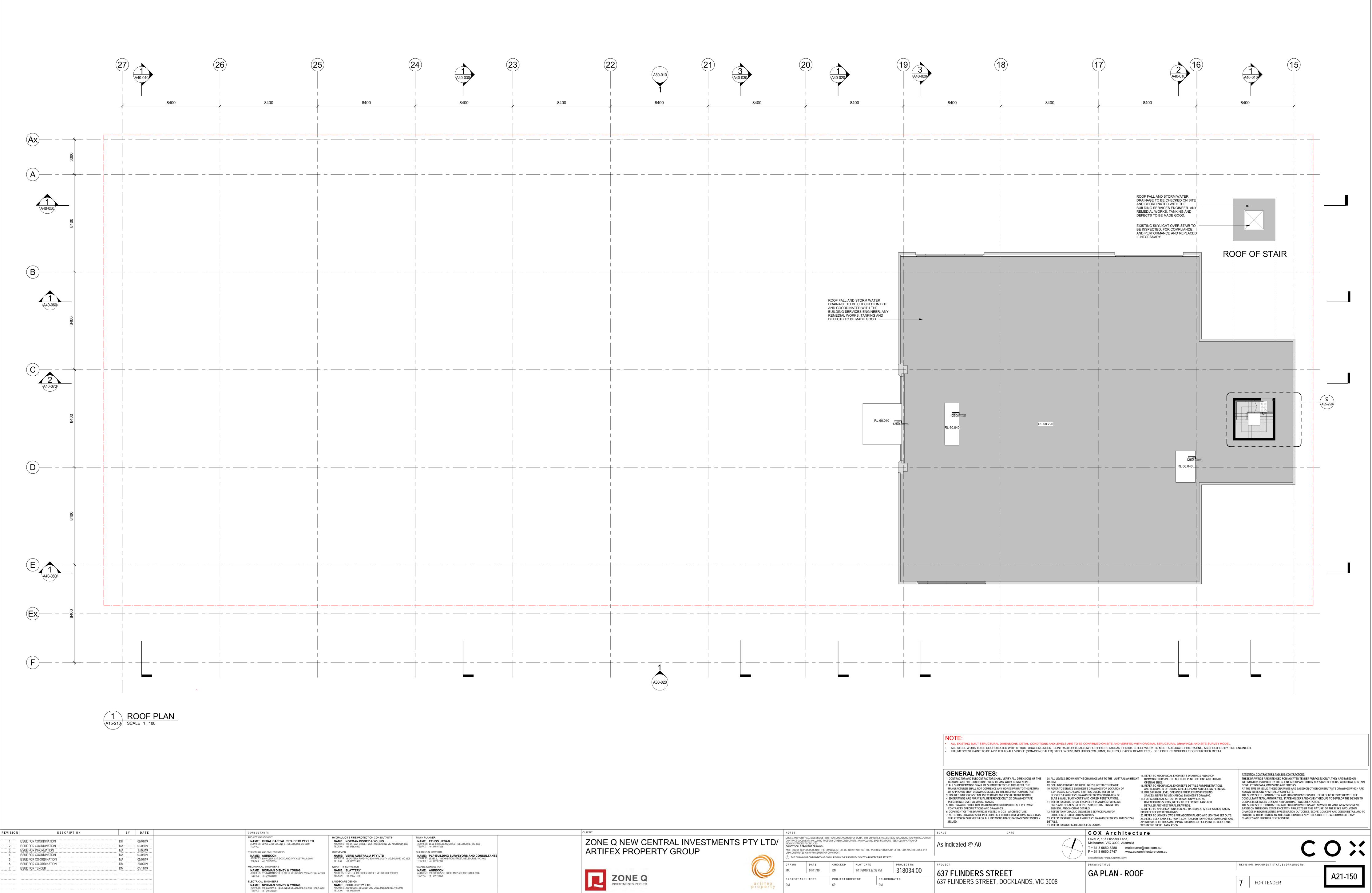
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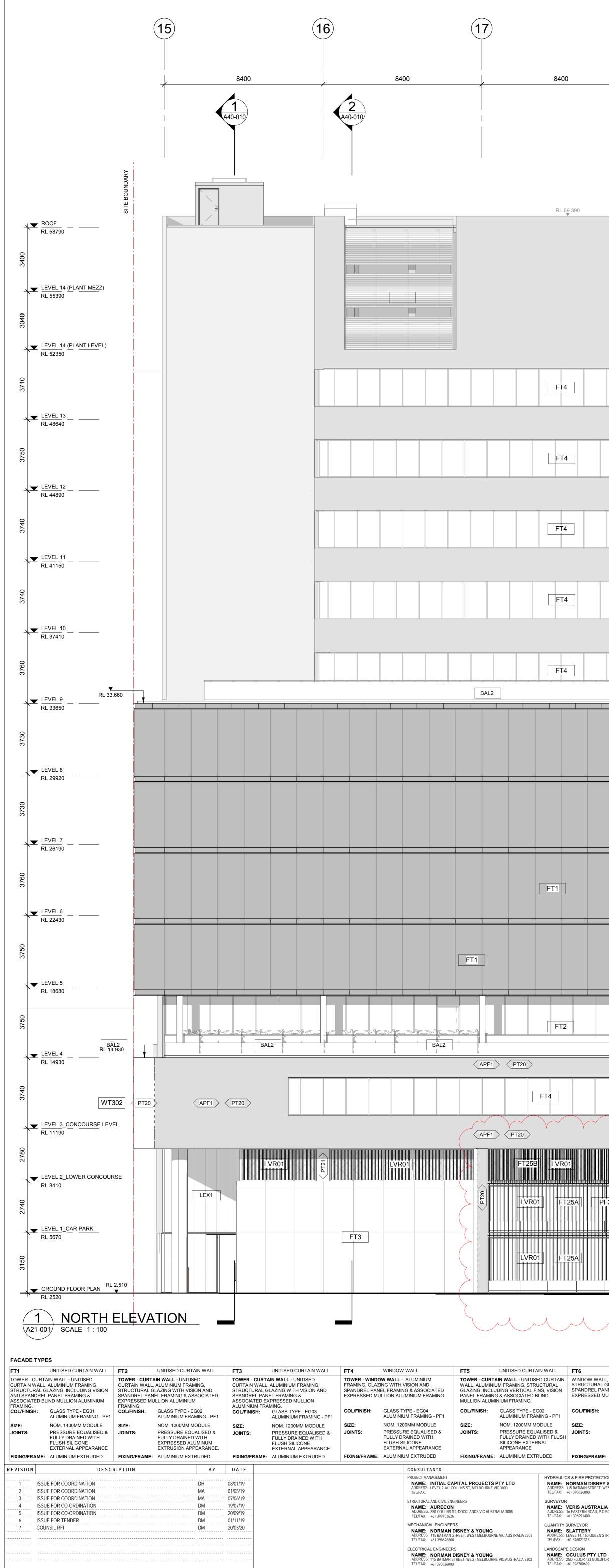
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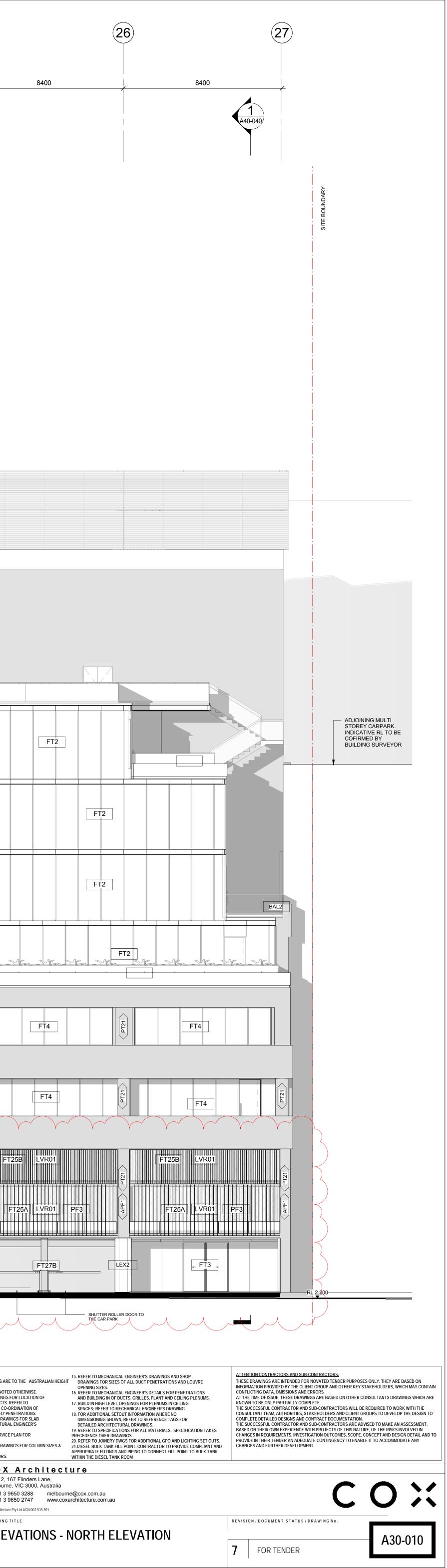


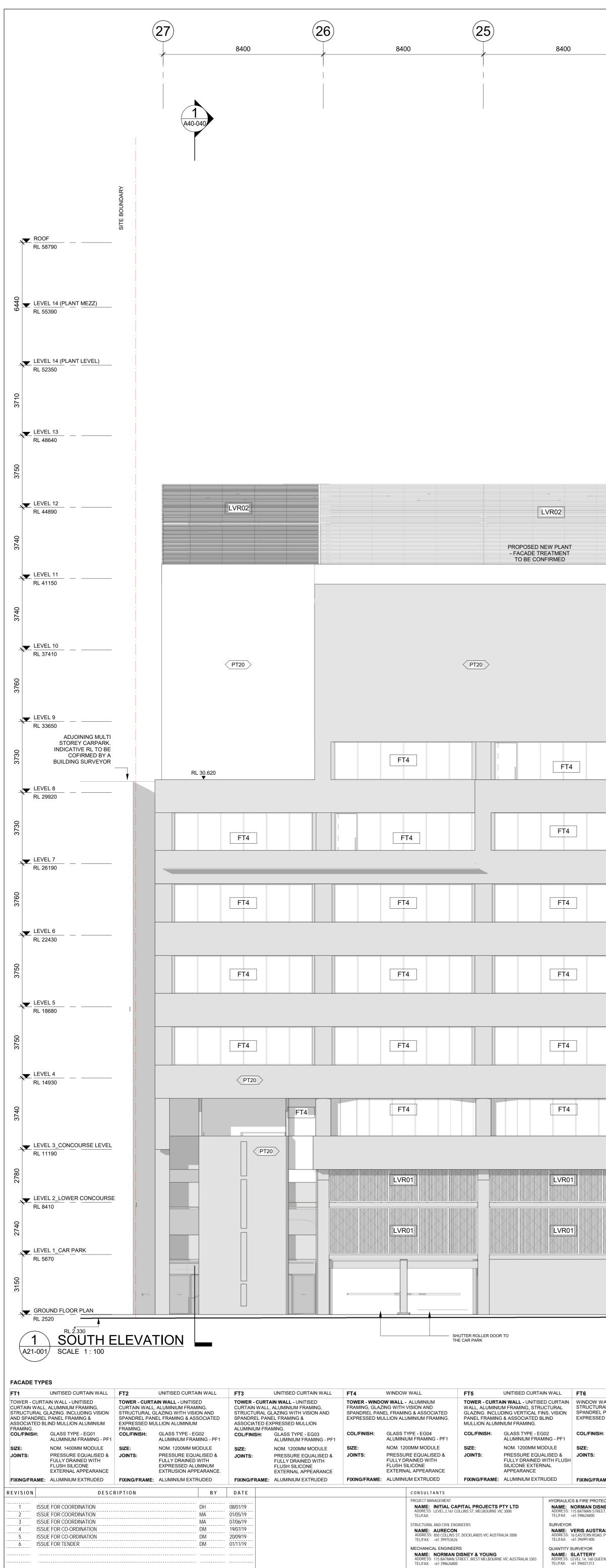
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WINDOW WALL L, ALUMINIUM FRAMING, GLAZING WITH VISION AND ANEL FRAMING & ASSOCIATED MULLION ALUMINIUM FRAMING GLASS TYPE - EG02 ALUMINIUM FRAMING - PF1 NOM. 1200MM MODULE PRESSURE EQUALISED & FULLY DRAINED WITH FLUSH SILICONE EXTERNA APPEARANCE E: ALUMINIUM EXTRUDED	O.     WINDOWS     STEEL FOLDED ME STAINLESS STEEL SPANNING VERTIC LANDSCAPE ARCH INTEGRATED LAND BASE.       1     SIZE:     TBC       JOINTS:     SILICON JOINT WITHIN FRAMEWORK   FT26 VERTIC AGED STEEL LATE	(DARK CHARCOAL) MILD       FOLDED ANODIZED (BRONZE) METAL SHEE         ETAL SURROUND, WITH       TOPHAT FRAME SUBSTRATE         TENSION CABLES       TOPHAT FRAME SUBSTRATE         MALLY BETWEEN. REFER TO       TOPHAT FRAME SUBSTRATE         SCAPE PLANTER TO       FT27B         METAL SCREEN WITH VERTICAL       FOLDED ANODIZED (BRONZE) METAL SHEE         CAL PLANTER PARTITION       FT27B         METAL SCREEN WITH VERTICAL       FOLDED ANODIZED (BRONZE) METAL SHEE         CHITECT'S DRAWING FOR       FOLDED ANODIZED (BRONZE) METAL SHEE         S.       OPHAT FRAME SUBSTRATE	T, TO COL/FINISH: TINTED GLASS SIZE: REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL EG01S TINTED GLASS (SPANDREL PANEL) COL/FINISH: TINTED WITH FULL BACKPAN SIZE: REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL SIZE:	CLEAR GLASS       EG03 CLEAR GLASS-LOBBY GLAZING RETAIL         CLEAR GLASS       REGO3 CLEAR GLASS-LOBBY GLAZING RETAIL         CLEAR GLASS       COL/FINISH:       CLEAR LAMINATED VISION GLASS         SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL       SIZE:       REFER TO THE FAÇA SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL         CLEAR GLASS       EG03S       CLEAR GLASS-LOBBY GLAZING SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL         CLEAR GLASS       EG03S       CLEAR GLASS-LOBBY GLAZE & ARCHITECTURAL DRAWINGS FOR DETAIL         CLEAR GLASS       EG03S       CLEAR GLASS-LOBBY GLAZE & ARCHITECTURAL DRAWINGS FOR DETAIL         CLEAR GLASS WITH FULL BACKPAN       EG03S       CLEAR GLASS WITH FULL BACKPAN         SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL       COL/FINISH:       CLEAR GLASS WITH FULL BACKPAN         SIZE:       REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL       DRAWINGS FOR DETAIL	ADE COL/FINISH: TINTED GLASS (CLEAR LAMINATED VISION GLASS) SIZE: REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL ZING EG05 GLASS BALUSTRADE COL/FINISH: CLEAR GLASS (CLEAR LAMINATED VISION GLASS) SIZE: REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL	COL/FINISH: FROSTED GLASS SIZE: REFER TO THE FAÇADE SCHEDULE & ARCHITECTURAL DRAWINGS FOR DETAIL 5. T C( 6. C 7. N TH IS	RAWING AND SITE CONDITIONS PRIOR TO ANY WORK COM ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHII IANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOI F APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALED DI 8D DRAWINGS ARE FOR VISUAL REFERENCE ONLY, 2D DRAV RECEDENCE OVER 3D VISUAL IMAGES THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH AL CONTRACTS, SPECIFICATIONS AND DRAWINGS. COPYRIGHT OF THIS DRAWING IS VESTED IN COX ARCHITE VOTE: THIS DRAWING ISSUE INCLUDING ALL CLOUDED REVI HIS REVISION IS REVISED FOR ALL PREVIOUS TRADE PACK, SSUED.	TECT. THE       09. COLUMNS CENTRED ON GRID UNLESS NOTI         R TO THE RETURN       10. REFER TO SERVICE ENGINEER'S DRAWINGS         CONSULTANT.       S.DF BOXES, G.P.O'S AND SKIRTING DUCTS.         WINGS TAKE       SLAB & WALL 'BLOCKOUTS' AND 'CORED' P         11. REFER TO STRUCTURAL ENGINEER'S DRAWINGS FOR CO.         KLR ELEVANT       SLAB & WALL 'BLOCKOUTS' AND 'CORED' P         11. REFER TO STRUCTURAL ENGINEER'S DRAWINGS FOR CO.         SCTURE.       SLAB & WALL 'BLOCKOUTS' AND 'CORED' P         ISIONS TAGGED AS       AGES PREVIOUSLY         AGES PREVIOUSLY       12. REFER TO STRUCTURAL ENGINEER'S SERVIC         LOCATION OF SUB-FLOOR SERVICES.       13. REFER TO STRUCTURAL ENGINEER'S DRAW         DETAILS.       14. REFER TO DOOR SCHEDULES FOR DOORS.
TION CONSULTANTS Y & YOUNG WEST MELBOURNE VIC AUSTRALIA 3303 IA PTY LTD D BOX 5075, SOUTH MELBOURNE, VIC 3205 STREET, MELBOURNE VIC3000	TEL/FAX: +61394197226 BUILDING SURVEYOR NAME: PLP BUILDING SURVEYORS AND CONSULTANTS	ARTIFEX	NEW CENTRAL INVESTME PROPERTY GROUP	ENTS PTY LTD/ CONTRACT DOCUMENT INCONSISTANCIES / COI DO NOT SCALE FROM T ANY FORM OF REPRODI LTD CONSTITUTES AN II C THIS DRAWING IS C D R A W N I	HE DRAWING.         JCTION OF THIS DRAWING IN FULL OR IN PART WITHOUT THE WRITTEN PENFRINGEMENT OF COPYRIGHT.         COPYRIGHT AND SHALL REMAIN THE PROPERTY OF COX ARCHITECTURE         D A T E       C H E C K E D         20/03/20       DM	AS INC	FLINDERS STREET	COX Level 2, Melbourr T + 61 3 F + 61 3 Cox Architectu DRAWING ELE
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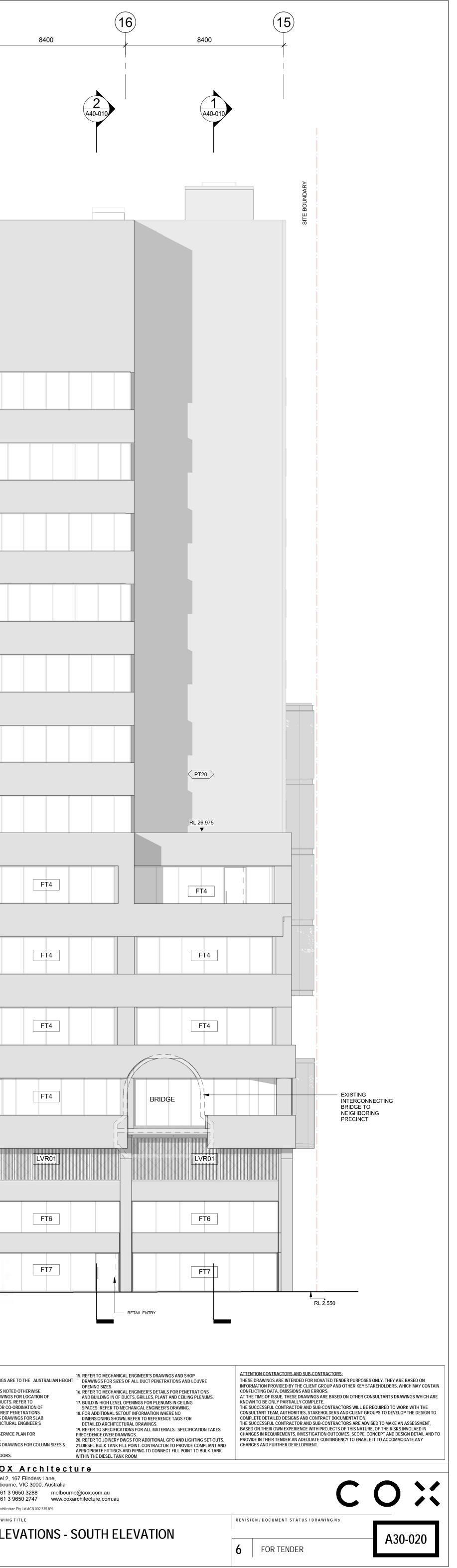




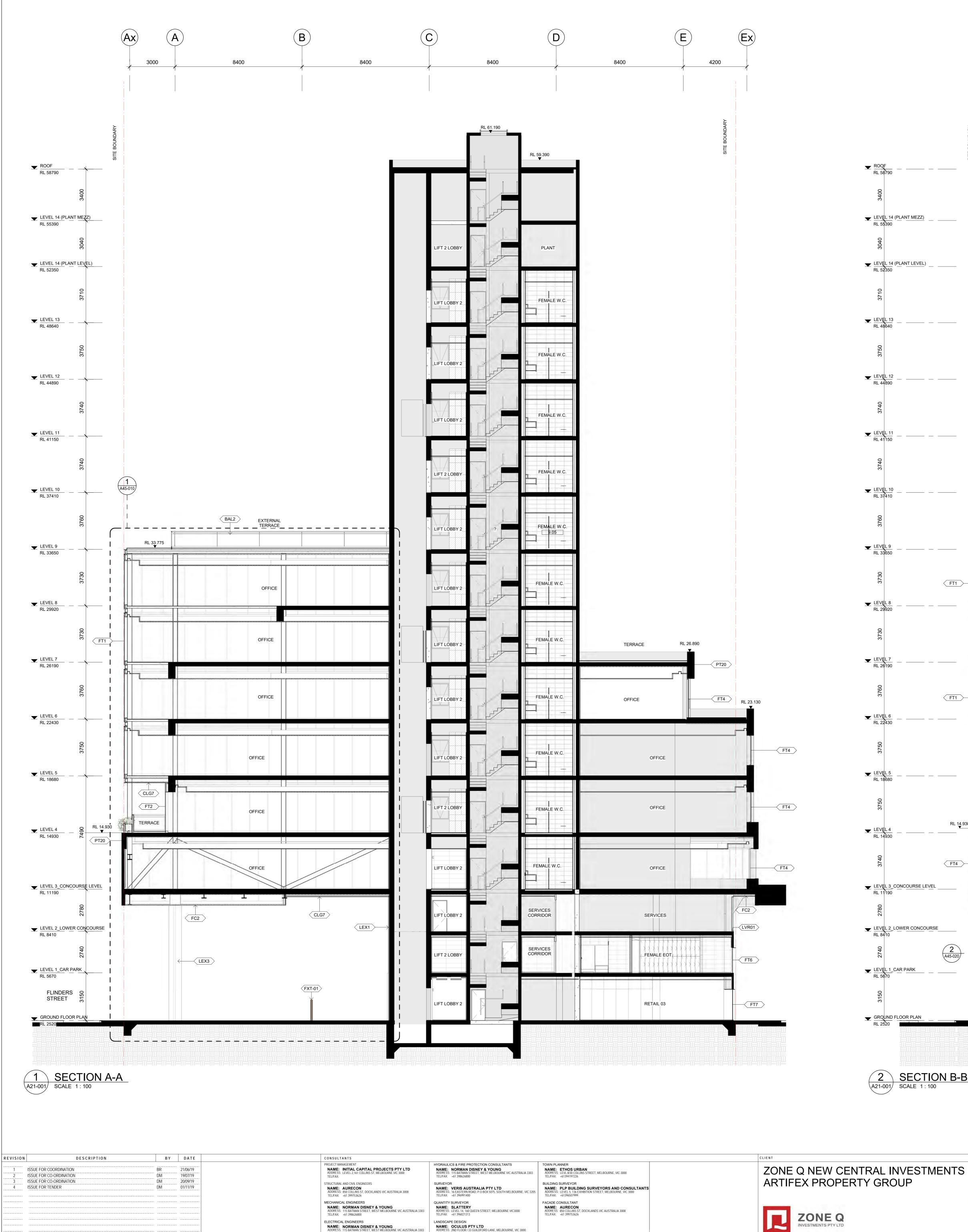
ELECTRICAL ENGINEERS

NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800

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HYDRAULICS & FIRE PROTECTION CONSULT NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURN TEL/FAX: +61 398626800 SURVEYOR NAME: VERIS AUSTRALIA PTY LTD ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUT	TANTS TOWN NE VIC AUSTRALIA 3303 ADDR TEL/F/ BUILDI	I PLANNER IE: ETHOS URBAN IESS: LEVL 8/30 COLLINS STREET, MELBOURNE, VIC- AX: +61394197226 ING SURVEYOR IE: PLP BUILDING SURVEYORS AND IESS: LEVEL 5, 136 EXHIBITION STREET, MELBOURNE	1000	I				STMENTS PT		N OT E S CHECK AND VERIFY ALL DIMENSIONS PRIOR TO CC CONTRACT DOCUMENTS INCLUDING THOSE BY OT INCONSISTANCIES / CONFLICTS. DO NOT SCALE FROM THE DRAWING. ANY FORM OF REPRODUCTION OF THIS DRAWING I LTD CONSTITUTES AN INFRINGEMENT OF COPYRIG (C) THIS DRAWING IS COPYRIGHT AND SHALL REM	MMENCEMENT OF WORK. THIS DRAWING HER CONSULTANTS, AND INCLUDING SPE N FULL OR IN PART WITHOUT THE WRITTE HT.	CIFICATIONS. SEEK CLARIFICATION OF	As indicated		DATE		<b>CO</b> Level 2, Melbour T + 61 3 F + 61 3
ADDRESS: 16 EASTERN ROAD, P.O. BOX 5075, SOUT TEL/FAX: +61 396991400 QUANTITY SURVEYOR ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOU TEL/FAX: +61 396021313 LANDSCAPE DESIGN NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELE TEL/FAX: +61 396700699	TEL/F/ FACAE NAM JURNE VIC3000 ADDRI TEL/F/	IESS: LEVEL 5, 136 EXHIBITION STREET, MELBOURNE AX: +61396507999 DE CONSULTANT IE: AURECON ESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3 AX: +61 399753626			R	ZONE Q NVESTMENTS PTY LTD		F	artifex property	DRAWN DATE CHI MA 01/11/19 DM	DJECT DIRECTOR	PROJECT No.		DERS STREE RS STREET, DO			







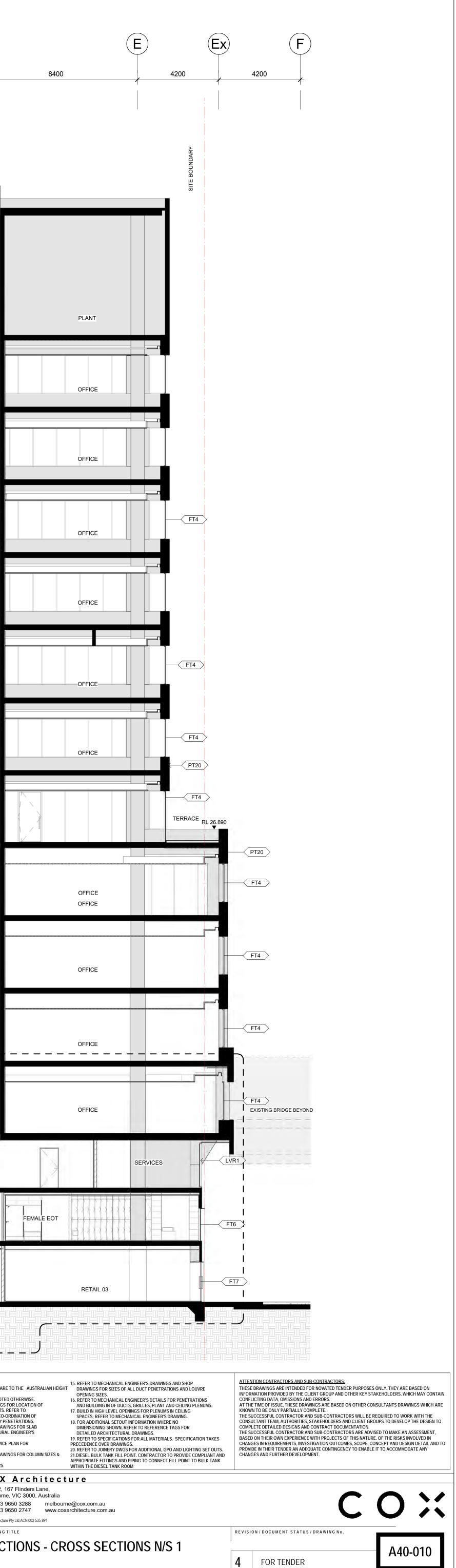
NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626 NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TELIFAX: +61 396700699

ELECTRICAL ENGINEERS

NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800

LANDSCAPE DESIGN

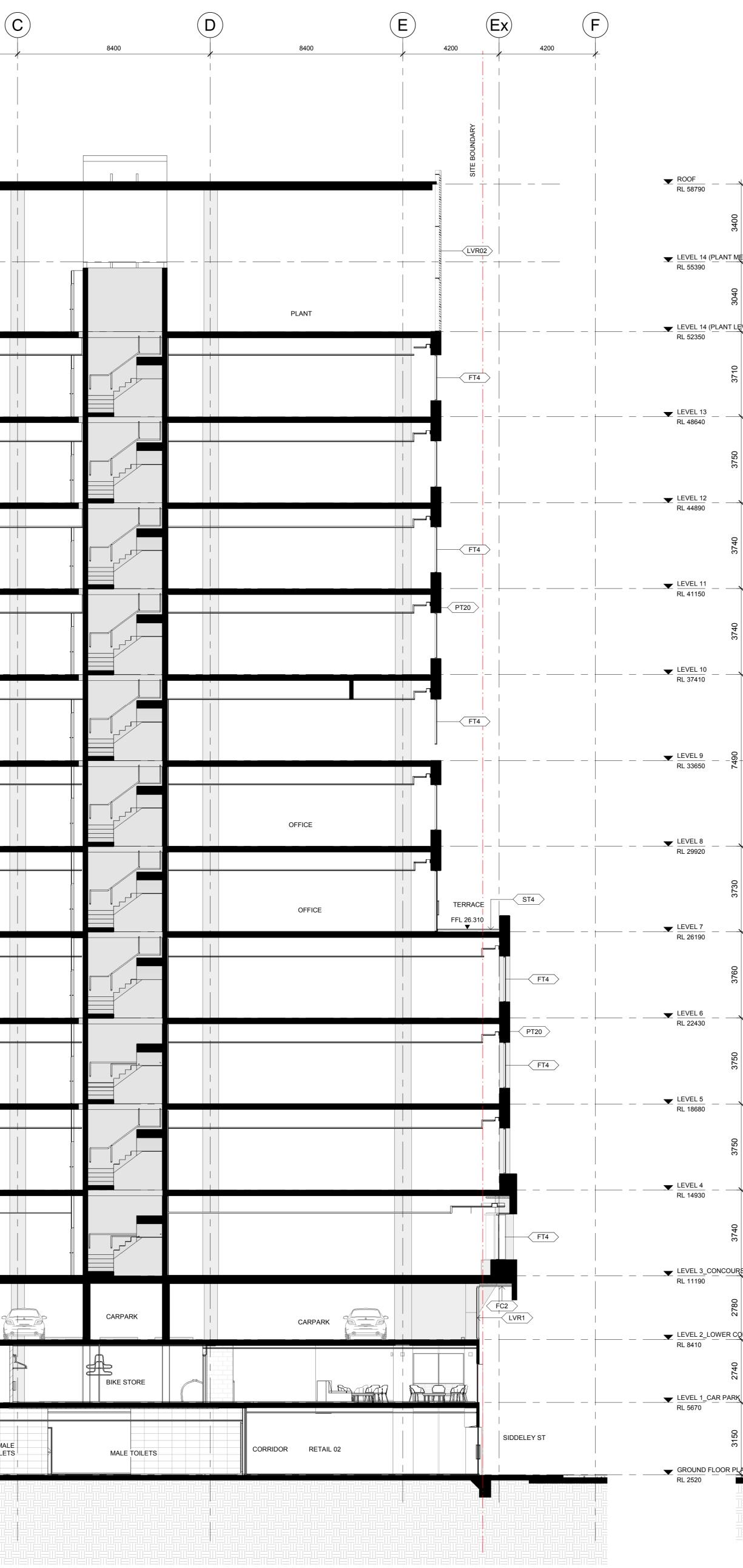
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3	SECTION B-B2
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▼ LEVEL 12 RL 44890	3750					
▼ <u>LEVEL 11</u> RL 41150	3740					
▼ <u>LEVEL 10</u>	3740					
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JSTRALIA PTY LTD IROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205 QUEEN STREET, MELBOURNE VIC3000 NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699



CLIENT ZONE Q NEW CENTRAL INVESTMENTS PT ARTIFEX PROPERTY GROUP

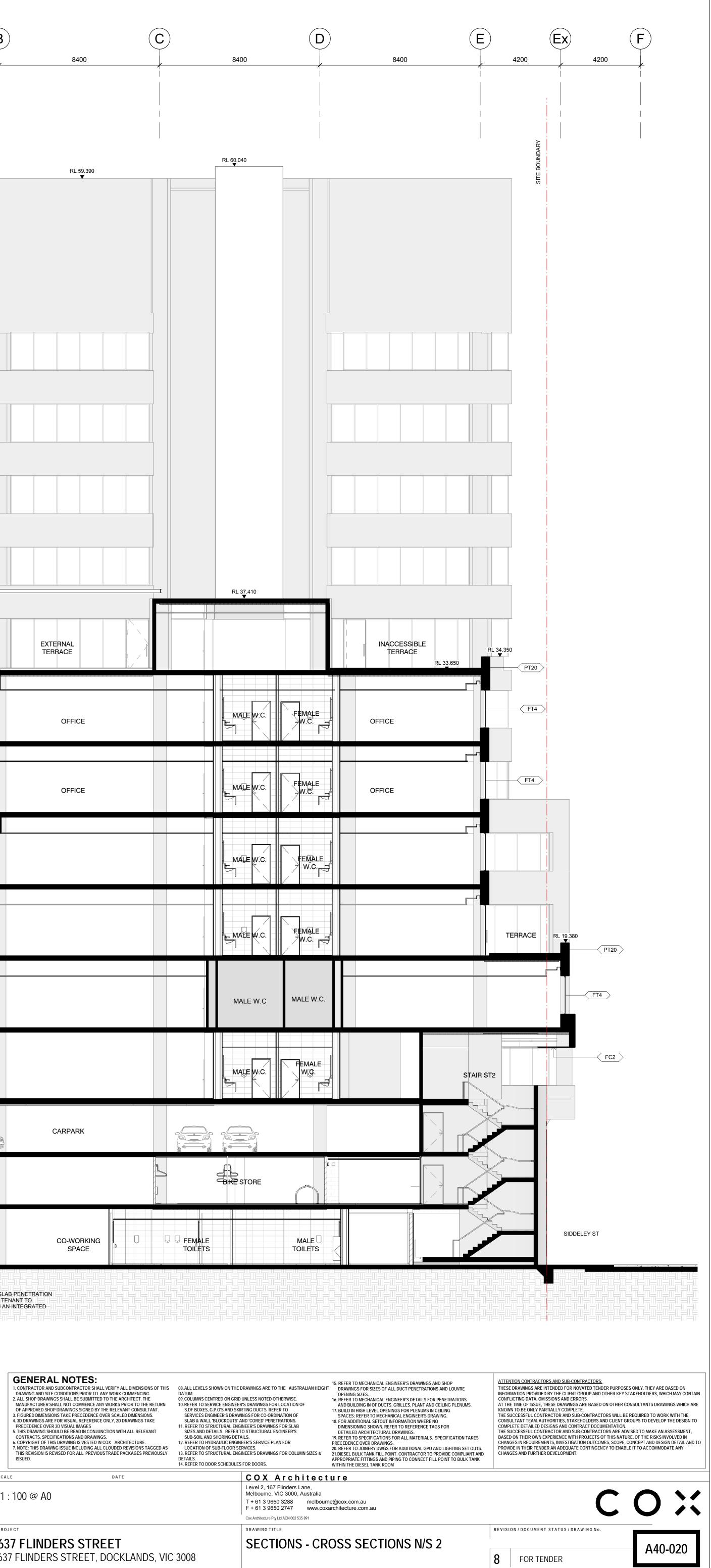


								5. THIS DRAWING SHOULD BE READ IN CONJUNCTION WITH ALL RELEVANT       SIZES AND DETAILS.         CONTRACTS, SPECIFICATIONS AND DRAWINGS.       SUB-SOIL AND SHORIN         6. COPYRIGHT OF THIS DRAWING IS VESTED IN COX ARCHITECTURE.       SUB-SOIL AND SHORIN         7. NOTE: THIS DRAWING ISSUE INCLUDING ALL CLOUDED REVISIONS TAGGED AS THIS REVISION IS REVISED FOR ALL PREVIOUS TRADE PACKAGES PREVIOUSLY       LOCATION OF SUB-FLC         13. REFER TO STRUCTURAL ISSUED.       14. REFER TO DOOR SCHE	G DETAILS. Engineer's Servic Or Services. L Engineer's Drav
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### A21-001 SCALE 1:100

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1	ISSUE FOR COORDINATION	DH	08/01/19
	ISSUE FOR COORDINATION	MA	01/05/19
3	ISSUE FOR COORDINATION	MA	07/06/19
	ISSUE FOR COORDINATION	BR	21/06/19
	ISSUE FOR CO-ORDINATION	DM	19/07/19
6	ISSUE FOR CO-ORDINATION	DM	20/09/19
7	ISSUE FOR TENDER	DM	01/11/19
8	COUNSIL RFI	DM	20/03/20



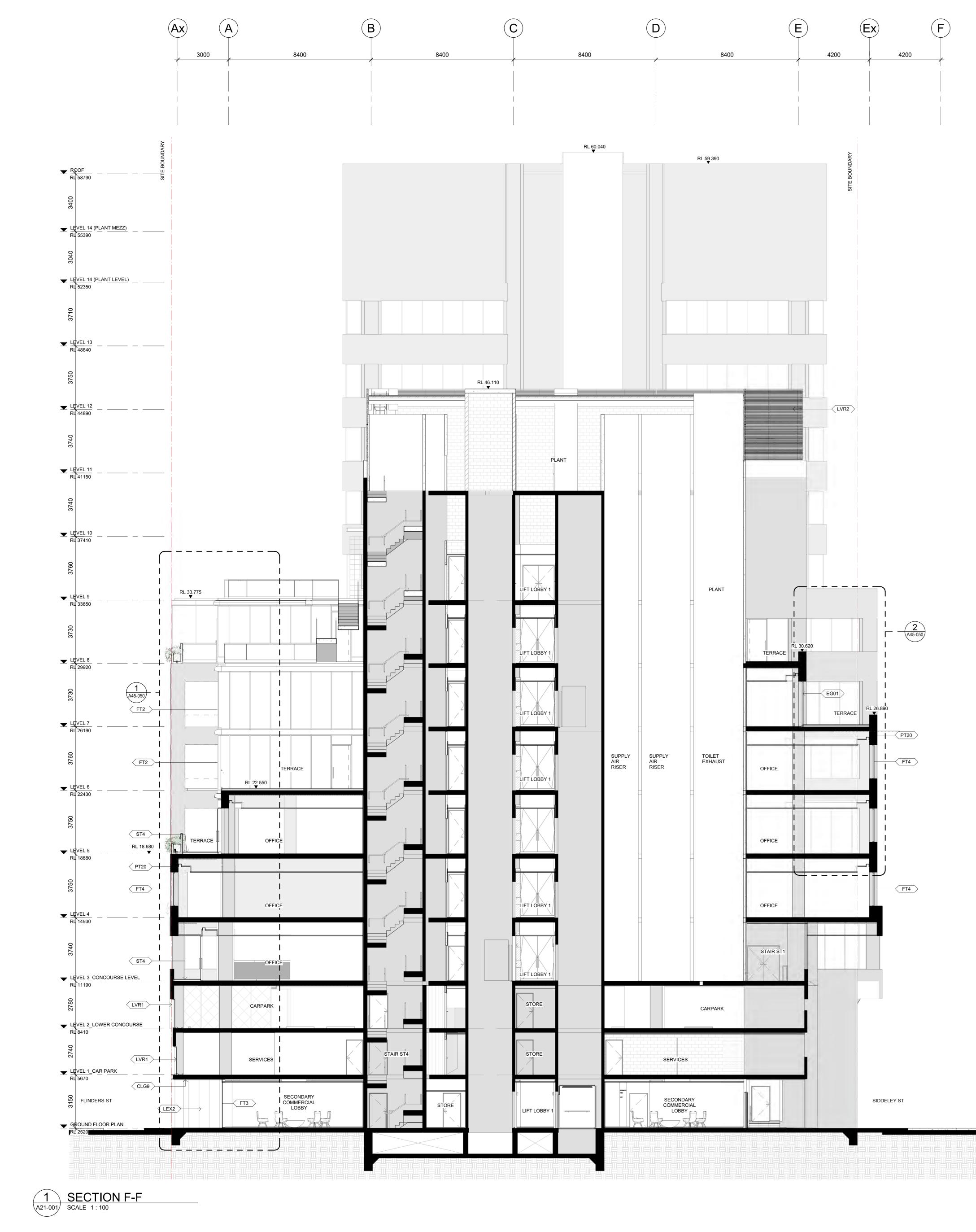


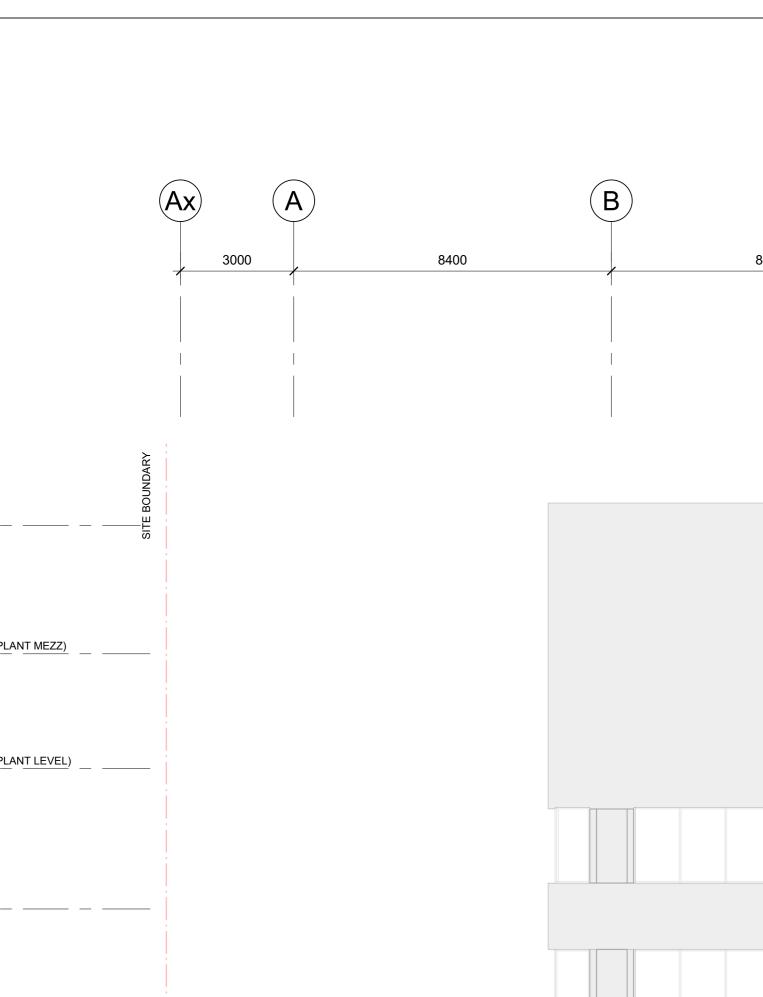
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-	GENERAL NOTES:	
	1. CONTRACTOR AND SUBCONTRACTOR SHALL VERIFY ALL DIMENSIONS OF THIS	08. ALL
	DRAWING AND SITE CONDITIONS PRIOR TO ANY WORK COMMENCING.	DATUN
	2. ALL SHOP DRAWINGS SHALL BE SUBMITTED TO THE ARCHITECT. THE	09.COL
	MANUFACTURER SHALL NOT COMMENCE ANY WORKS PRIOR TO THE RETURN	10. REF
	OF APPROVED SHOP DRAWINGS SIGNED BY THE RELEVANT CONSULTANT.	S.D
	3. FIGURED DIMENSIONS TAKE PRECEDENCE OVER SCALED DIMENSIONS.	SER
	4. 3D DRAWINGS ARE FOR VISUAL REFERENCE ONLY, 2D DRAWINGS TAKE	SLA
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	CONTRACTS, SPECIFICATIONS AND DRAWINGS.	SUE
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REVISION	DESCRIPTION	ΒY	DATE	CONSULTANTS	
	ISSUE FOR TENDER	DM	01/11/19	PROJECT MANAGEMENT NAME: INITIAL CAPITAL PROJECTS PTY LTD ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX:	HYDRAULICS & FIRE PROTECTION CON NAME: NORMAN DISNEY & YOU ADDRESS: 115 BATMAN STREET, WEST MEL TEL/FAX: +61 398626800
				STRUCTURAL AND CIVIL ENGINEERS NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEU/FAX: +61 399753626	SURVEYOR NAME: VERIS AUSTRALIA PTY ADDRESS: 16 EASTERN ROAD, P.O BOX 5075 TEUFAX: +61 396991400
				MECHANICAL ENGINEERS NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEU/FAX: +61 398626800	QUANTITY SURVEYOR NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN STREET, M TEI/FAX: +61 3960/21313
				ELECTRICAL ENGINEERS NAME: NORMAN DISNEY & YOUNG	LANDSCAPE DESIGN NAME: OCULUS PTY LTD
				ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	ADDRESS: 2ND FLOOR / 33 GUILDFORD LAN TEL/FAX: +61 396700699





PROTECTION CONSULTANTS I DISNEY & YOUNG STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TOWN PLANNER NAME: ETHOS URBAN ADDRESS: LEVL 8/30 COLLINS STREET, MELBOURNE, VIC-3000 TEL/FAX: +61394197226 BUILDING SURVEYOR JSTRALIA PTY LTD IROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205 NAME: PLP BUILDING SURVEYORS AND CONSULTANTS ADDRESS: LEVEL 5, 136 EXHIBITION STREET, MELBOURNE, VIC 3000 TEL/FAX: +61396507999 FACADE CONSULTANT NAME: AURECON ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626 QUEEN STREET, MELBOURNE VIC3000 PTY LTD 33 GUILDFORD LANE, MELBOURNE, VIC 3000

CLIENT ZONE Q NEW CENTRAL INVESTMENTS PT ARTIFEX PROPERTY GROUP



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PTY LTD/	CONTRACT DOCU INCONSISTANCIES DO NOT SCALE FF ANY FORM OF REI LTD CONSTITUTES	CHECK AND VERIFY ALL DIMENSIONS PRIOR TO COMMENCEMENT OF WORK. THIS DRAWING SHALL BE READ IN CONJUNCTION WITH ALL OTHER CONTRACT DOCUMENTS INCLUDING THOSE BY OTHER CONSULTANTS, AND INCLUDING SPECIFICATIONS. SEEK CLARIFICATION OF INCONSISTANCIES / CONFLICTS. DO NOT SCALE FROM THE DRAWING. ANY FORM OF REPRODUCTION OF THIS DRAWING IN FULL OR IN PART WITHOUT THE WRITTEN PERMISSION OF THE COX ARCHITECTURE PTY ITD CONSTITUTES AN INFRINGEMENT OF COPYRIGHT. THIS DRAWING IS COPYRIGHT AND SHALL REMAIN THE PROPERTY OF COX ARCHITECTURE PTY LTD						100 @ A0		Level 2, 1 Melbourn T + 61 3 9 F + 61 3 9 Cox Architectur	
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AND BUILDING IN OF DUCTS, GRILLES, PLANT AND CEILING PLENUMS. 17. BUILD IN HIGH LEVEL OPENINGS FOR PLENUMS IN CEILING SPACES; REFER TO MECHANICAL ENGINEER'S DRAWING.

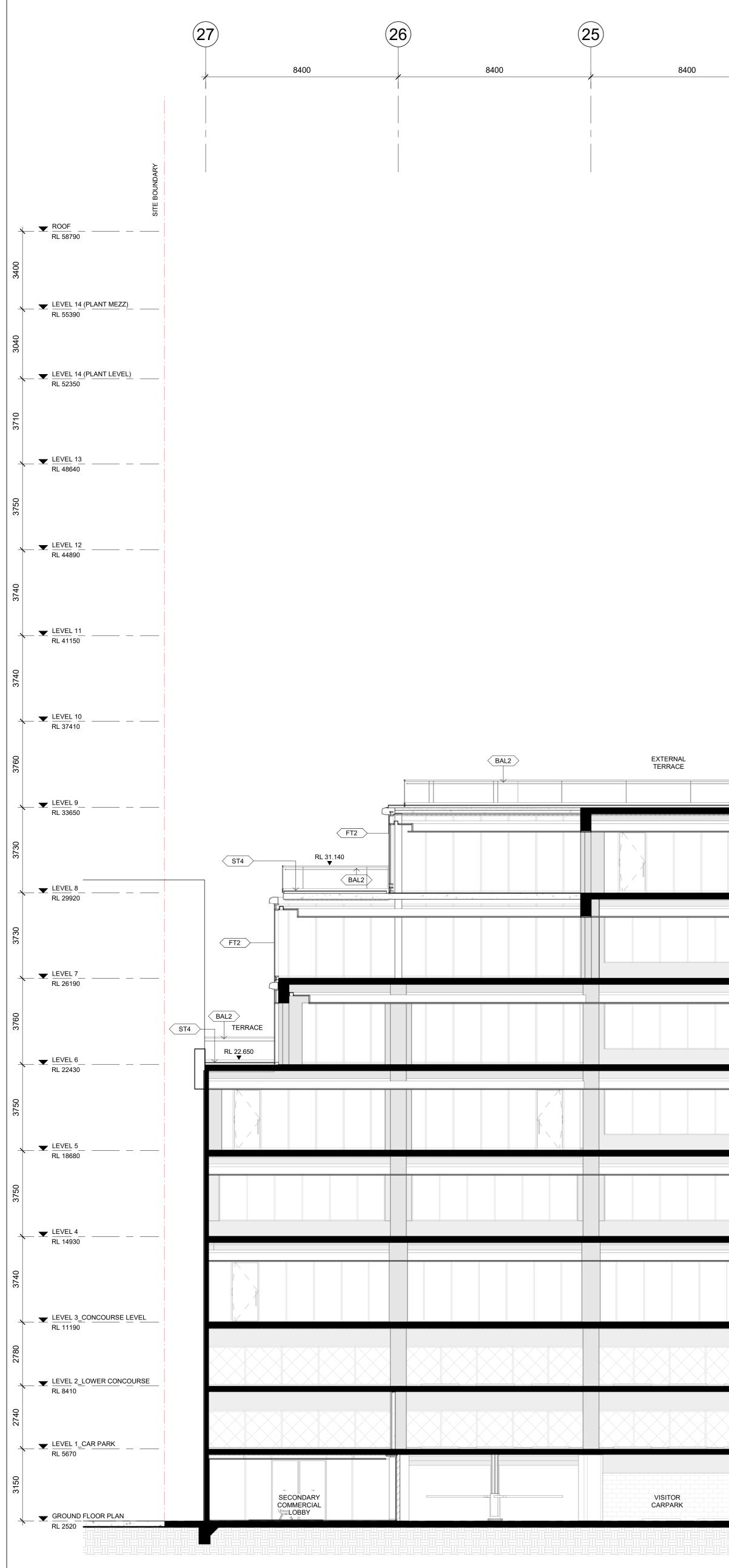
ATTENTION CONTRACTORS AND SUB-CONTRACTORS:

THESE DRAWINGS ARE INTENDED FOR NOVATED TENDER PURPOSES ONLY. THEY ARE BASED ON

INFORMATION PROVIDED BY THE CLIENT GROUP AND OTHER KEY STAKEHOLDERS, WHICH MAY CONTAIN CONFLICTING DATA, OMISSIONS AND ERRORS.

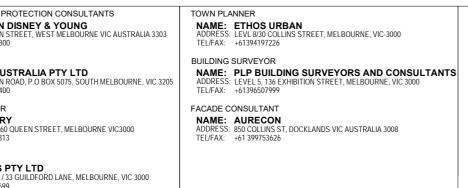
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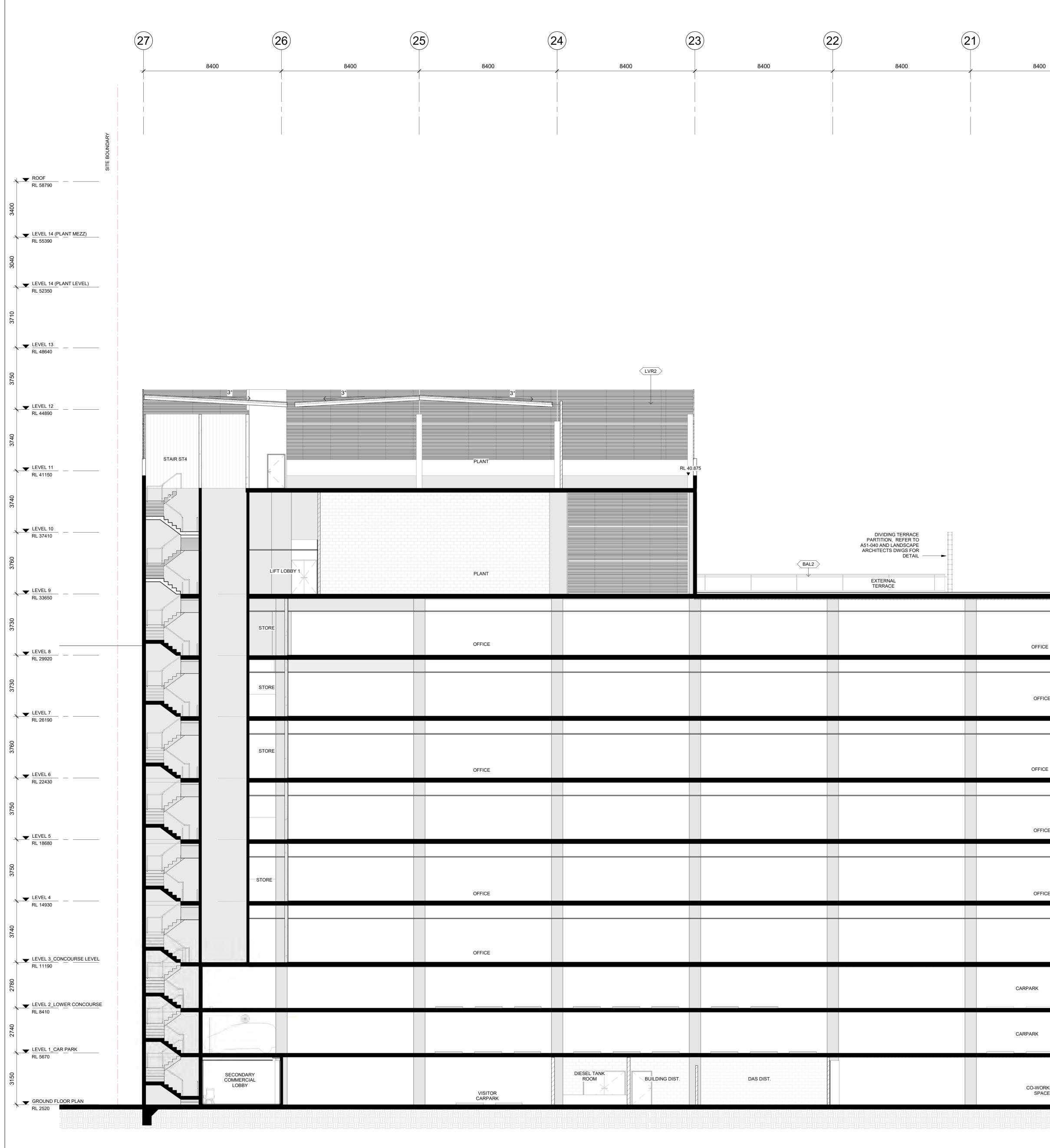
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EXTERNAL TERRACE

REVISION	DESCRIPTION	BY	DATE	CONSULTANTS	
	·			PROJECT MANAGEMENT	HYDRAULICS & FIRE PROTECTION CONSULTANTS
1	ISSUE FOR COORDINATION	MA	01/05/19	NAME: INITIAL CAPITAL PROJECTS PTY LTD	NAME: NORMAN DISNEY & YOUNG
2	ISSUE FOR COORDINATION	MA	07/06/19	ADDRESS: LEVEL 2,161 COLLINS ST, MELBOURNE VIC 3000 TEL/FAX:	ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800
3	ISSUE FOR COORDINATION	BR	21/06/19	STRUCTURAL AND CIVIL ENGINEERS	SURVEYOR
4	ISSUE FOR CO-ORDINATION	DM	19/07/19	NAME: AURECON	NAME: VERIS AUSTRALIA PTY LTD
5	ISSUE FOR CO-ORDINATION	DM	20/09/19	ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008 TEL/FAX: +61 399753626	ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205 TEL/EAX: +61 396991400
6	ISSUE FOR TENDER	DM	01/11/19		
				MECHANICAL ENGINEERS	QUANTITY SURVEYOR
				ADDRES: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303	NAME: SLATTERY ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000
				TEL/FAX: +61 398626800	TEL/FAX: +61 396021313
				ELECTRICAL ENGINEERS	LANDSCAPE DESIGN
				NAME: NORMAN DISNEY & YOUNG ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303 TEL/FAX: +61 398626800	NAME: OCULUS PTY LTD ADDRESS: 2ND FLOOR / 33 GUILDFORD LANE, MELBOURNE, VIC 3000 TEL/FAX: +61 396700699

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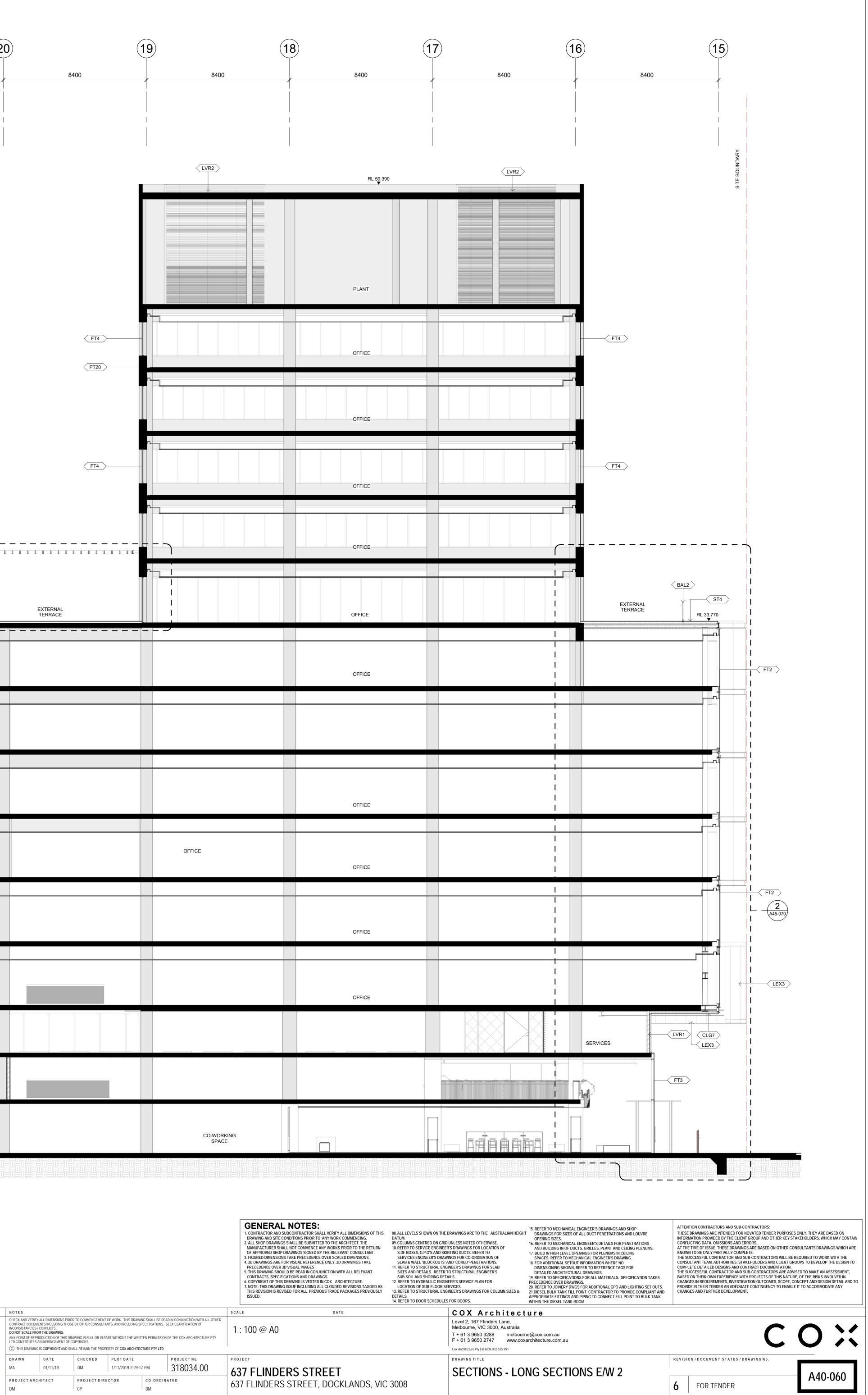
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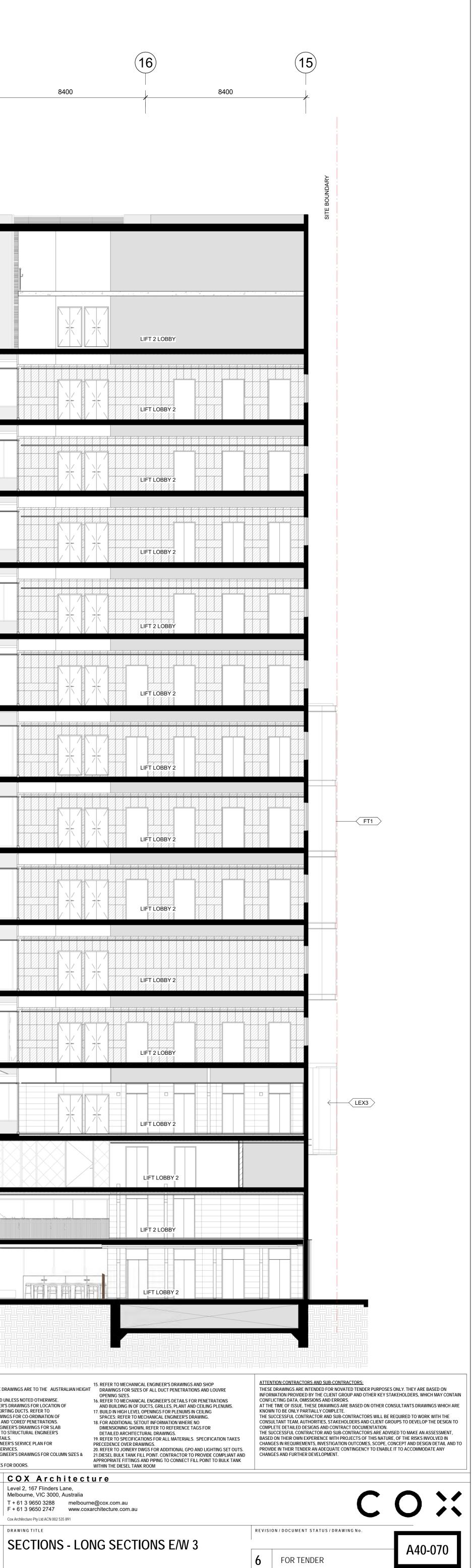
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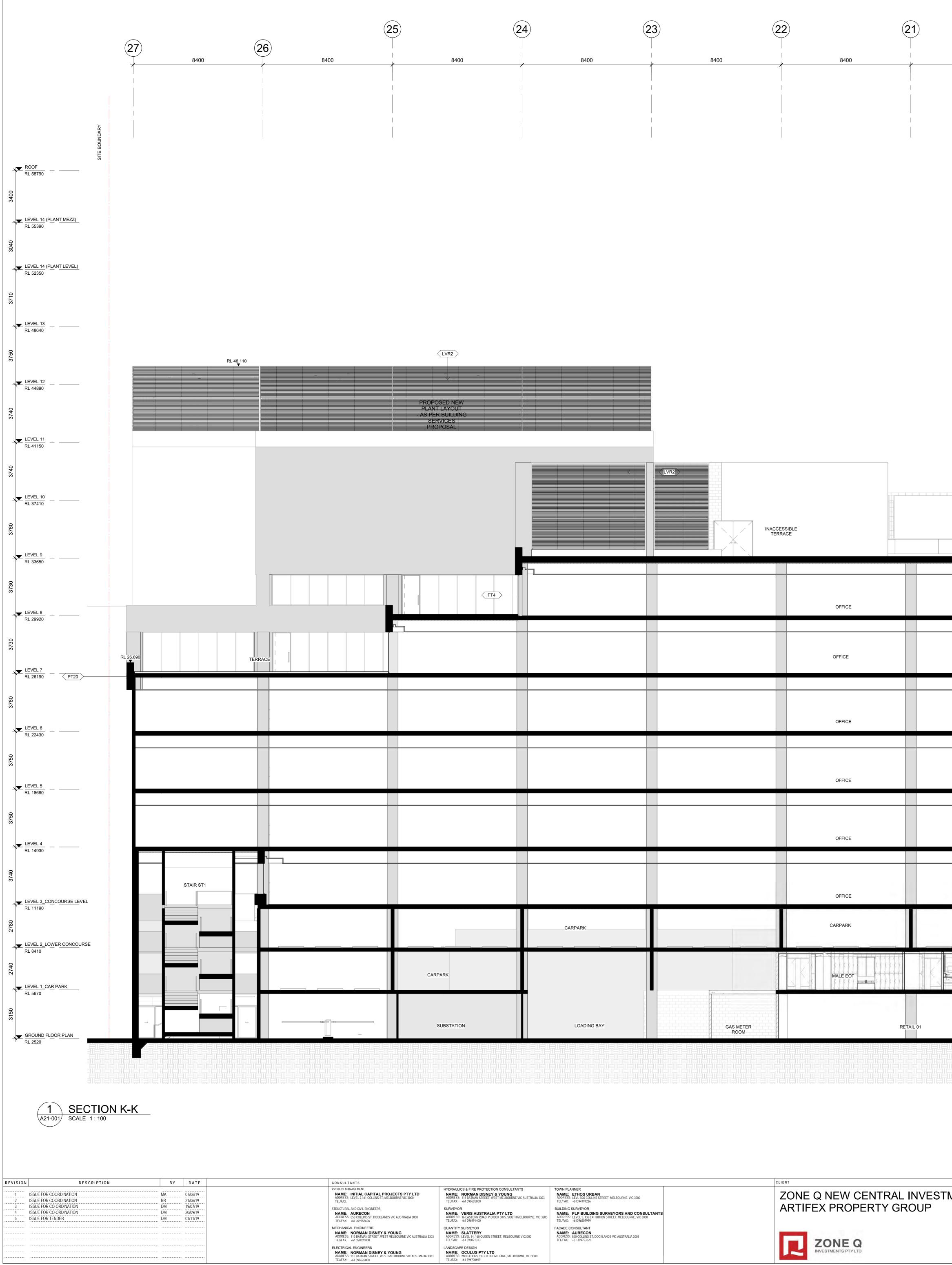
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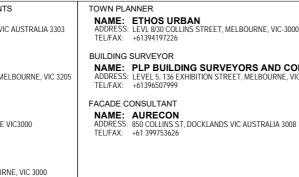


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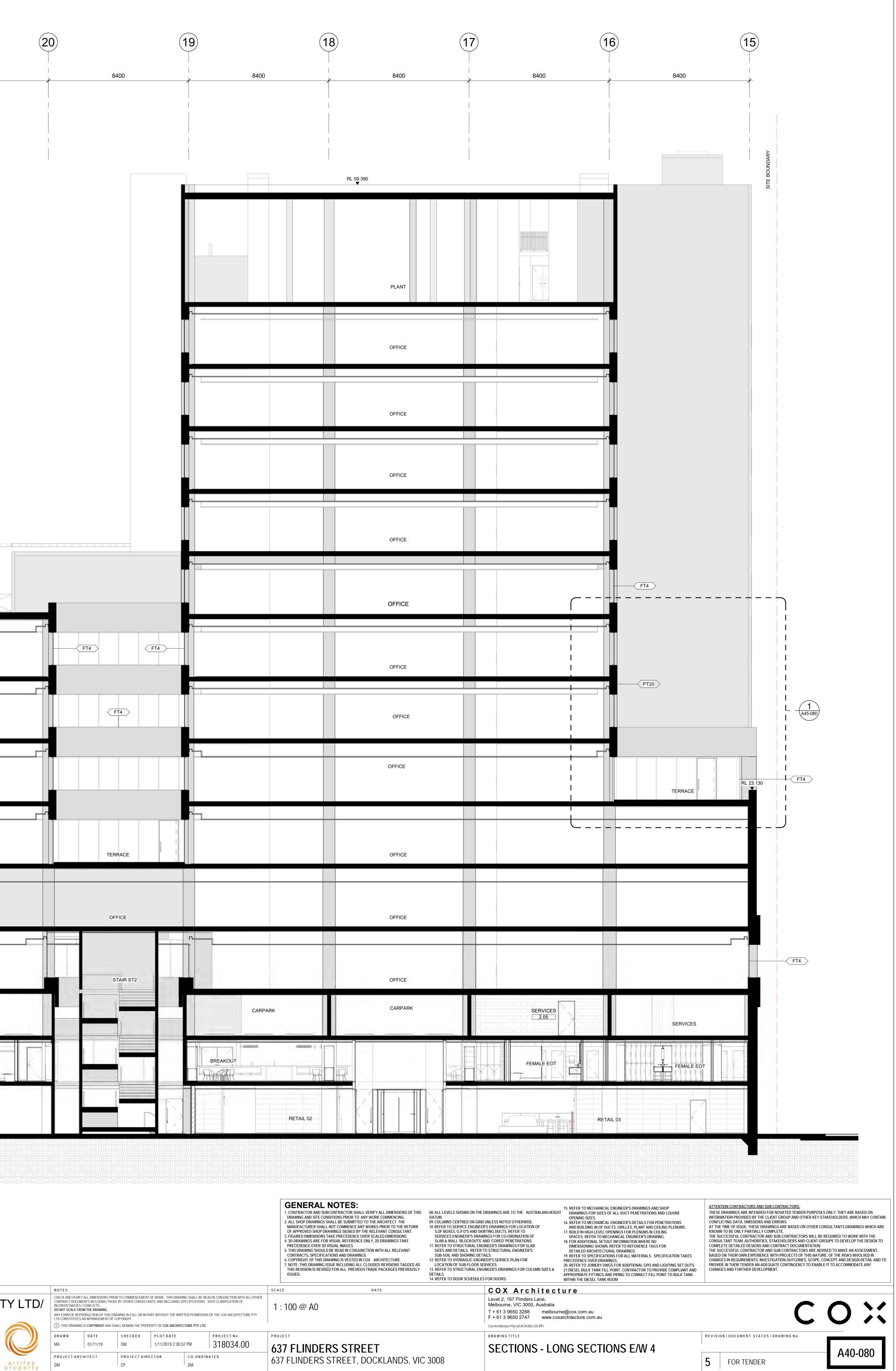


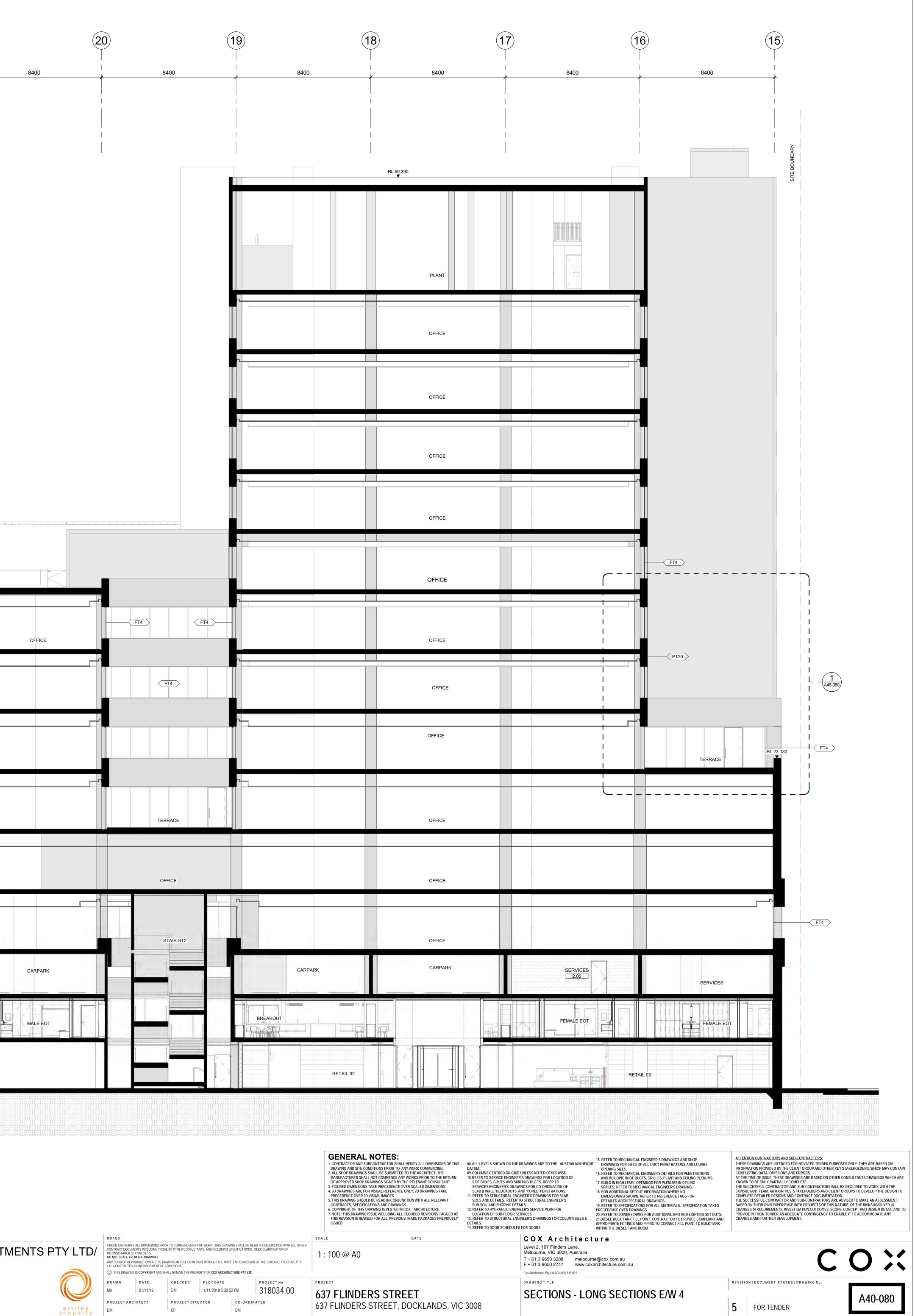


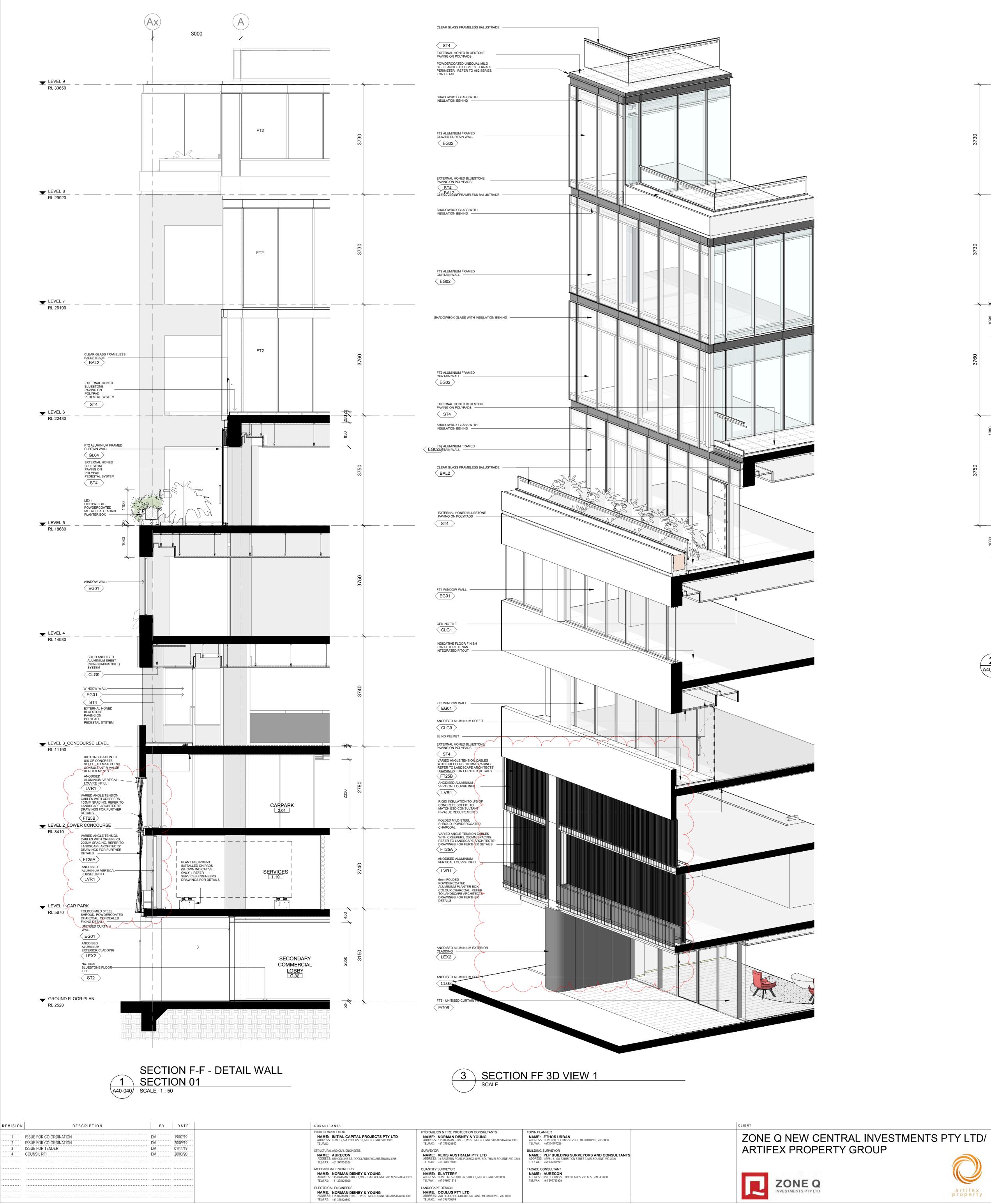


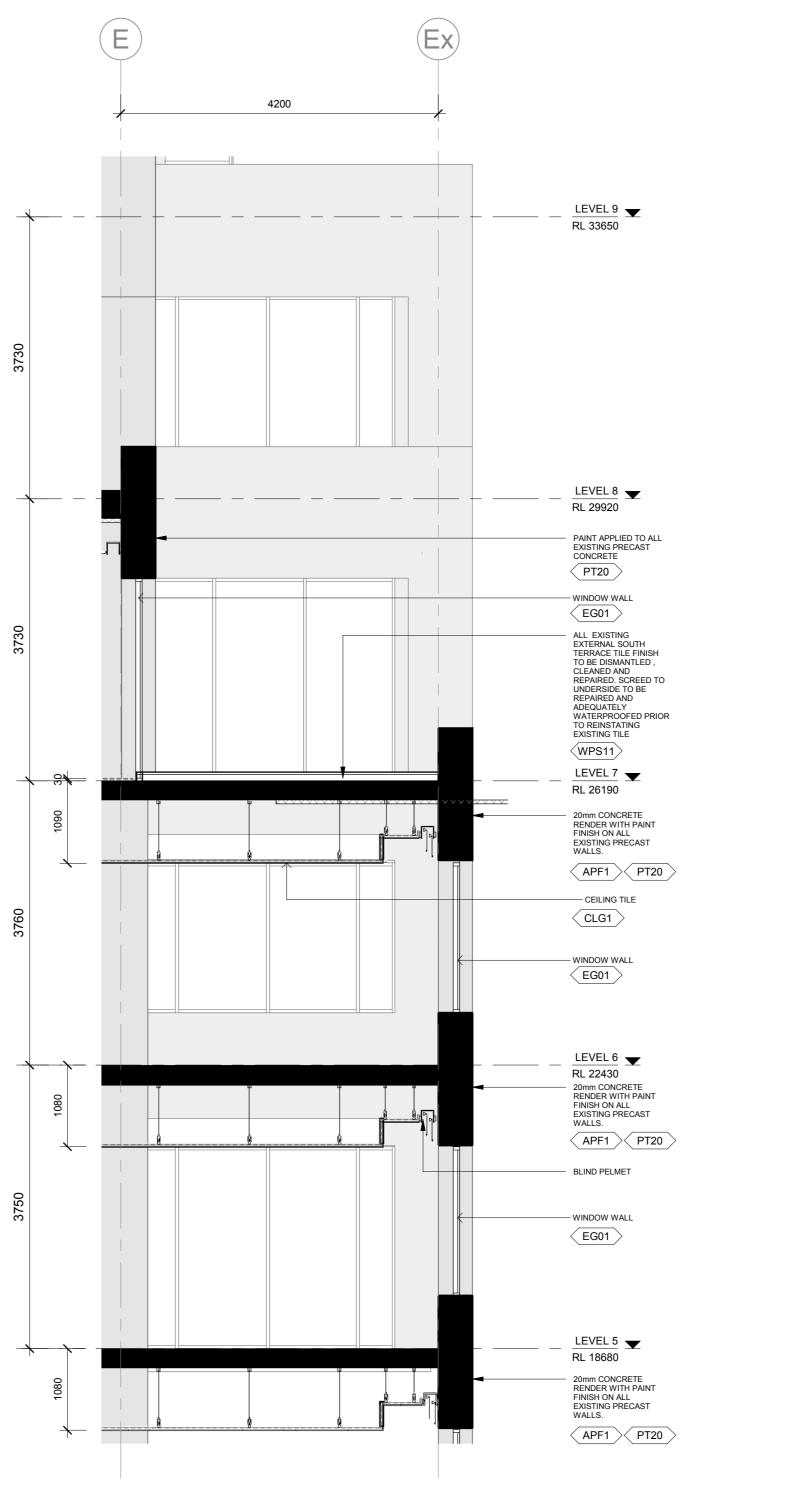


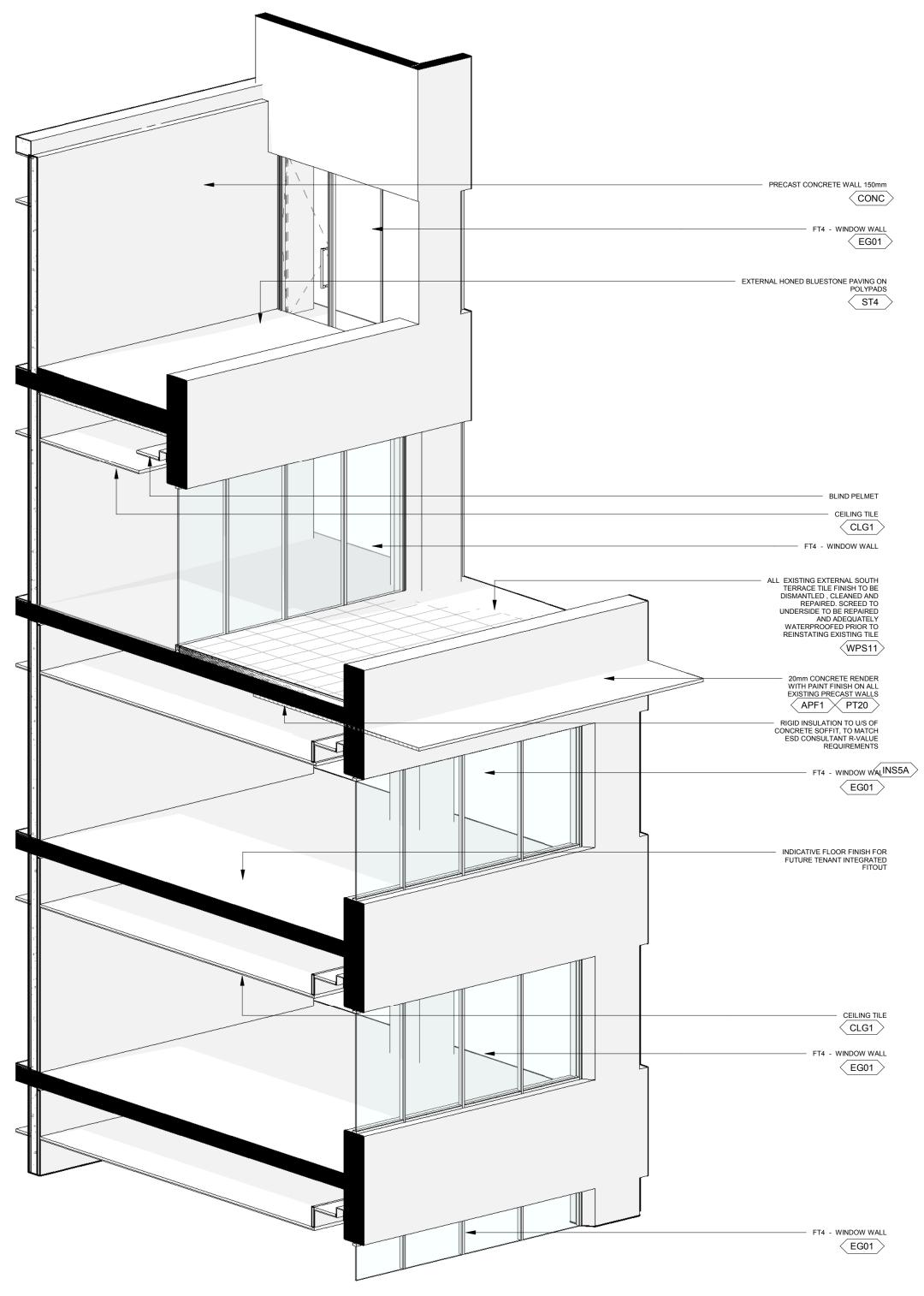
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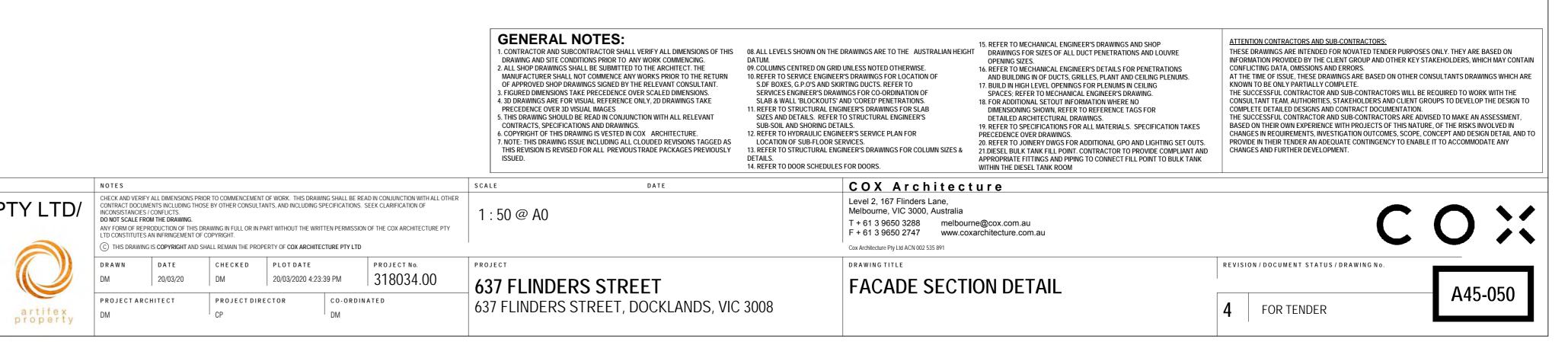


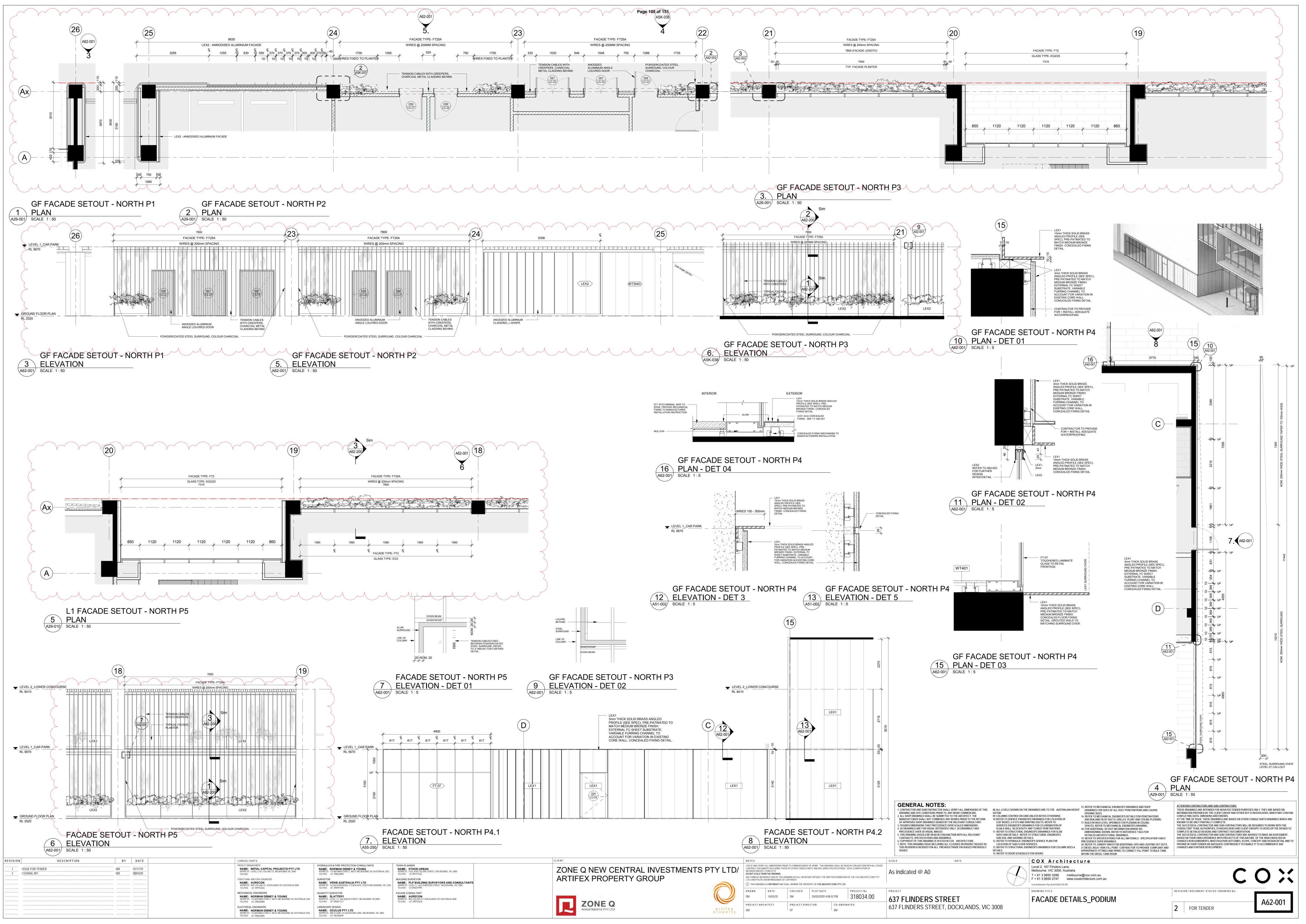


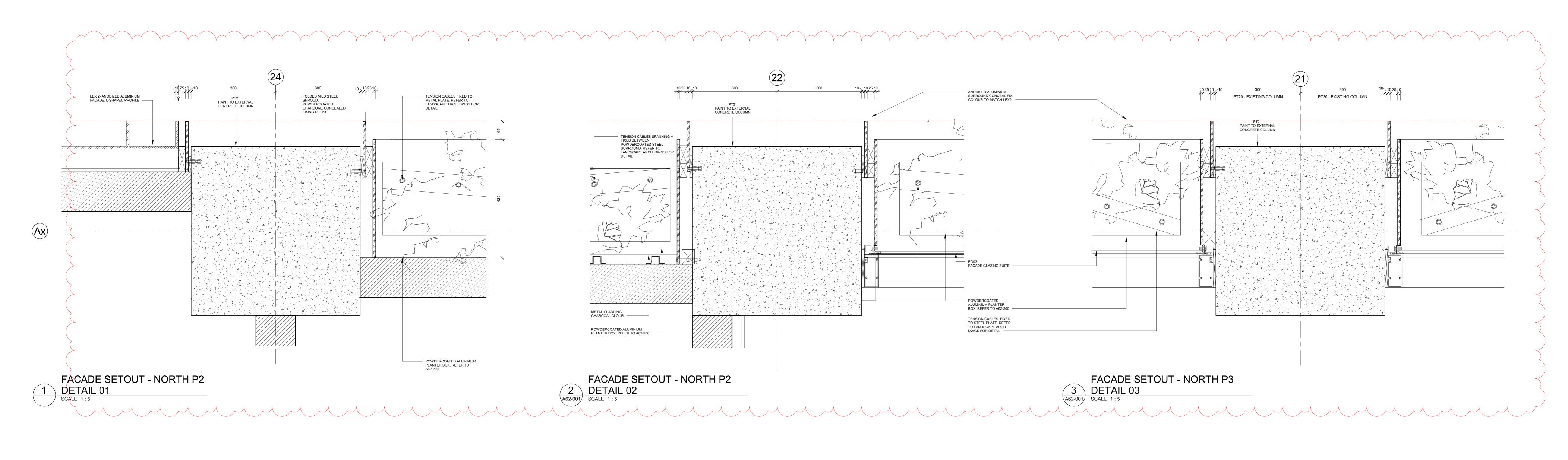






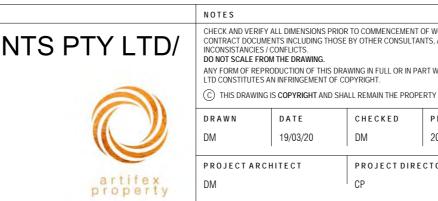






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1         ISSUE FOR TENDER         DM         01/11/19           2         COUNSIL RFI         DM         20/03/20	PROJECT MANAGEMENT       HYDRAULICS & FIRE PROTECTION CONSULTANTS       TOWN PLANNER         NAME: INITIAL CAPITAL PROJECTS PTY LTD       ADDRESS: IS NORMAN DISNEY & YOUNG       ADDRESS: USUBLE VIC 3000         ADDRESS: USUBLE VIC 2,161 COLLINS ST, MELBOURNE VIC 3000       ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303       TOWN PLANNER         TEL/FAX:       +61 398626800       TEL/FAX: +61 398626800       ADDRESS: USUBLE VIC AUSTRALIA 2000         STRUCTURAL AND CIVIL ENGINEERS       SURVEYOR       BUILDING SURVEYOR         NAME: AURECON       ADDRESS: 850 COLLINS ST, DOCKLANDS VIC AUSTRALIA 3008       ADDRESS: 16 EASTERN ROAD, P.O BOX 5075, SOUTH MELBOURNE, VIC 3205       NAME: PLP BUILDING SURVEYORS AND CONSULTANT	ZONE Q NEW CENTRAL INVESTMEN ARTIFEX PROPERTY GROUP
	TEL/FAX: +61 399753626     TEL/FAX: +61 396991400     TEL/FAX: +61 396507999       MECHANICAL ENGINEERS     QUANTITY SURVEYOR     FACADE CONSULTANT       NAME: NORMAN DISNEY & YOUNG     NAME: SLATTERY     ADDRESS: 115 BATMAN STREET, WEST MELBOURNE VIC AUSTRALIA 3303       TEL/FAX: +61 398626800     TEL/FAX: +61 396021313     ADDRESS: LEVEL 14, 160 QUEEN STREET, MELBOURNE VIC3000       ELECTRICAL ENGINEERS     LANDSCAPE DESIGN     ADDRESS: NORMAN DISNEY & YOUNG       NAME: NORMAN DISNEY & YOUNG     NAME: OCULUS PTY LTD     ADDRESS: 2ND FLOOR / 33 QUILDFORD LANE, MELBOURNE, VIC 3000       TEL/FAX: +61 3987264000     TEL/FAX: +61 396021313     TEL/FAX: +61 399753626	ZONE Q INVESTMENTS PTY LTD

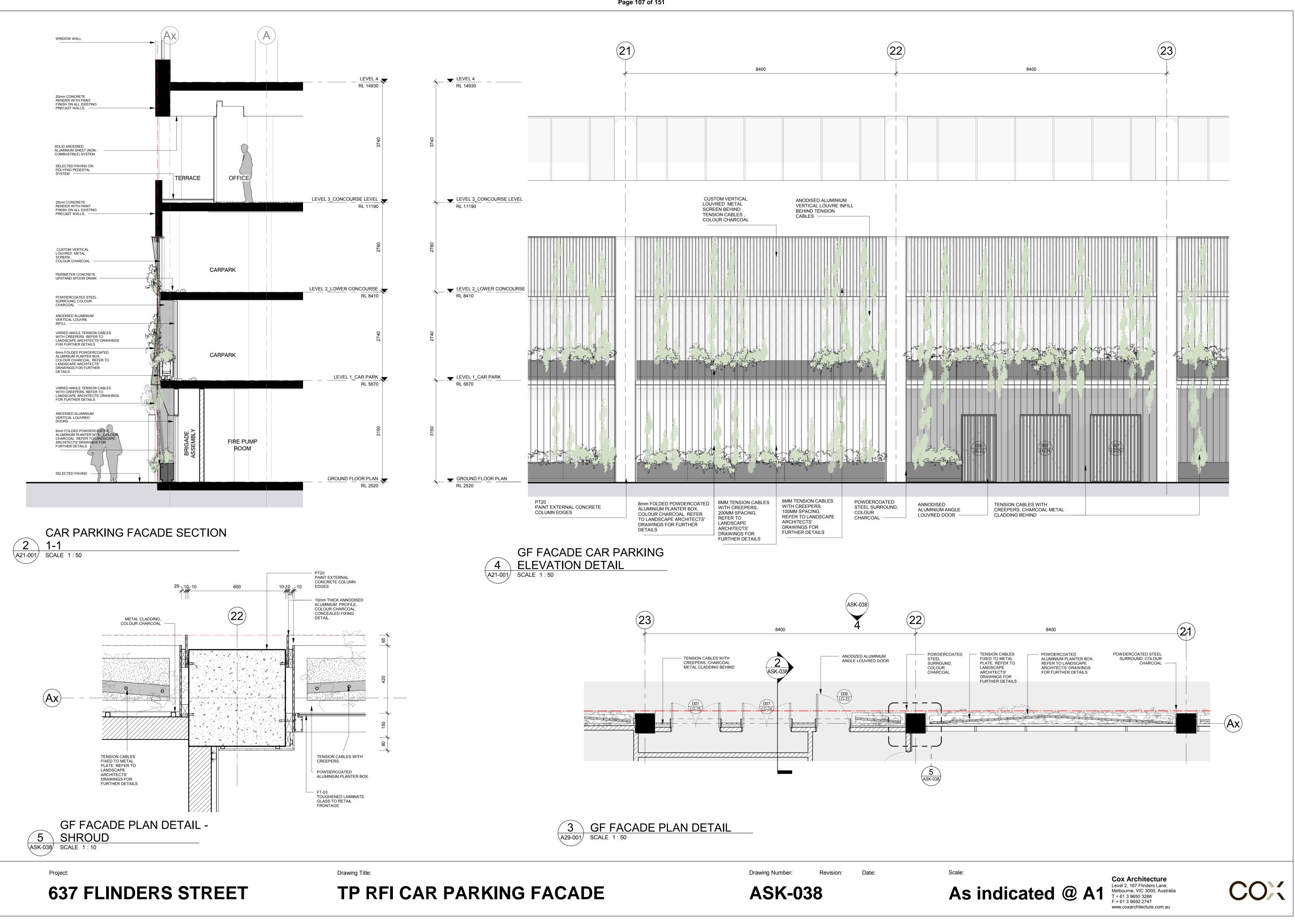
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### **637 FLINDERS STREET**

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Project:

### **TP RFI CAR PARKING FACADE 3D VIEW**

Drawing Number: Revision

**ASK-039** 

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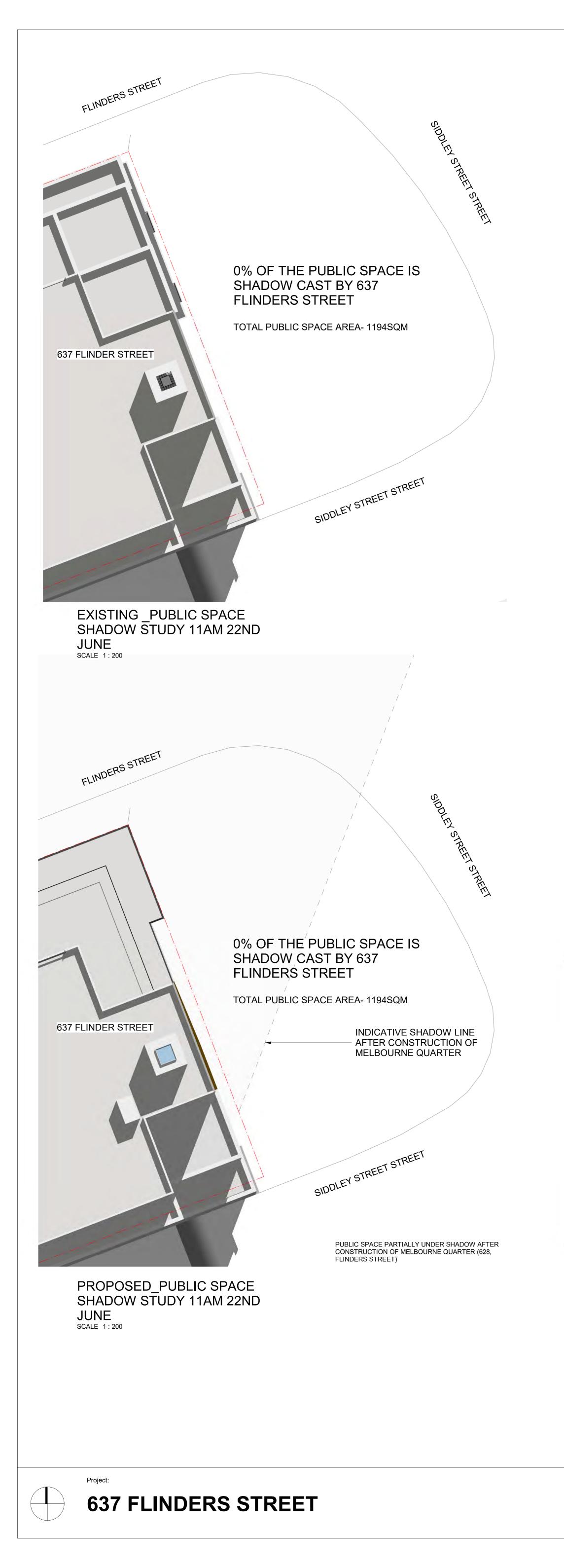
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**Cox Architecture** Level 2, 167 Flinders Lane, Melbourne, VIC 3000, Australia T + 61 3 9650 3288 F + 61 3 9650 2747 www.coxarchitecture.com.au

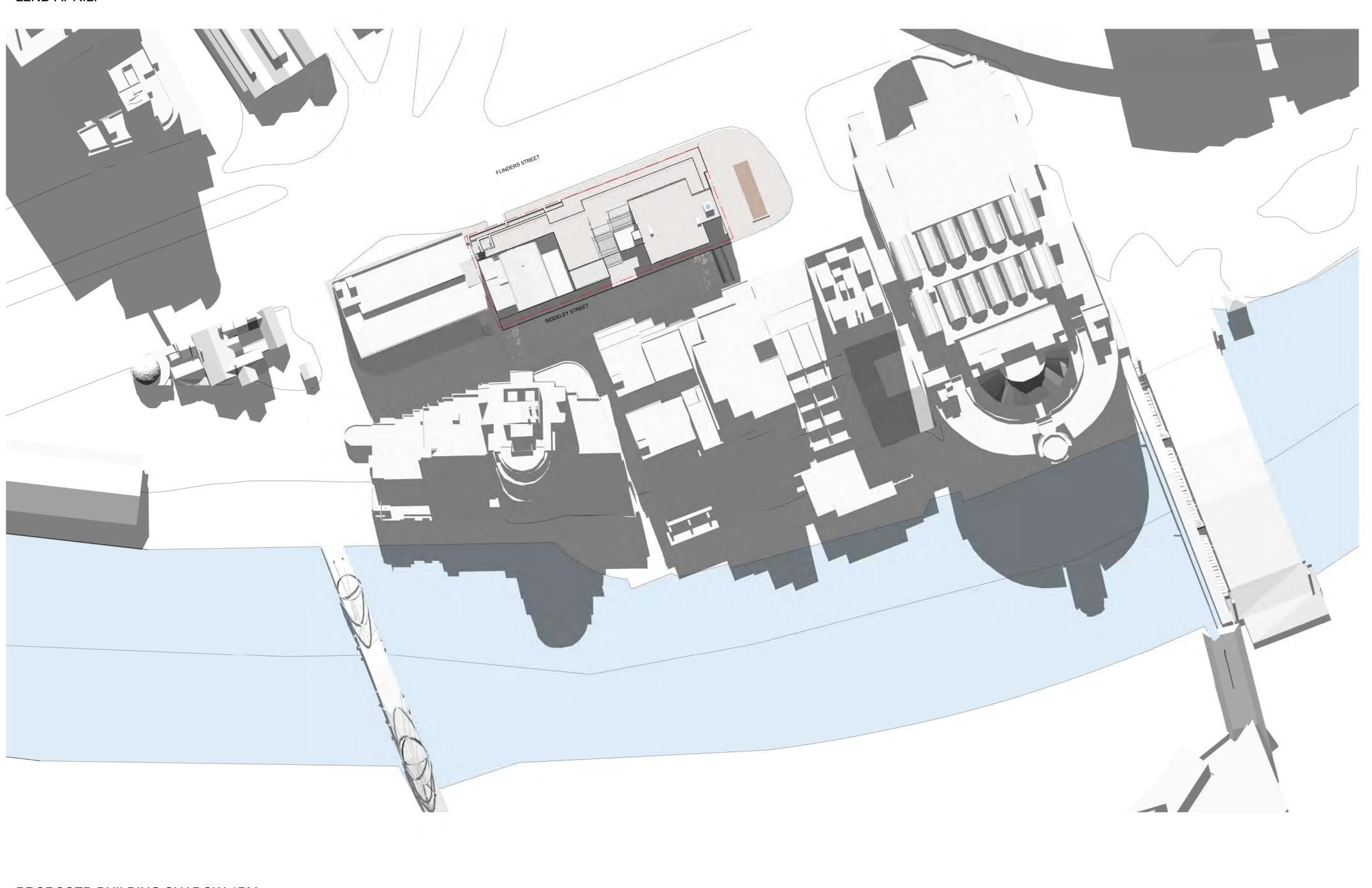




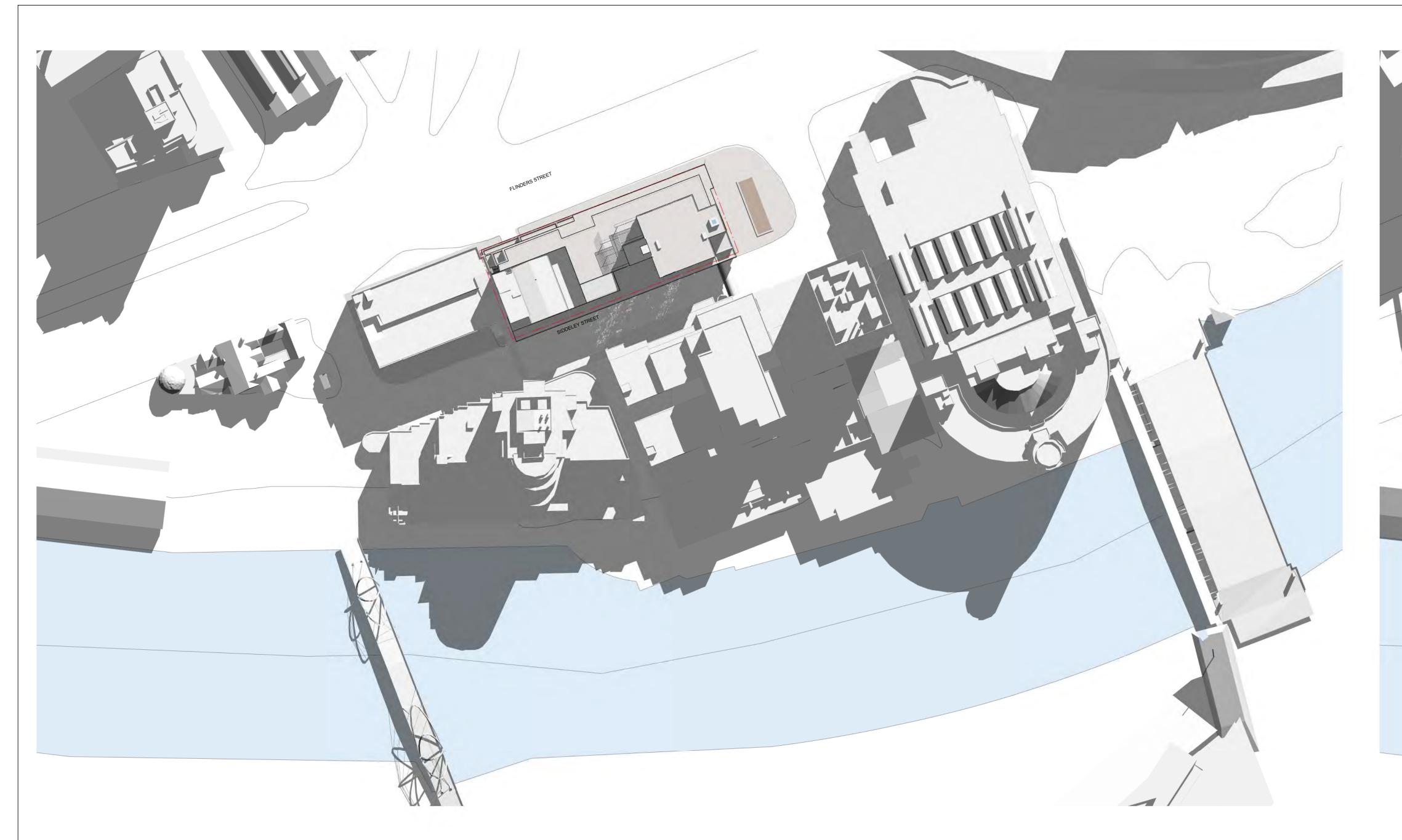




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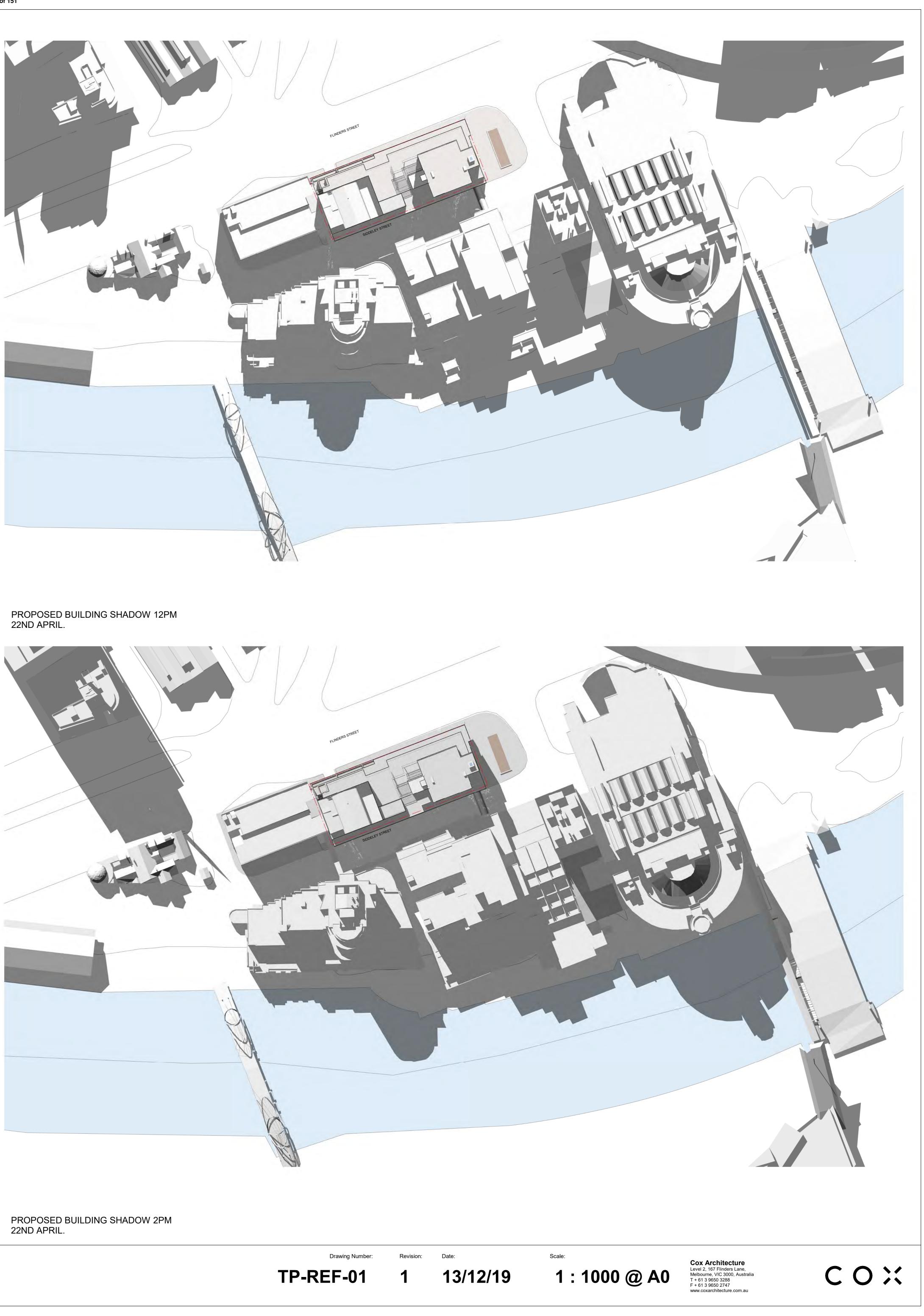
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# SOLAR STUDY\_APRIL

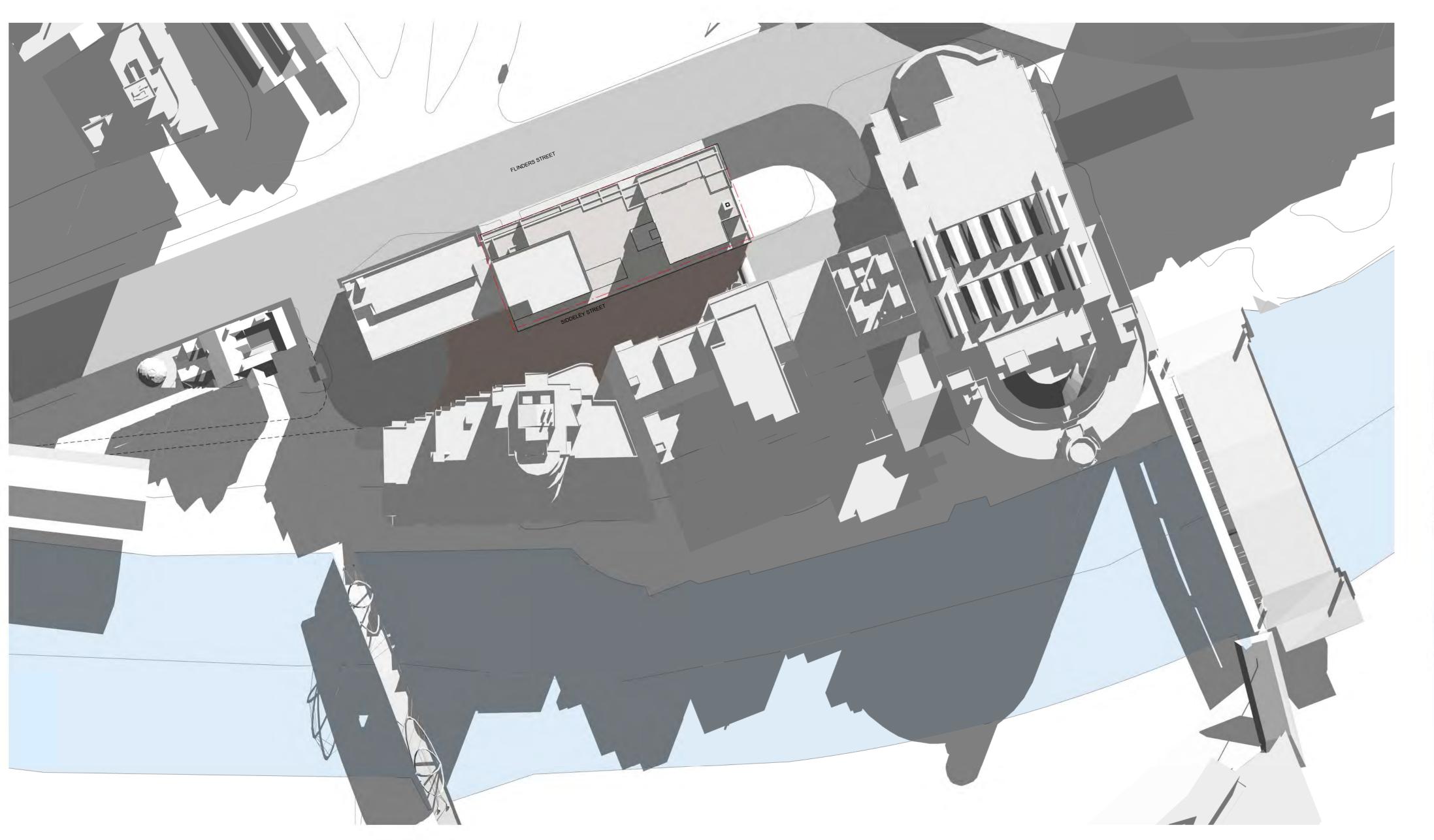
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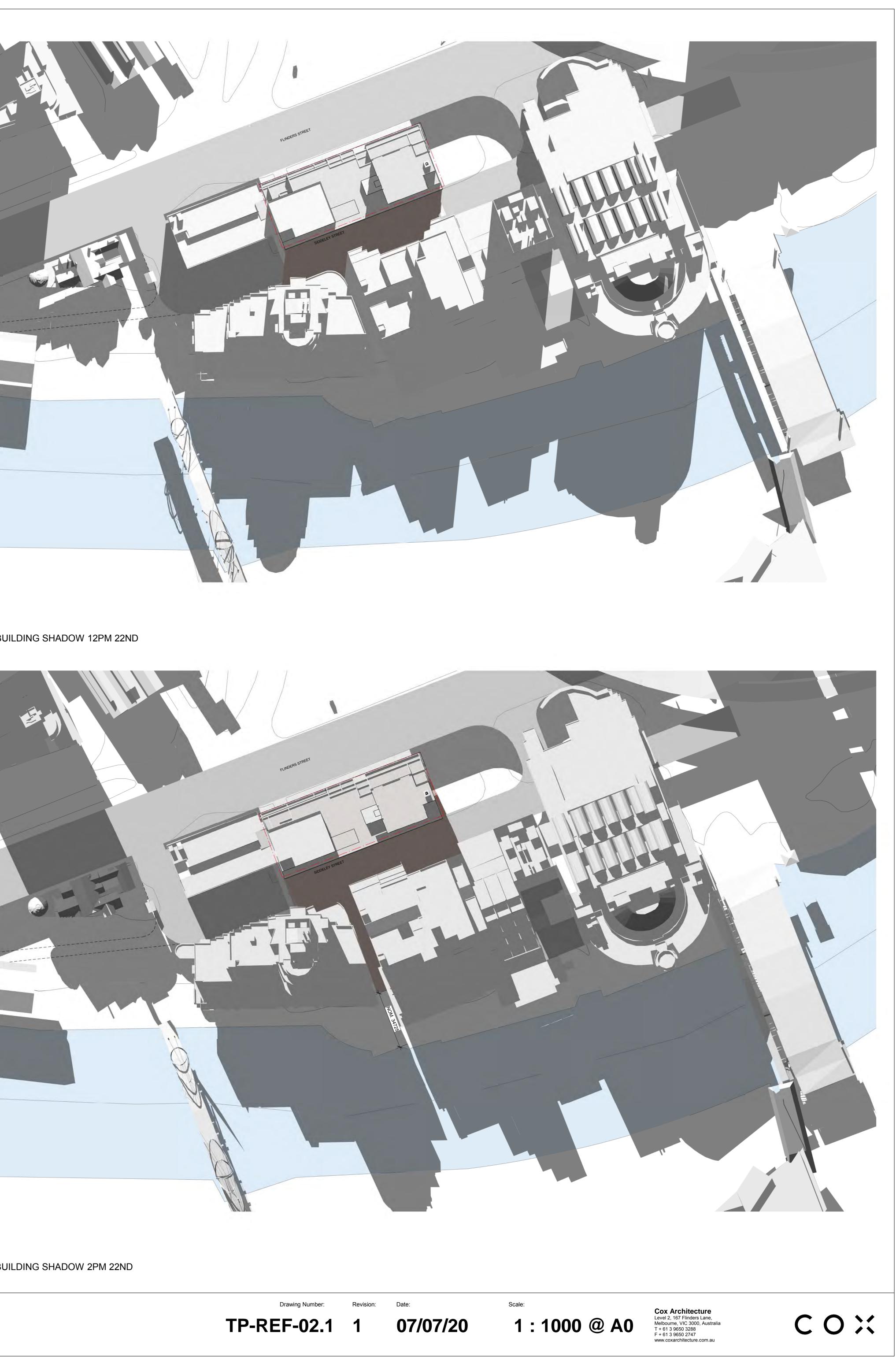
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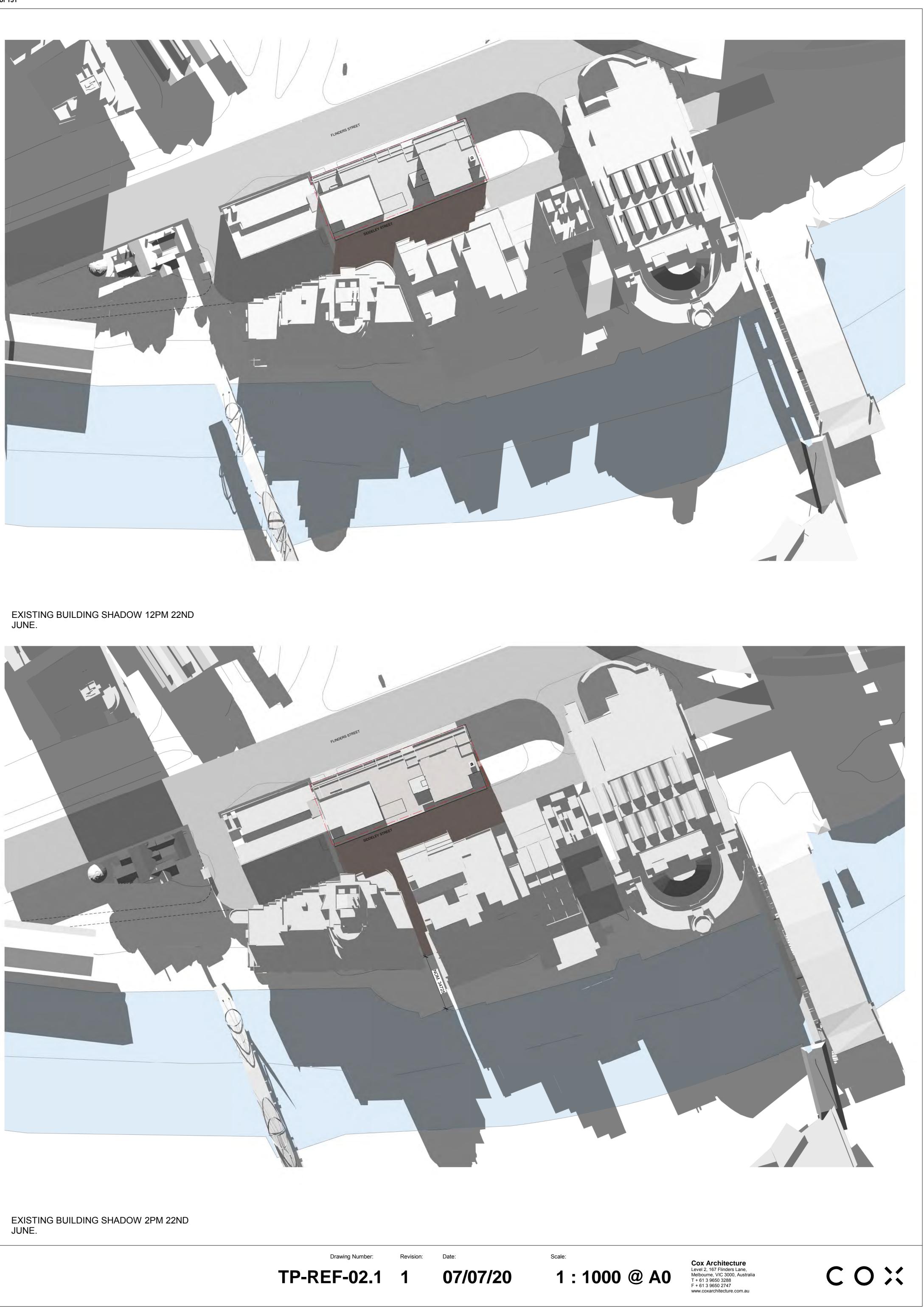










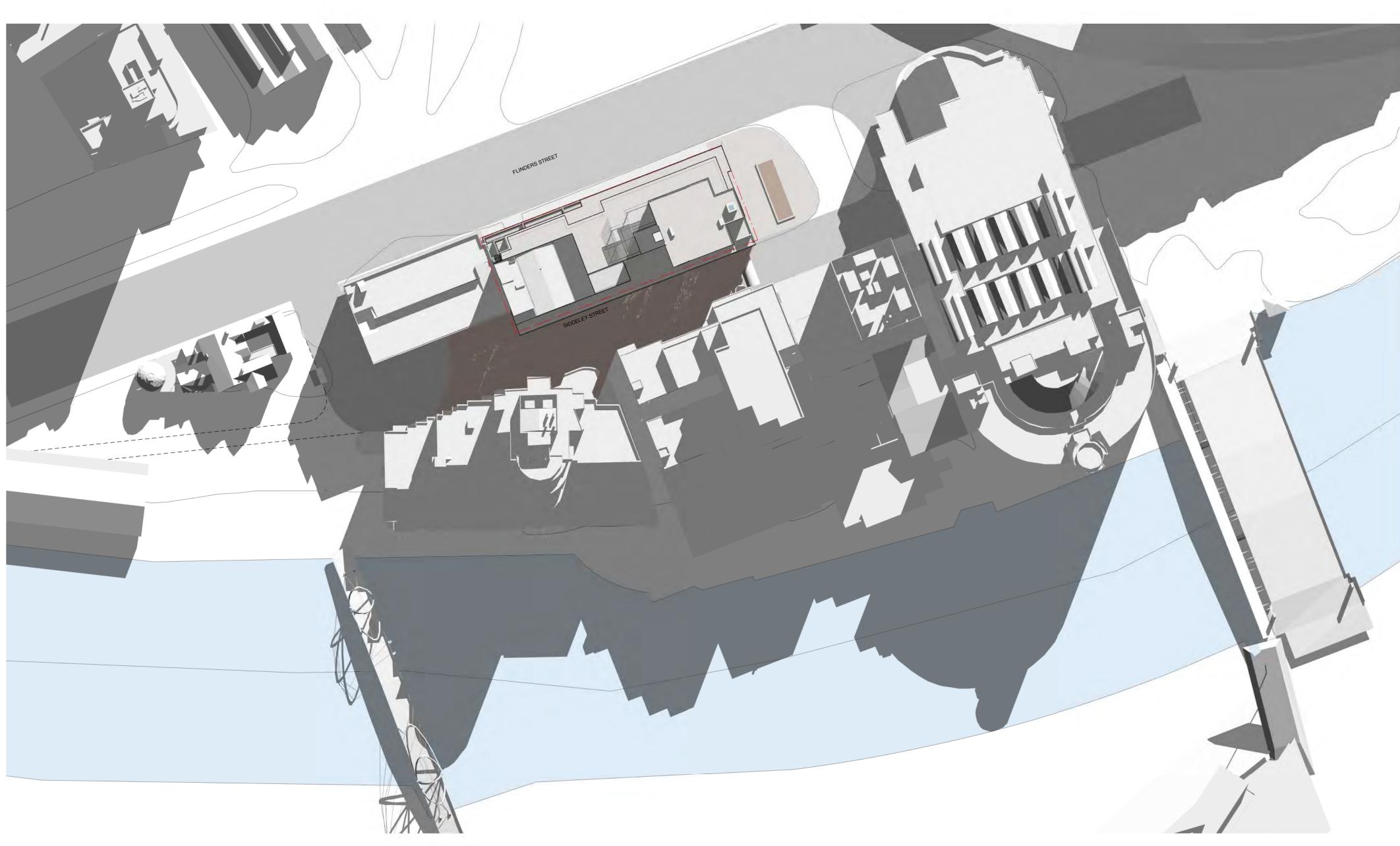




PROPOSED BUILDING SHADOW 1PM 22ND JUNE.

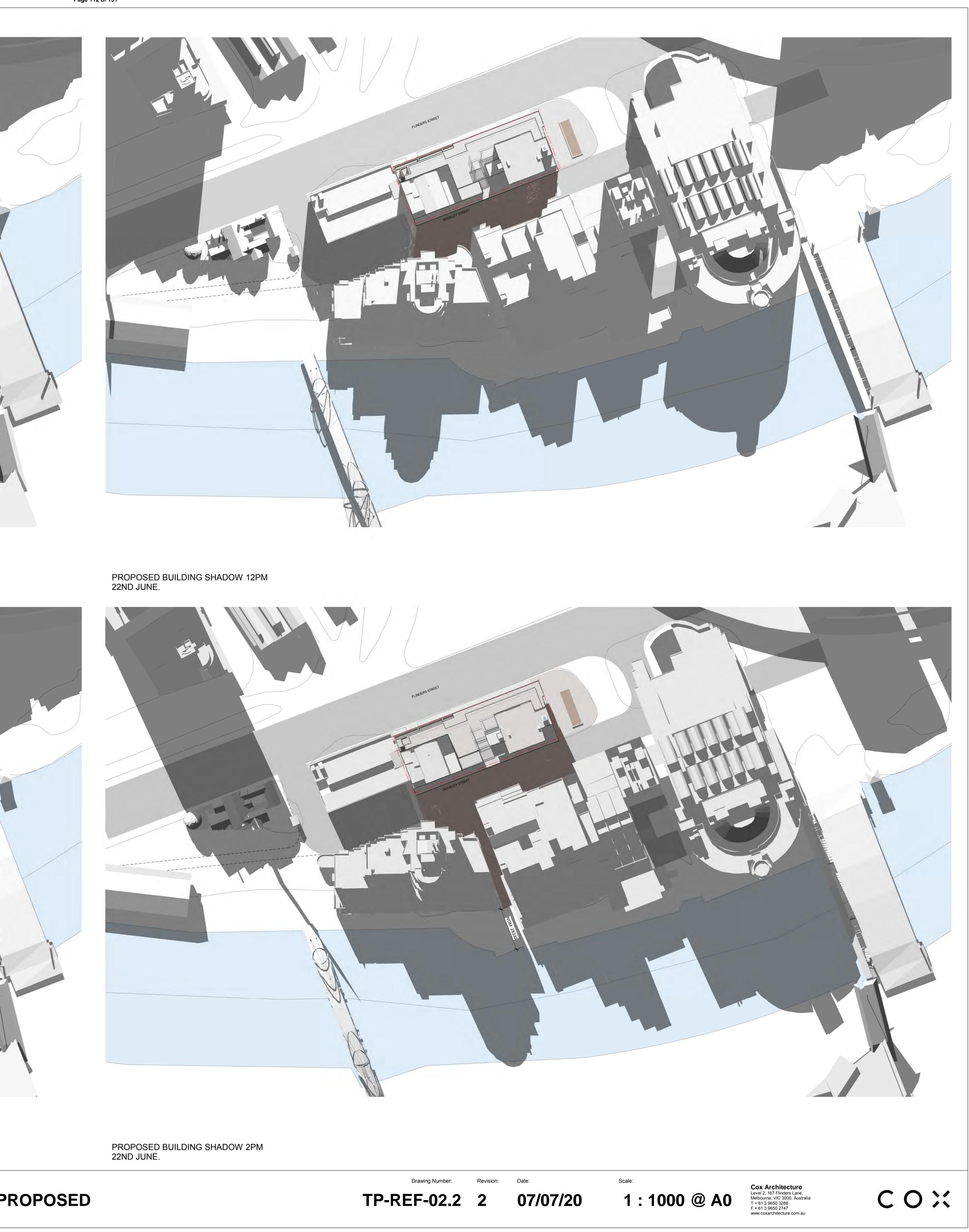


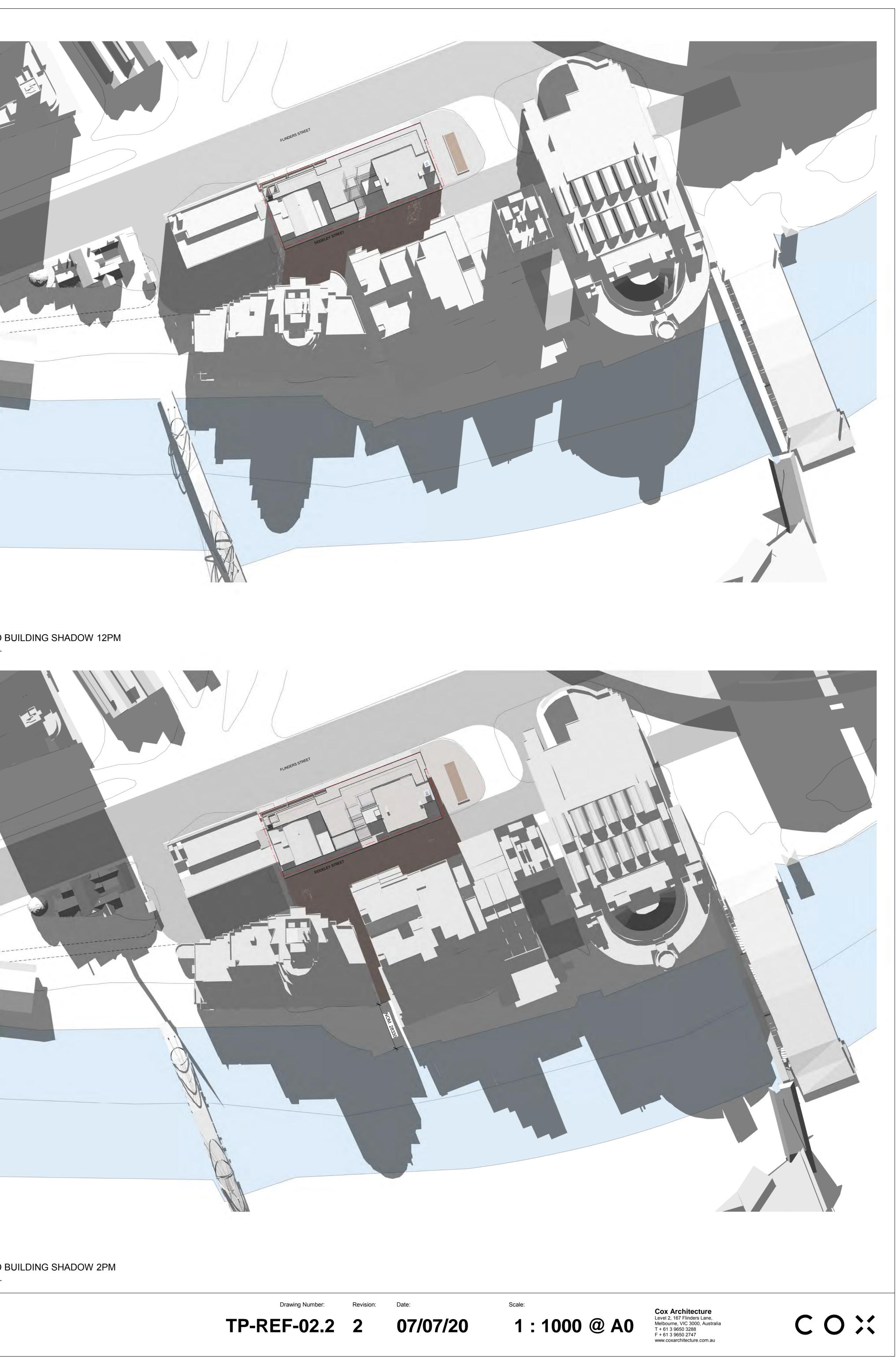
PROPOSED BUILDING SHADOW 11AM 22ND JUNE.



# SOLAR STUDY\_JUNE PROPOSED

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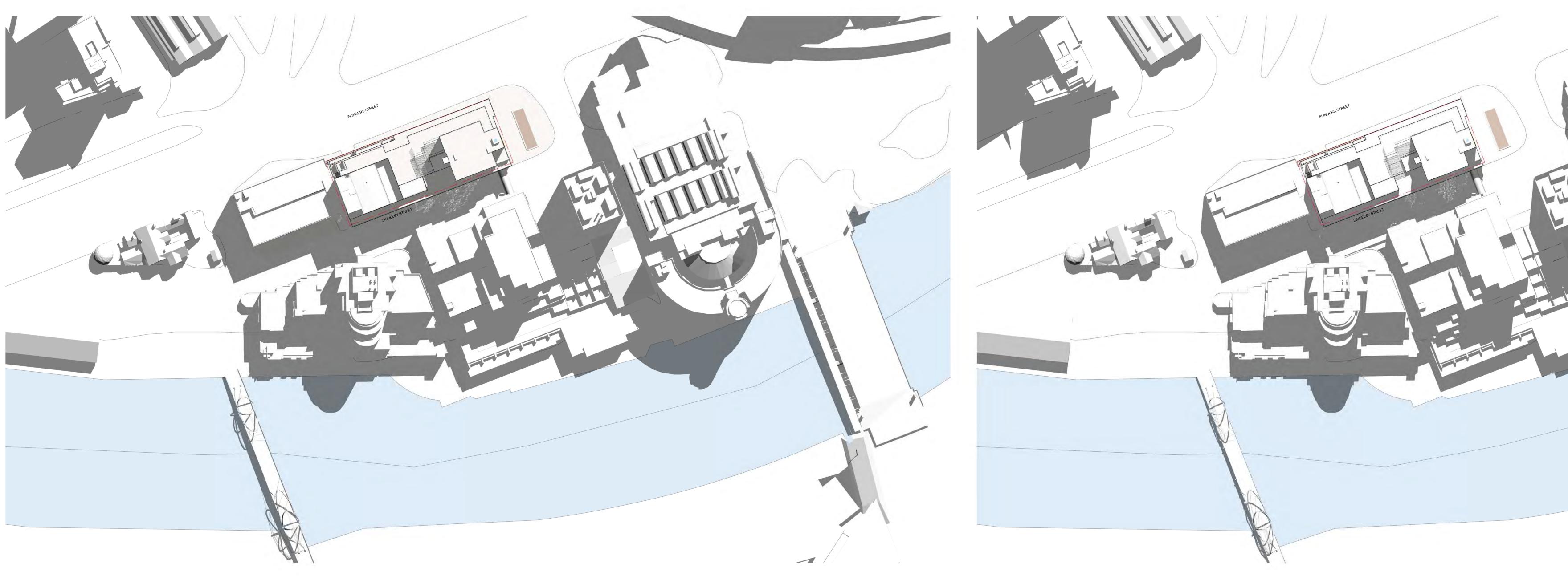




PROPOSED BUILDING SHADOW 1PM 22ND SEPT.



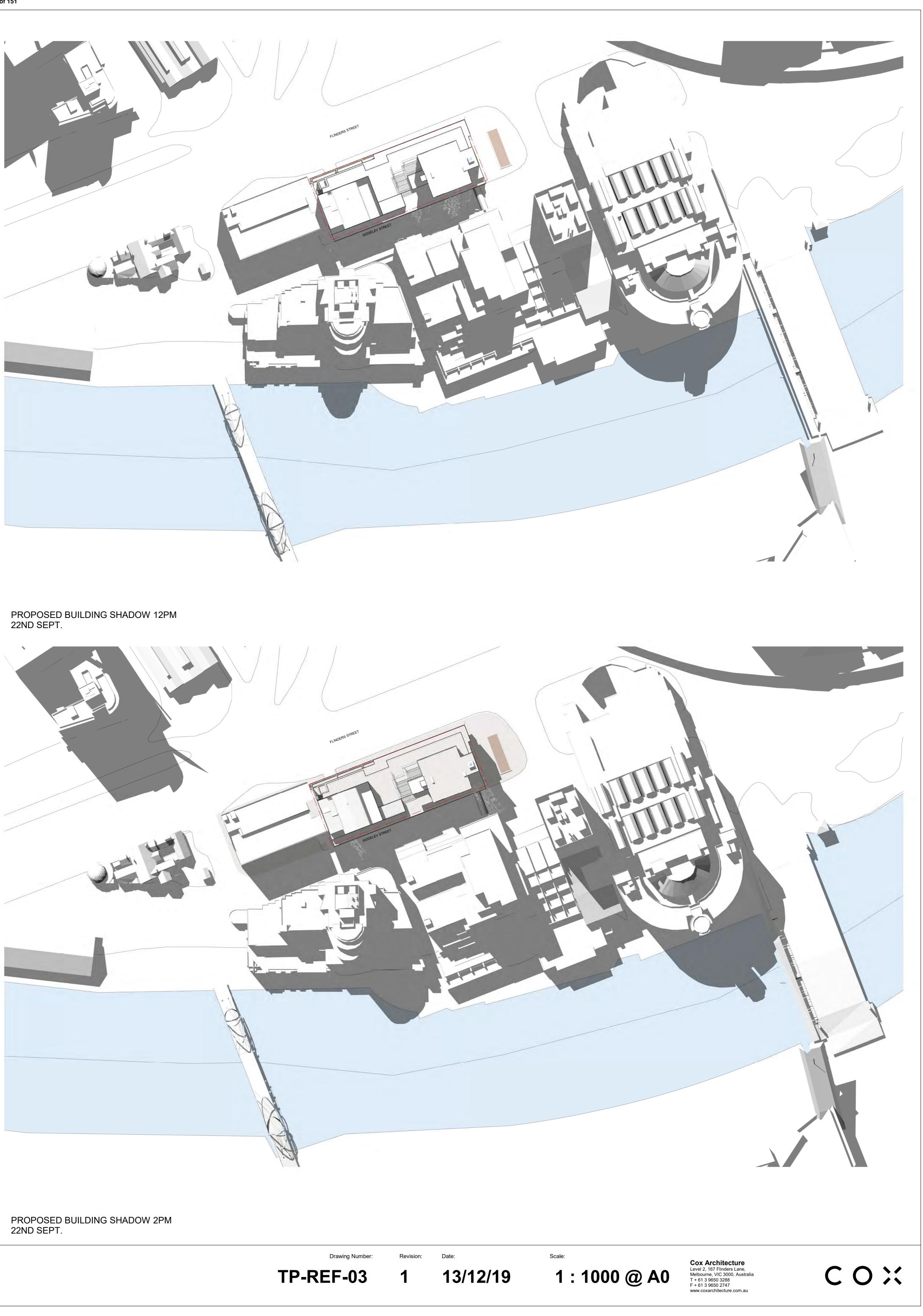
PROPOSED BUILDING SHADOW 11AM 22ND SEPT.



# SOLAR STUDY\_SEPT.

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## MINISTERIAL PLANNING REFERRAL

### **DELEGATE REPORT**

Application number:	TPMR-2019-27
DELWP Application number:	PA1900686
Applicant / Owner / Architect:	Ethos Urban / Zone Q New Central Investments Pty Ltd / Cox Architects
Address:	633-669 Flinders Street, Docklands VIC 3008
Proposal:	Ministerial Planning Permit Application PA1900686 for the partial demolition and buildings and works for refurbishment of the existing building.
Cost of works:	\$73,329,000
Date received by City of Melbourne:	9 October 2019
Responsible officer:	Markus Tschech, Senior Urban Planner
Report Date:	3 July 2020
(DM# 13713464)	

#### 1. SUBJECT SITE AND SURROUNDS

#### 1.1. The site

The subject site is located on the southern side of Flinders Street, between Spencer Street to the east and Siddeley Street to the west (and south). The site is formally described as Crown Allotment 6, Section 98, City of Melbourne Parish of Melbourne North (Vol. 11630, Fol. 575).

It is noted however, that the Siddeley Street road reserve, which adjoins the site to the south, has a maximum height of 9.58 metres AHD, above which Lot 2 on TP005183A (Vol. 11893, Fol. 436) exists. The existing building, constructed in 1983, extends partially into Lot 2 on TP005183A at levels 3 and above.

This matter has been discussed with Council's Surveyors and Property Team who have confirmed that the existing building does not occupy Council or Crown land; that the air space forms part of Lot 2 on TP005183A, and that a boundary realignment is required so that the title boundaries reflect the existing built form. The permit applicant has advised that an application will shortly be lodged with the City of Melbourne to realign the title boundary to match the existing building. (This matter is also addressed by condition 8 in section 9 of this report).

The site is rectangular in shape with a frontage of 104 metres to Flinders Street, a depth of 40.46 metres and a total site area of 4,209m<sup>2</sup>.

The subject site is improved by a 14 storey concrete office building, known as 'Tower 1' of the World Trade Centre complex, and is occupied by Victoria Police. The first

three levels of the building are primarily used for parking, with offices on levels 3-13. The existing building has an enclosed pedestrian bridge across Siddeley Street to the building to the south.

The permit applicant has advised that an application will shortly be lodged with the City of Melbourne to realign the title boundary to match the existing building.

The site is not encumbered by any easements, agreements or restrictions on title. The accompanying Title Plan TP302822K does, however, include conditions guaranteeing rights in relation to natural resources.

The site is located within an area of Aboriginal Cultural Heritage Sensitivity.

An inspection of the site and surrounding area was undertaken on 10 June 2020.



Figure 1 - Site Context Map

Attachment 4 Agenda item 6.2 Future Melbourne Committee 21 July 2020

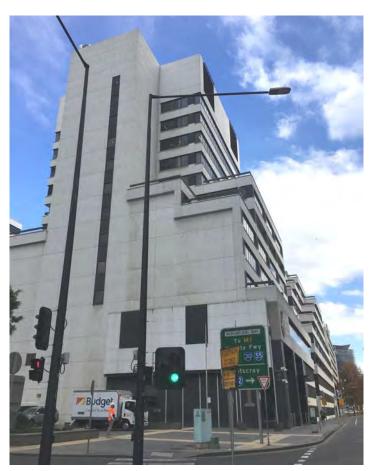


Figure 2 - View of site from the north-east, along Flinders Street

Attachment 4 Agenda item 6.2 Future Melbourne Committee 21 July 2020



Figure 2 - View west along Siddeley Street, with subject site on right



Figure 3 - View east along Flinders Street, with subject site on right

#### 1.2. Surrounds

The existing building forms part of the World Trade Centre comprising five concrete office buildings ranging in height from eight to fourteen storeys and interconnected with an above-ground concourse, constructed in 1983.

The site is located near the southern edge of the Docklands precinct between Flinders Street to the north and the Yarra River less than 100 metres to the south.

In terms of direct abuttals:

 To the north of the site is Flinders Street as it intersects with Wurundjeri Way. A tram line, serviced by Routes 35, 70 and 75, is located in the centre median, with a tram stop located immediately opposite the site. The site has a double width crossover to Flinders Street.

Across Flinders Street to the north is the Melbourne Quarter development. This development is partially completed and will ultimately comprise four multi-storey commercial buildings and two residential buildings.

To the **south** is Siddeley Street, which provides a second vehicular access point to the site and on-street parking.

Across Siddeley Street to the south, at 18-38 Siddeley Street, is the remainder of the World Trade Centre, which contains a number of offices, retail premises and a commercial carpark.

Also to the south is the Flinders Wharf Apartments at 40-66 Siddeley Street. This site contains a 19 storey residential building with 297 dwellings.

- Adjoining the site to the east is the World Trade Centre Plaza, which is private land that provides a road connection between Flinders Street and Siddeley Street.
- Adjoining the site to the west, at 671-701 Flinders Street, is a ten storey commercial carpark containing 690 spaces that forms part of the World Trade Centre. This site benefits from Planning Permit No. TP-2018-722 for an 'Urban Farm' on the rooftop.



Figure 4 - View north-east across World Trade Centre plaza and Siddeley Street

Attachment 4 Agenda item 6.2 Future Melbourne Committee 21 July 2020



Figure 5 - Melbourne Quarter buildings R1 (left) and MQ2 (right)



Figure 6 - 3D render of the approved 'Melbourne Sky Farm' at 671-701 Flinders Street

### 2. BACKGROUND AND HISTORY

#### 2.1. **Pre-application discussions (PA-2019-255)**

The permit applicant met with Department of Environment, Land, Water and Planning (DELWP) and Council Officers on 5 June 2019 to discuss the proposal.

A number of matters to be resolved were raised at the meeting, including:

- Resolution of title anomalies; and
- Activation of the public realm.

#### 2.2. Planning application history

There is no relevant planning application history for the subject site.

#### 3. THE PROPOSAL

The application seeks planning permission for the partial demolition and buildings and works for refurbishment of the existing building. The proposal primarily involves a variation to the building envelope and appearance, an increase in office floor space, a reduction in the number of car spaces and introduction of ground level retail tenancies.

The plans that have been considered in this assessment are those prepared by Cox Architects dated 1 November 2019 and 19 March 2020 (Drawings A01-000 – A40-080).

The proposal can be summarised as follows:

- Partial demolition of the external building, including all windows and frames, large parts of the northern façade and some areas of the eastern façade.
- An extension to the built form towards the eastern end of the Flinders Street frontage on levels 3-14, creating an additional 1,177m<sup>2</sup> of Gross Floor Area (GFA).
- Replacement of the majority of car parking spaces and Victoria Police lobby and service areas at ground level with:
  - A large commercial lobby accessible via entries to both Flinders Street and Siddeley Street.
  - A 670m<sup>2</sup> co-working space fronting Flinders Street.
  - A 323m<sup>2</sup> retail tenancy fronting Siddeley Street.
  - A further two retail tenancies within the commercial lobby, fronting Siddeley Street.
  - A secondary commercial lobby at the western end of the building, with entries off both Flinders Street and Siddeley Street.
  - Building services.
- Replacement of the car parking spaces in the eastern half of the building on level 1 with a co-working space, bicycle parking and end-of-trip facilities.
- Internal reconfiguration of levels 3-13 to accommodate flexible office space.

Further details of the proposal can be summarised as follows:

	Existing	Proposed	
Building height 15 storeys / 58.8 metres (incl. plant)		15 storeys / 58.8 metres (incl. plant)	
Gross Floor Area (GFA)	41,613m²	42,790m <sup>2</sup>	
Net Lettable Area (NLA)	23,927m²	27,270m <sup>2</sup>	
Floor Area Ratio (FAR)	9.89:1	10.16:1	
Office Space	-	21,508m <sup>2</sup>	
Co-Working Space -		1,842m²	
Retail Premises -		619m²	
Car spaces	263	121	
Motorcycle spaces Unknown 5		5	
Bicycle parking Unknown 202		202	
Vehicular accessDouble width crossovers to Flinders and Siddeley Streets		Double width crossovers to Flinders and Siddeley Streets	
Loading / unloading	Separate loading dock with a double width crossover to Siddeley Street	Separate loading dock with a double width crossover to Siddeley Street	

Relevant extracts of the proposed development are provided on the following pages.



Figure 7 - 3D render of the proposed development from the NE corner



Figure 8 - Proposed Demolition Elevation depicting extent of demolition to the northern facade. From Drawing A15-210

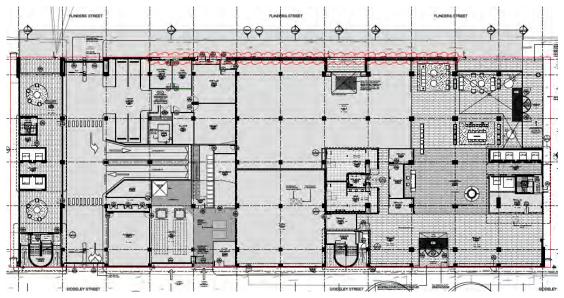


Figure 9 - Proposed Ground Level Plan. From Drawing A21-001

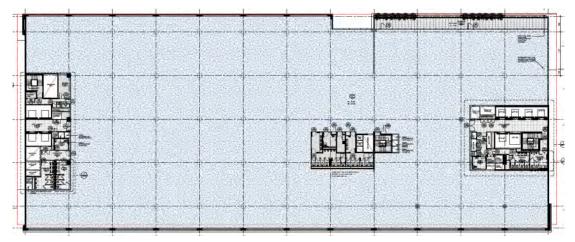


Figure 10 - Proposed Level 4 Plan. From Drawing A21-040

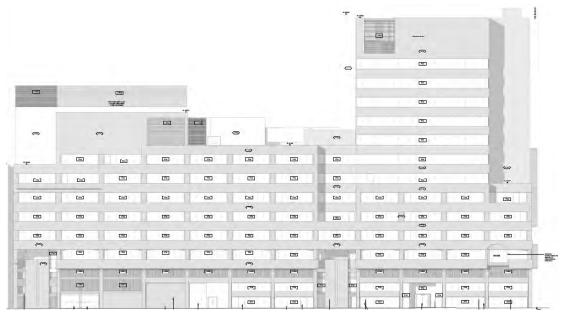


Figure 11 - Proposed South Elevation. From Drawing A30-020

#### 4. PLANNING SCHEME PROVISIONS

### 4.1. Statutory Controls

The following provisions of the Melbourne Planning Scheme apply:

State Planning	Clause 10: Planning Policy Framework	
Policies	Clause 11: Settlement	
	Clause 15: Built Environment and Heritage	
	Clause 17: Economic Development	
	Clause 18: Transport	
Municipal	Clause 21.04: Settlement	
Strategic Statement	Clause 21.06: Built Environment and Heritage	
	Clause 21.08: Economic Development	

	Clause 21.09: Transport
	Clause 21.10: Infrastructure
	Clause 21.13-2: Docklands
Local Planning Policies	Clause 22.01: Urban Design within the Capital City Zone
	Clause 22.02 Sunlight to Public Spaces
	Clause 22.19: Energy, Water and Waste Efficiency
	Clause 22.23: Stormwater Management (Water Sensitive Urban Design)

Statutory Controls		
Clause 37.04	A planning permit <b>is not</b> required to use the land for an Office or Retail	
Capital City Zone - Schedule 1	premises (other than Adult sex bookshop, Department store, Hotel, Supermarket and Bar).	
Outside the Retail Core	A permit <b>is required</b> to construct a building or construct or carry out works, and to demolish or remove a building or works.	
	A permit must not be granted to construct a building or construct or carry out works with a floor area ratio in excess of 18:1 unless a public benefit is agreed to by the Responsible Authority. This application does not seek permission for a building with an FAR above 18:1.	
Clause 43.02	A permit <b>is required</b> to construct a building or construct or carry out	
Design and	works, unless the Schedule states that a permit is not required.	
Development Overlay - Schedule 10	Schedule 10 does not exempt buildings and works from requiring a permit and sets out mandatory building height and setback requirements.	
General Development Area – Built Form		
Parking Overlay 1 Capital City Zone	A permit is required to provide car parking spaces in excess of the car parking rates in Clause 3.0 of this schedule.	
– Outside the Retail Core	The proposal provides 121 car parking spaces which is below the maximum requirement of 136 spaces. A permit is therefore <b>not required</b> pursuant to this Overlay.	

Particular Provisions		
Clause 52.06	Plans must be prepared to the satisfaction of the responsible authority	
Car Parking	before a new use commences and the floor area is increased.	
Clause 52.29	A permit is required to create or alter access to a road in a Road Zone,	
Land adjacent to	Category 1.	
a Road Zone,	The north-western corner of the site is adjacent to a Road Zone,	
Category 1, or a	Category 1. The proposal does not seek to vary current access	
Public Acquisition	arrangements. As such, no permit is required under this Clause.	
Overlay for a		
Category 1 Road		

1	
Clause 52.34 Bicycle Facilities	A new use must not commence or the floor area of an existing use must not be increased until the required bicycle facilities and associated signage has been provided on the land. A permit may be granted to reduce or waive the bicycle parking requirement.
	The proposal provides in excess of the minimum requirements and a permit <b>is not</b> required pursuant to Clause 52.34.
Clause 66.02-11	An application for an office development of 10,000 or more square
Integrated Public Transport Planning	metres of leasable floor area must be referred to the Department of Transport as a Determining Referral Authority.

General Provisions		
Clause 72.01 Responsible authority for administering and enforcing a provision of this planning scheme	The Minister for Planning is the Responsible Authority for this matter as the total floor area of the proposal exceeds 25,000 square metres.	
Clause 65 Approval of an application or plan	The responsible authority must decide whether the proposal will produce acceptable outcomes in terms of the decision guidelines of this clause, which include the matters set out in Section 60 of the Planning and Environment Act 1987.	

#### 4.2. Planning Scheme Amendments

#### 4.2.1. Amendment C308

Amendment C308 proposes new urban design provisions to the Melbourne Planning Scheme by:

- Deleting Clause 22.01 (Urban Design within the Capital City Zone).
- Replacing Schedule 1 to the Design and Development Overlay (Active Street Frontages) with a new Schedule 1 (Urban Design within the Central City and Southbank).
- Deleting Schedule 4 to the Design and Development Overlay (Weather Protection) and incorporating these provisions into the new DDO1.

Council has considered the panel report and submitted a final amendment to the Minister for Planning for approval, as such Amendment C308 is considered to be seriously entertained.

#### 5. PUBLIC NOTIFICATION

Pursuant to Schedule 1 of the Capital City Zone, this application is exempt from the notice requirements of Section 52 (1) (a), (b) and (d), the decision requirements of Section 64 (1), (2) and (3) and the review rights of Section 82 (1) of the Act.

Pursuant to Schedule 10 of the Design and Development Overlay an application to construct a building or construct or carry out works is exempt from the notice requirements of Section 52(1)(a), (b) and (d), the decision requirements of Section 64(1), (2) and (3) and the review rights of Section 82(1) of the Act.

The Minister for Planning has referred the application to the City of Melbourne as a Recommending Referral Authority in accordance with Section 1.0 of the Schedule to Clause 66.04 (Referral of permit applications under local provisions).

#### 6. **REFERRALS**

The application was referred to the following areas of the City of Melbourne for comment with the responses provided below.

#### 6.1. Urban Design

Initial urban design comments were provided to the permit applicant on 28 October 2019 and noted overall support for the application; in particular the reuse of the existing building, reduction of car parking, greater diversity of functions and pedestrian permeability. The advice did, however, note concerns with the:

- Lack of articulation within the additions;
- Extent of inactive façade along the street frontages;
- Legibility of the secondary pedestrian entrance at the western end of the site;
- Reduced extent of landscaped terraces (compared with the design presented at the pre-application meeting);
- Retention of car parking within the podium;
- Use of dark metal louvres to screen the car parking areas;
- Use of metal cladding to screen the building services areas;
- Detailed design of the shopfronts; and
- Lack of weather protection to the public realm.

The amended drawings which formed part of the RFI response on 15 December 2019 and 25 March 2020 were also forwarded to Council's Urban Design Team for review and comment. Although some matters have been addressed, the concerns relating to the articulation, materiality, secondary entry and weather protection remain.

#### 6.2. Land Survey

Council's Land Surveyor reviewed the original application, as well as the RFI response of 15 December 2019. They noted a number of concerns relating to the title boundary, proposed windows on boundary, property access and Council liability in relation to the pedestrian bridge.

These issues do not impact the design of the development and can be resolved via conditions recommended by Council's Land Surveyor.

#### 6.3. City Design

Having provided feedback on the original application in relation to the width of the Siddeley Street footpath, Council's Principal Landscape Architect provided the following comments on 9 April 2020:

'Point 16 in the response addresses the issue raised by City Design concerning the pedestrian conditions in Siddeley Street. We advise that the response is reasonable and constructive, referring to a current working group (that we were unaware of) and to the overarching need for a masterplan and for the City of Melbourne's implementation. As stated, most of this is outside the applicant's responsibility. This issue is discussed in a similar way in points 37 and 40.

Point 23 in the response refers to maintenance of the proposed landscape works and we are satisfied with this response. In this context, please note our advice of 16 January 2020 which refers to a condition to cover management.'

Council's Principal Landscape Architect also recommended a condition relating to the landscape plan which will be recommended for inclusion on any permit issued.

#### 6.4. ESD & Green Infrastructure

Council's ESD & Green Infrastructure Officer provided feedback in relation to the original application, as well as revised versions of the ESD Report prepared by Norman Disney & Young. They advised on 5 June 2020 that the most recent version of the report, dated 12 February 2020, was generally acceptable and provided conditions requiring the development to be undertaken in accordance with the report, which will be recommended for inclusion on any permit issued.

#### 6.5. Urban Forestry

Council's Urban Forester provided the following comments on 29 October 2019:

'The most significant public trees are adjacent to the site's southern elevation within the Siddeley Street central median. Should a permit be issued, these trees will need to be protected during the works, including any civil upgrades.

There appears to be scope for new tree plots within the Flinders Street footpath. The applicant should undertake a feasibility study to demonstrate the viability of new plots, especially in relation to any existing or proposed below ground utilities.'

Conditions that address these matters were provided on 26 June 2020, and will be recommended for inclusion on any permit issued.

#### 6.6. Traffic

Final comments were received from Council's Traffic Engineer on 9 April 2020. Most of the initial concerns have been addressed in an updated traffic report, though the following concerns, which the permit applicant does not agree with, remain:

- At least 1 car share and electric charging space should be provided on-site.
- A Loading Management Plan (LMP) must be prepared, specifying how the access/egress of loading vehicles is to be managed.
- The developer should fund the reconstruction of Siddeley Street.
- A formal independent desktop Road Safety Audit of the proposed development must be undertaken prior to construction, at the developer's expense, which should include the vehicular/bicycle/pedestrian access arrangements, loading arrangements, internal circulation/layout and all works within the public realm.

The recommended conditions are discussed at Section 7.4 of this report.

#### 6.7. Civil Design

Final comments were received from Council's Infrastructure Engineer on 15 May 2020. All matters, including removal of the Victoria Police security bollards at the building entry and a tram pole located centrally within the footpath can be addressed via conditions.

It is noted however, that the permit applicant opposes the following condition:

 The vehicle crossings shall be redesigned in alignment with the driveways in Flinders Street and Siddeley Street.

This matter is discussed at Section 7.4 of this report.

#### 6.8. Waste Engineering

Council's Waste Engineer advised on 15 April 2020 that the Waste Management Plan (WMP) prepared by Impact dated 27 February 2020 was acceptable.

#### 7. ASSESSMENT

The key issues in the consideration of this application are:

- Built form
- Public realm
- Sustainability
- Traffic and engineering
- Other matters, including Aboriginal Cultural Heritage and Land Survey issues

Each is addressed in turn below.

#### 7.1. Built Form

The subject site is located within the 'Batman's Hill' precinct of Docklands, where Clause 21.13-2 seeks, inter alia, to:

- Ensure Docklands is physically and visually linked with the west end of the Hoddle Grid.
- Ensure that buildings provide weather protection and an attractive built form to promote an attractive, vibrant, safe and comfortable street environment.
- Ensure that the design of buildings encourages sustainable outcomes.
- Encourage a built form profile in the Docklands that forms an extension of the Hoddle Grid building profile.
- Ensure development in Docklands extends and reinforces Council's public environment initiatives and practices.

Clause 22.01 (Urban Design within the Capital City Zone) also provides guidance on the design of new buildings. Objectives that relate to the proposal seek:

- To enhance the physical quality and character of Melbourne's streets, lanes and Capital City Zone form through sensitive and innovative design.
- To ensure developments contribute to a high quality public realm and to passive surveillance of the public domain.

 To improve the experience of the city for pedestrians by providing a human scale to the street wall, weather protection, sunlight access, summer shadow, comfortable wind conditions, active [sic].

Schedule 10 to the Design and Development Overlay (DDO10), which applies to the site, provides the following additional Design Objectives:

- To ensure development achieves a high quality of pedestrian amenity in the public realm in relation to human scale and microclimate conditions such as acceptable levels of sunlight access and wind.
- To ensure that development respects and responds to the built form outcomes sought for the Central City.
- To encourage a level of development that maintains and contributes to the valued public realm attributes of the Central City.
- To ensure that new buildings provide equitable development rights for adjoining sites and allow reasonable access to privacy, sunlight, daylight and outlook for habitable rooms.
- To provide a high level of internal amenity for building occupants.
- To ensure the design of public spaces and buildings is of a high quality.
- To encourage intensive developments in the Central City to adopt a podium and tower format.

An assessment of the proposal, having regard to the Design Objectives and Built Form Requirements of DDO10 follows.

#### 7.1.1. Street Wall Height

'Total building height' is defined at Section 2.0 of DDO10 as:

"...the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the building, with the exception of non-habitable architectural features not more than 3.0 metres in height and building services setback at least 3.0 metres behind the façade."

'Street wall' is defined as:

"...any part of the building constructed within 0.3 metres of a lot boundary fronting the street."

'Street wall height' is defined as:

"...the vertical distance between the footpath or natural surface level at the centre of the site frontage and the highest point of the street wall, with the exception of non-habitable architectural features not more than 3.0 metres in height and building services setback at least 3.0 metres behind the street wall."

The existing building has a height of 56.88 metres (not including the lift overrun), with street wall heights of up to 28 metres to Flinders Street and 31.8 metres to Siddeley Street, noting that the existing building extends up to 1.5 metres beyond its southern title boundary.

Pursuant to Table 3 to DDO10, the preferred street wall height is 20 metres, with a modified (mandatory) height of no greater than 40 metres (or 80 metres if certain criteria are met).

The relevant Built Form Outcomes, which are of increased importance where the preferred street wall height is exceeded, are as follows:

- Street wall height is scaled to ensure:
  - A human scale.
  - An appropriate level of street enclosure having regard to the width of the street with lower street wall heights to narrower streets.
  - Consistency with the prevalent parapet height of adjoining buildings.
  - Height that respects the scale of adjoining heritage places.
  - Adequate opportunity for daylight, sunlight and skyviews in the street.
  - Definition of main street corners and/or public space where there are no significant impacts on the amenity of public spaces.
  - Maintenance of the prevailing street wall height and vertical rhythm on the street.

The proposed works would maintain the existing street wall height to Siddeley Street and increase the street wall height to Flinders Street from 28 metres to 31.09 metres. The revised street wall height is less than the 40 metre maximum, though greater than the 20 metre preferred height; and an assessment against the relevant Design Objectives and Built Form Outcomes is warranted. They have been grouped and addressed as follows:

#### Human Scale and Pedestrian Amenity

The Flinders Street frontage is characterised by three levels of car parking, with offices and some narrow terraces above.

This frontage will be substantially redesigned with active uses (including building entries, office and terraces) located along the ground level. Above ground level, the proposed works will bring a fully glazed façade to the street frontage, providing an active frontage and increased opportunities for passive surveillance.

A condition requiring weather protection along both street frontages will further improve the pedestrian amenity.

It is noted that Council's Urban Design Team expressed concern with the scale of the glazed infill and recommended that some form of horizontal rebate be introduced to moderate the mass. The glazed infill is four storeys in height with an expressed slab edge on each level. It is separated from surrounding built form by a series of terraces. Notwithstanding the Urban Design Team's concern, the glazed infill is considered to be appropriately scaled and articulated within this context and the additional 3.09 metre street wall height is an acceptable design response.

#### **Street Enclosure**

This section of Flinders Street is relatively wide and presents as a generally hostile environment due to a lack of active frontages and high level of vehicular traffic. It is not heavily used by pedestrians.

Based on the substantial width of Flinders Street and adjoining buildings, it is considered that a podium height up to 31.8 metres can be comfortably accommodated within this location without overwhelming the street.

#### **Consistency with Adjoining Parapets**

The proposal involves a 3.08 metre increase in the street wall height at the eastern half of the building's Flinders Street frontage. As illustrated in Figure 12, this nine

storey street wall would step down to an eight storey street wall to the west, which would be retained.



Figure 12 - 3D Render showing proposed extension (curtain wall glazing) in relation to the existing street wall (white concrete and glazing)

In terms of wider context, the proposed 31.8 metre street wall also relates appropriately to the 27.1 metre high street wall of the building at 671-701 Flinders Street to the west.

#### **Contribution to the Public Realm**

The existing building's ground level frontage relates poorly to its Flinders Street frontage, with car parking extending across the majority of the interface.

The proposal would remove all but four car parking spaces at ground level and locate a new building lobby, secondary building entry and co-working office space along the Flinders Street frontage.

The existing and proposed conditions are provided in the images at Figure 13.



Figure 13 – Street view image of existing building (left) and 3D render of proposed ground level

As a result, the proposal would substantially improve the building's contribution to the public realm along Flinders Street.

Notwithstanding the above, as identified by Council's Urban Design Team, the secondary building entry is not sufficiently legible within the streetscape due to its low height and materiality. It is recommended that this matter be addressed via a condition requiring an increase in the height of the entry and provision of a canopy.

#### Daylight, Sunlight and Skyviews

Shadow diagrams prepared by the permit applicant, which depict the equinox and winter solstice, indicate that the shadow cast by the proposal falls almost entirely within shadows cast by the existing building. The only additional shadow caused by the taller street wall occurs to the land to the east, which currently functions as a public plaza (despite being private land), occurs after 2pm and represents two percent of the space.

Furthermore, the proposal by virtue of its minimal additional built form also meets the mandatory overshadowing controls of DDO10 and pedestrian amenity objectives at Clause 22.01 as discussed in Section 7.2 of this report.

The impact of the proposal on 'skyviews' is considered to be acceptable given the height and articulation of the street wall.

#### Definition of the Main Street Corners / Public Spaces

The subject site is not on a main street corner, though it does adjoin a parcel of private land which currently functions as public space. As depicted in Figure 2 on page 2 of this report, the building's eastern elevation to this public space is characterised by a solid concrete wall and minimal glazing.

The proposal would significantly improve this interface through:

- A larger and more transparent building entry at the north-eastern corner;
- The creation of a new retail tenancy at the south-eastern corner;
- The replacement of car parking with office space; and
- The introduction of a high level of glazing above ground level.

#### Maintenance of the Prevailing Street Wall Height

As noted in the sections relating to 'consistency with adjoining parapets' and 'human scale and pedestrian amenity', the 31.8 metre street wall height will sit comfortably within this section of Flinders Street.

#### 7.1.2. Building Setbacks

DDO10 encourages a robust, boundary-to-boundary built form at lower levels and provides recommended setbacks for buildings that include a tower or addition component above the street wall.

'Setback' is defined at Section 2.0 of DDO10 as:

'Setback is the shortest horizontal distance from a building façade, including projections such as balconies, building services and architectural features greater than 300mm, to the boundary.'

Design Element	Preferred Requirement	Modified Requirement (Mandatory)
Building setback(s) above street wall	Above the street wall, towers and additions should be setback 10 metres from the title boundary.	Above the street wall, towers must be setback a minimum of 5 metres from the title boundary.
Building setbacks from side boundaries and rear boundaries (or from the centre line of an adjoining laneway) and tower separation within a site	Above the street wall or 40 metres, towers should be setback a minimum of 5 metres or 6% of the total building height whichever is greater.	Above the street wall or 40 metres (where there is no street wall), towers and additions must be setback a minimum of 5 metres and must meet the design element requirements for tower floorplate.

Specifically, the preferred and mandatory setback requirements are as follows:

The tallest element of the existing building (not including the stairwell enclosure) is located towards the eastern boundary and is setback at least 10.1 metres from the northern boundary (front), 1.55 metres from the eastern boundary (side) and 1.82 metres from the southern boundary (rear). The side and rear setbacks are less than the five metre requirement of DD10. The existing non-compliance is depicted by the blue shading in Figure 14.



Figure 14 - Proposed North (left) and East (right) Elevations depicting extent of non-compliance of existing building with the setback requirements of DDO10

The proposed elements above the existing built form are:

- Pergolas to an outdoor terrace on level 9;
- A new generator room enclosure on level 9; and
- A new plant services enclosure on level 11 towards the western boundary.

These additions are highlighted in Figure 15 on the following page.

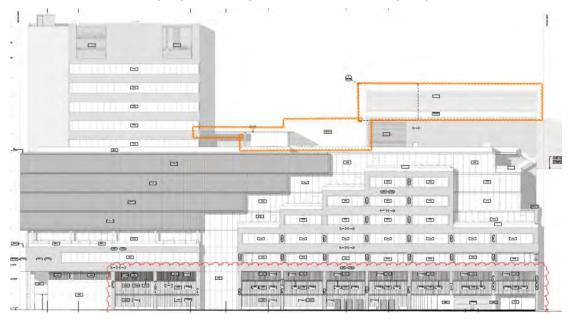


Figure 15 - Marked up East Elevation (A30-1010) highlighting additions above the street wall with orange clouds

In terms of compliance with the setback provisions of DDO10:

- The centrally located pergola structures on level 9 are setback at least 10.45 metres from the street wall below and therefore meet the setback requirements.
- Pursuant to Section 2.2 of DDO10, a permit is not required for:
  - Buildings and works to install or modify plant and service fixtures to an existing building.

The additional plant on levels 9 and 11 benefits from this exemption.

#### 7.1.3. Façade Design and Materiality

The proposal involves the refurbishment of the existing building and seeks to integrate contemporary built form elements.

The primary changes occur within the ground floor and podium levels, where solid concrete walls and mesh screens to the carpark are relaced with glazing to the lobby areas and office space.

Screening to some areas of car parking and building services comprise a mix of vertical wire screens with integrated planting and metal louvres.

The appearance and materiality of the refurbished building is, in the main, a significant improvement to the existing condition. Council's Urban Design Team has, however, expressed some concern with the materiality within the ground floor and podium levels, stating that:

'While the requirements for natural ventilation to the carpark may limit material selection, the dark coloured metal louvres puts forward a visually impermeable and low quality interface close to the public realm. While we acknowledge that planting wire is proposed to front this interface, we require the design quality of this facade to be of high quality on its own merits, should planting not propagate in the future. We are unconvinced that the proposed solution will provide a better public interface than what is existing.'

This matter can be resolved via conditions as part of a revised façade strategy and detailed landscape plans. Such conditions are recommended for inclusion on any permit issued.

#### 7.1.4. Streetscape Design

In accordance with the Application Requirements of the CCZ1:

'An application to construct a building or to construct or carry out works must include, as appropriate, upgrading of adjacent footpaths or laneways to the satisfaction of the responsible authority.'

The Siddeley Street footpath adjacent to the subject site has a width of as little as 1.1 metres, which is not conducive to pedestrian amenity or safety.

Council's Principal Landscape Architect and Infrastructure Engineer have both recommended that the footpath be widened to at least 2.4 metres in front of the building. This will require the removal of a No Standing Zone used by police vehicles, which will become redundant once Victoria Police vacate the building. A condition is recommended accordingly.

#### 7.1.5. Amendment C308

As noted at Section 4.2.1 of this report, Planning Scheme Amendment C308 is seriously entertained and proposes a new Schedule 1 to the Design and Development Overlay (DDO1) that would affect the subject site.

An assessment of the proposal against the key Design Requirements of the new DDO1 follows:

- The existing building has a frontage of 104 metres to Flinders Street and forms part of a block that is over 270 metres in length. The proposal retains the 1.55 metre setback from the eastern boundary at ground level; thereby retaining the 24-hour mid-block pedestrian connection that is almost entirely open-to-sky.
- The retained pedestrian connection along the eastern boundary will be improved by the inclusion of active uses along the interface.
- Recommended conditions require an upgrade of the immediately surrounding public realm, including a widening of Siddeley Street. These conditions have been discussed with the permit applicant and are not opposed.
- The proposal will improve the streetscape interface through the replacement of car parking with active uses and a substantial increase in glazing.
- The ground plane does not include any entrapment spaces.
- The building mass will remain highly modulated, with varying parapet heights and setbacks.
- The site is not within a 'Special Character Area' or Heritage Overlay. The frontage requirements therefore do not apply. The refurbished building will, nevertheless, significantly increase the active frontages at ground level.
- A façade strategy condition is recommended to require a more tactile and high quality finish to the shopfronts and facades of the ground floor and podium levels.
- As the proposal involves the refurbishment of an existing building, it is unable to meet the recommended floor-to-ceiling heights on all levels.
- Building services comprise less than 40 percent of the street frontages.
- Council's Waste Engineer has deemed the waste storage and collection arrangements to be acceptable.
- A total of 142 above ground car parking spaces would be removed as part of the proposal, with the remaining spaces partially sleeved within the podium.
- A condition is recommended to provide weather protection canopies along both street frontages (where possible).

Noting that the proposal is for the refurbishment of an existing building, it is considered that, subject to conditions, the proposal appropriately responds to Amendment C308 DDO1.

#### 7.2. Public Realm Impacts

#### 7.2.1. Wind

Section 2.3 of DDO10 includes both mandatory and preferred requirements to minimise wind effects on public areas surrounding development sites. These are set out below:

A permit <u>must</u> not be granted for buildings and works with a total building height in excess of 40 metres that would cause <u>unsafe wind conditions</u> in publicly accessible areas within a distance equal to half the longest width of the building above 40 metres in height measured from all façades, or half the total height of the building, whichever is greater as shown in Figure 1.

A permit <u>should</u> not be granted for buildings and works with a total building height in excess of 40 metres that do not achieve <u>comfortable wind</u> <u>conditions</u> in publicly accessible areas within a distance equal to half the longest width of the building above 40 metres in height measured from all façades, or half the total height of the building, whichever is greater as shown in Figure 1.

(emphasis added)

As highlighted above, the development should maintain comfortable wind conditions, and must not cause unsafe conditions within close proximity of the subject site. What constitutes 'unsafe' and 'comfortable' wind conditions is defined at Section 2.1 of DDO10.

With a maximum building width of 102 metres, the applicable area for the wind assessment is 51 metres from the site's title boundaries. The submitted Wind Tunnel Report prepared by MEL Consultants dated February 2020 is based on a study area which includes test points located up to (approximately) 64 metres from the site's title boundaries.



Figure 16 - Wind analysis study area, pursuant to DDO10

While DDO10 specifies different wind speed criteria for 'sitting', 'standing' and 'walking' areas, it does not provide any guidance on which parts of the surrounding public realm need to meet these criteria. In lieu of this, the submitted Wind Tunnel Report deems that compliance with the following would result in a 'comfortable' wind environment:

- Sitting Criterion: outdoor seating areas
- Standing Criterion: building entrances

Walking Criterion: pedestrian transit areas and terraces / balconies

The results of the wind tunnel study, which tested both the existing and proposed building configurations, is summarised in the extract at Figure 17.



Figure 17 - Wind Tunnel Study results for the proposed development

A review of the above results indicates that all areas surrounding the site would meet the safety criterion and the footpath on Siddeley Street would meet the sitting criterion. Although test locations 15 and 18 do not meet the walking criterion, the wind conditions in these locations already fall short of meeting the walking criterion under the existing conditions.

The proposed development therefore achieves the preferred wind conditions set out at Section 2.3 of DDO10, as well as the mandatory requirements. Importantly, the report does not recommend any design changes to achieve this result.

#### 7.2.2. Overshadowing

Tables 1 and 2 to DDO10 list key locations that must not be affected by additional shadow at certain times of the day and year. The following area is of particular relevance to this site:

Space	Hours between	Date(s)
The Yarra River corridor, including 15 metres from the edge of the north bank of the river to the south bank of the river	11.00am and 2.00pm	22 June

Furthermore, Clause 22.02 (Sunlight to Public Spaces) states that:

'Development should not unreasonably reduce the amenity of public spaces by casting additional shadows on any public space, public parks and gardens, public squares, major pedestrian routes including streets and lanes, open spaces associated with a place of worship and privately owned plazas accessible to the public between 11.00 am and 2.00 pm on 22 September.'

As the bulk of the additional form is proposed along the northern elevation and the overall building height is not increased, the extent of additional shadow to the surrounding public realm is restricted to a small portion of Siddeley Street and the laneway connecting Siddeley Street to the Yarra River. The neighbouring plaza to the east (which remains private land), will also receive a small amount of additional overshadowing (an additional two percent of this space on the equinox). The increase in overshadowing to the plaza is not considered to be unreasonable.

Importantly, the submitted shadow diagrams show that the proposed building will not result in any shadow to land within 15 metres of the north bank of the Yarra River on the winter solstice (refer to Figure 18).



Figure 18 - Winter Solstice Shadow Diagram, confirming that the shadow at 2pm would fall approximately 28 metres short of the north bank of the Yarra River

#### 7.3. Sustainability

Clause 22.19 (Energy, Water and Waste Efficiency) requires office developments over 5,000m<sup>2</sup> to achieve:

- A 5 star rating under a current version of Green Star Office rating tool or equivalent.
- NABERS Office Energy 5 Stars or equivalent.
- 3 points for Wat-1 credit under a current version of the Green Building Council of Australia's Green Star Office rating tool or equivalent.
- A Waste Management Plan prepared in accordance with the current version of the City of Melbourne's Guidelines for Waste Management Plans.

The proposal will significantly improve the environmental performance of the existing building through the incorporation of a range of sustainable design measures, including high performance glazing, energy efficient lighting and rainwater collection and reuse.

The Sustainable Management Plan prepared by Norman Disney Young dated February 2020 demonstrates that the building has the design potential to meet the following benchmarks:

- A 5 Star rating against the Green Star Design & As Built v1.2 tool, which represents the current relevant tool in the Green Star suite.
- 4 points for the Green Star Design & As Built v1.2 Water credit.

As noted in Section 6.8 of this report, Council's Waste Team found the submitted Waste Management Plan to be acceptable.

Although a NABERS assessment has not been provided, it is considered that achieving a 5 Star Green Star rating in a refurbished building is a commendable achievement. The ESD strategy is also supported by Council's ESD and Green Infrastructure Officer.

It is recommended that a condition on any permit issued require compliance with the recommendations of the Environmentally Sustainable Design Statement.

#### 7.4. Traffic and Engineering

#### 7.4.1. Traffic Impacts

The proposal will retain the existing vehicle access and egress arrangements, and reduce the number of car parking spaces on the site from 263 to 121. The traffic impact on the surrounding road network will therefore be significantly reduced.

Council's Traffic Engineer has nevertheless raised a number of concerns relating to pedestrian sightlines, turning movements and loading arrangements. These matters can be addressed via permit conditions.

They have also recommended that a Road Safety Audit be prepared to the satisfaction of Council. As the existing access and egress arrangements are not proposed to be varied, and the number of car parking spaces will be more than halved, this condition is considered an unreasonable and unnecessary imposition.

Similarly, Council's Infrastructure Engineer recommended conditions requiring a redesign of the crossovers and driveways, including the provision of a pedestrian refuge to the main loading dock crossover. As the proposal does not seek to vary existing vehicle access and egress arrangements, it is not considered reasonable to impose such requirements.

#### 7.4.2. Bicycle Parking

Pursuant to Clause 52.34 (Bicycle Facilities), a total of 111 bicycle parking spaces, including 85 staff spaces and 26 visitor spaces is required. As well as nine showers and change rooms for staff.

The development will provide a total of 202 bicycle parking spaces on level 1, and 24 showers and 192 lockers within separate male and female change rooms. This is well in excess of the minimum requirements and is commendable.

#### 7.4.3. Car Parking

The subject site is located within Parking Overlay – Schedule 1 where a permit is required to provide in excess of 136 spaces. The proposal involves a reduction of existing car parking from 263 to 121 spaces.

Although Council's Traffic Engineer would prefer the removal of all car parking spaces to encourage alternative modes of transport, it is considered that the substantial reduction proposed is a positive outcome.

Council's Traffic Engineer has also recommended at least one car share and one electric vehicle charging space. The proposal includes the provision of six electric vehicle charging spaces, however the recommended car share space is disputed by the permit applicant. While the provision of a car share space would be a positive outcome for the site, there is no requirement for such in the Melbourne Planning Scheme and such a condition is therefore not recommended.

#### 7.5. Other matters

#### 7.5.1. Aboriginal Cultural Heritage

The land is within an area of cultural heritage sensitivity as described in the *Aboriginal Heritage Regulations 2007*.

A 'Process List' questionnaire completed by the permit applicant indicates that a Cultural Heritage Management Plan is not required due to existing significant ground disturbance.

#### 7.5.2. Title Boundary

As noted in Section 1 of this report, the title boundaries do not reflect the existing building.

Council's Land Surveyor has recommended a condition requiring this matter to be rectified prior to the commencement of works. The permit applicant does not oppose this condition and has advised that a boundary realignment application will be submitted to Council in August of this year.

#### 7.5.3. Windows on Boundary

Portions of the proposed addition on levels 5 to 8 are built to the eastern boundary with windows along this interface.

The offices on these levels do not rely on these windows for light and outlook, and the glazing along this façade is considered to be a positive aspect of the design.

Locating windows on a boundary to private land, does however, negatively impact the equitable development of the adjoining land. This matter can be resolved via a Section 173 Agreement, which would be registered on title and require the removal of the windows should the neighbouring land be redeveloped in a manner that necessitates such.

#### 7.5.4. Pedestrian Bridge

The proposal seeks to retain the existing pedestrian bridge across Siddeley Street. It is understood that this bridge is not subject to any agreements with Council in relation to maintenance or indemnity. This matter can be resolved via a Section 173 Agreement, which would be registered on Title and set out requirements in relation to maintenance and indemnify Council from any liability.

#### 8. CONCLUSION

The proposal is generally consistent with the relevant sections of the Melbourne Planning Scheme.

#### 9. OFFICER RECOMMENDATION

That a letter be sent to the Department of Environment, Land, Water and Planning (DELWP) advising that the Melbourne City Council does not object to the proposal subject to the following conditions:

#### Amended Plans

- Prior to the commencement of development, including demolition, an electronic set of plans drawn to scale must be submitted to and approved by the Responsible Authority in consultation with Melbourne City Council, generally in accordance with the plans prepared by Cox Architects, dated 1 November 2019 and 19 March 2020 (Drawings A01-000 – A62-002 and the development schedule), but amended to show:
  - a) Integrated weather protection canopies along both street frontages.
  - b) The footpath along Siddeley Street adjacent to the subject site widened to at least 2.4 metres.
  - c) All doors to the public realm to open inwards.
  - d) Further activation and legibility to the secondary lobby entrance from the Flinders Street frontage (to the west) including:
    - i. An identifying canopy of a high quality materiality.
    - ii. The provision of a double height glazed entry.
  - e) Any changes required by Traffic Engineering at Conditions 19 to 22.
  - f) Any changes required by the Façade Strategy at Condition 5.

These amended plans must be to the satisfaction of the Responsible Authority and when approved shall be the endorsed plans of this permit.

#### Endorsed Plans

2. The development as shown on the endorsed plans must not be altered or modified without the prior written consent of the Responsible Authority.

#### Completion of Development

3. Once the development has started it must be continued and completed to the satisfaction of the Responsible Authority.

#### Glare

4. Glazing materials used on all external walls must be of a type that does not reflect more than 20% of visible light when measured at an angle of 90 degrees to the glass surface to the satisfaction of the Responsible Authority.

#### Façade strategy

5. Prior to the commencement of the development a revised Façade Strategy must be submitted to and approved by the Responsible Authority. Unless otherwise

approved by the Responsible Authority, the Façade Strategy must be generally in accordance with the endorsed plans and must show:

- a) All podium and ground level car-parking frontages to have a high quality, visually interesting and interactive interface with material and design detail that allows for adequate ventilation to podium car-parking, as well as adequate levels of visual interest, human scale and a higher level of transparency.
- b) All frontages which interface with or are close to the street level, including: service cabinets, service doors, and service frontages must demonstrate a high level of design detail and material quality where additional sleeved activation is not possible. The materials must be high quality, robust, durable, visually interesting with a high level of visual interest and human scale.
- c) All shopfront and lobby entrances must demonstrate a high level of façade depth, including: an increase in solid elements such as plinths, framing of entries and glazing, and integrated awnings over key building entries.
- d) Detailed design information regarding external materials, colours and finishes and glazing, to ensure a high quality interface is achieved to all works to the building façade as per requirements of façade strategy conditions. This must include annotated, coloured building elevations, with corresponding descriptions of all annotations as a legend on each page and a material schedule with detailed finish descriptions and images of all materials annotated on building elevations.
- e) Elevation and section details to scale of at least 1:50, illustrating a high level of detail to key components of the façade, including detailed drawings depicting service frontages and cabinets, the car-park podium frontage, awnings over key building entries, other shopfront awnings, and ground level elevations of shopfronts and lobby frontages.

#### Landscape Design

- 6. Prior to the commencement of development, including demolition, a landscape plan prepared by suitably qualified professional(s) must be submitted to the satisfaction of the Responsible Authority and in consultation with the Melbourne City Council. When approved the landscape plan will be endorsed and will then form part of the permit. The landscape plan should be generally in accordance with and derived from the Oculus Town Planning Report of 17 September 2019 and must include the following specific details for the ground level, façade and entrance lobby planting proposed in that report:
  - a) Plans and planting schedules of all proposed trees, shrubs and ground covers, including botanical names, common names, pot sizes, sizes at maturity, and quantities of each plant.
  - b) Ground level and raised planter soil, irrigation and drainage provision, including planter designs and the planting medium depths and volumes to be provided.

#### Use of Retail Tenancies

7. The Retail tenancies are not to be used for Adult sex product shop, Hotel or Tavern, except with a further permit from the Responsible Authority.

#### Land Survey

8. Prior to the commencement of the development (including demolition), all the land for the development (not including weather protection canopies and the existing pedestrian bridge) must be owned by the one entity and consolidated onto the one certificate of title to the satisfaction of the Responsible Authority.

#### Legal Agreements

#### Windows on boundary

- 9. Prior to the commencement of the development (excluding any demolition, bulk excavation, construction or carrying out of works) on the land, the owner of the land must enter into an agreement with Melbourne City Council pursuant to Section 173 of the *Planning and Environment Act 1987*. The agreement must provide the following:
  - a) The windows/openings on the boundary must be removed when the adjoining property is further developed in a manner that the Responsible Authority considers would affect these windows/openings.

The owner of the land must pay all of the Melbourne City Council's reasonable legal costs and expenses of this agreement, including preparation, execution and registration on title.

#### Projections

- 10. Prior to the commencement of the development (excluding any demolition, bulk excavation, construction or carrying out of works) on the land, the owner of the land must enter into an agreement with Melbourne City Council pursuant to Section 173 of the *Planning and Environment Act 1987*. The agreement must provide the following:
  - a) Liability and maintenance of the pedestrian bridge over Siddeley Street which is under the care and maintenance of the City of Melbourne and disclaiming any right or intention to make any claim or application relating to adverse possession of the land.
  - b) Liability and maintenance of the pedestrian bridge over Siddeley Street under the care and management of the City of Melbourne, including provision of an indemnity and comprehensive insurance cover against damage and injury resulting from the erection and use of the pedestrian bridge, to the satisfaction of the City of Melbourne.
  - c) The owner shall maintain and repair all projections and structures beyond the title boundary at their cost and to the satisfaction of the City of Melbourne.

The owner of the land must pay all of the Melbourne City Council's reasonable legal costs and expenses of this agreement, including preparation, execution and registration on title.

#### Pedestrian Link

- 11. Prior to the commencement of the development, including demolition, the owner of the land must enter into a legal agreement pursuant to Section 173 of the Planning and Environment Act 1987 with Melbourne City Council. The agreement must provide the following:
  - a) Give rights of 24 hour public access to the north-south pedestrian link located along the eastern boundary; but to remain at all times in private ownership as part of the subject land.

The owner of the land must pay all of the City of Melbourne's reasonable legal costs and expenses of this agreement, including preparation, execution and registration on title.

#### ESD

#### Implementation of Environmentally Sustainable Design (ESD)

12. Prior to occupation of the refurbished building approved under this permit, a report from the author of the endorsed ESD report, or similarly qualified persons or companies, outlining how the performance outcomes specified in the amended ESD report have been implemented must be submitted to the Responsible Authority.

The report must be to the satisfaction of the Responsible Authority and must confirm and provide sufficient evidence that all measures specified in the approved ESD report have been implemented in accordance with the relevant approved plans. The report must include all final calculations and modelling reports, commissioning and testing reports, building user guides and other supplementary materials etc. that have been produced to demonstrate compliance with the relevant targets included in the endorsed ESD report.

#### Landscape Management Plan

13. Prior to commencement of development, a Landscape Management Plan providing details of proposed maintenance regimes with provision for maintenance beyond the fifty two week period following Practical Completion and a Landscape Irrigation Performance Specification must be submitted to and approved by the Melbourne City Council. Except with the prior written consent of the Melbourne City Council the approved landscaping must be implemented prior to the occupation of the development. The landscaped area(s) must be maintained to the satisfaction of the Responsible Authority.

#### Construction management plan

14. Prior to the commencement of the development, including demolition, a detailed Construction Management Plan (CMP) must be submitted to and approved by Melbourne City Council – Construction Management Group.

The CMP must be prepared in accordance with the City of Melbourne – Construction Management Plan Guidelines and is to consider the following:

- a) Management of public access and linkages around the site during construction.
- b) Site access and traffic management (including any disruptions to adjoining vehicular and pedestrian accessways).
- c) Any works within the adjoining street network road reserves.
- d) Sediment control and site drainage.
- e) Discharge of polluted waters.
- f) Public safety and site security.
- g) Hours of construction.
- h) Control of noise, dust and soiling of roadways.
- i) Collection and disposal of building and construction waste.

j) Reasonable measures to ensure that disruption to adjacent public transport services are kept to a minimum.

#### Urban Forestry

#### Tree Protection Plan

- 15. Prior to the commencement of any works, including demolition, a Tree Protection Plan (TPP) must be provided to the satisfaction of Melbourne City Council Urban Forestry & Ecology). The TPP must identify all impacts to public trees, be in accordance with AS 4970-2009 – Protection of trees on development sites and include:
  - a) City of Melbourne asset numbers for all public trees potentially impacted (Asset numbers can be found at <a href="http://melbourneurbanforestvisual.com.au">http://melbourneurbanforestvisual.com.au</a>).
  - b) Reference to the finalised Construction and Traffic Management Plan, including any public protection gantries or scaffolding.
  - c) Site specific details of the temporary tree protection measures to be used to isolate publicly owned trees from the demolition and construction activities or details of any other tree protection measures considered necessary and appropriate to the site and associated works.
  - d) Specific details of any special construction methodologies to be used within the Tree Protection Zone of any public trees. These must be provided for any utility connections or civil engineering works.
  - e) Full specifications of any pruning required to public trees with reference to marked images.
  - f) Any special arrangements required to allow ongoing maintenance of public trees for the duration of the development.
  - g) Details of the frequency of the Project Arborist monitoring visits, interim reporting periods and final completion report (necessary for bond release).

#### Public Tree Protection

- 16. All works (including demolition), within the Tree Protection Zone of public trees must be undertaken in accordance with the endorsed Tree Protection Plan and supervised by a suitably qualified Arborist where identified in the report, except with the further written consent of the Melbourne City Council.
- 17. Following the approval of a Tree Protection Plan (TPP) a bank guarantee equivalent to the combined environmental and amenity values of public trees that may be affected by the development will be held against the TPP for the duration of construction activities. The bond amount will be calculated by Council and provided to the applicant/developer/owner of the site. Should any tree be adversely impacted on, the City of Melbourne will be compensated for any loss of amenity, ecological services or amelioration works incurred.
- 18. In the event that public trees are proposed for removal at any stage of the development, the applicant must submit plans for the approval of council that show replacement and or additional tree plots of a larger size and increased soil volume than currently exists in the street frontages adjacent to the development.

#### Traffic Engineering

- 19. Prior to the commencement of works (including demolition), a revised Traffic Report shall be prepared and submitted to the satisfaction of the City of Melbourne. The report must be generally in accordance with the report prepared by Traffix, dated 15 December 2019, but modified to include:
  - a) A detailed assessment of all turning movements at both access vehicle points.
  - b) Swept Path Diagrams for all vehicle access points, clearly showing all vehicles accessing / egressing the carpark and loading bays, with the required clearances and all infrastructure (including kerbs, poles, parking bays, etc.) shown on dimensioned functional layout plans.
- 20. Flashing / warning devices must be installed to alert pedestrians of exiting vehicles.
- 21. A convex mirror must be installed for drivers to see pedestrians as they exit the carpark.
- 22. A narrow road hump must be provided close to the carpark exit points, to facilitate low speed for exiting vehicles.
- 23. All roller doors to the street frontages are to remain open between 7am and 10am, Monday to Friday.
- 24. Unless with the prior written consent of the Responsible Authority, the loading and unloading of vehicles and delivery of goods to and from the site must at all times take place within the boundaries of the site to the satisfaction of the Responsible Authority.

#### **Civil Design**

- 25. All projections over the street alignment must conform to Building Regulations 2018, Part 6, Sections 98 to 110 as appropriate. Reference may be made to the City of Melbourne's Road Encroachment Operational Guidelines with respect to projections impacting on street trees and clearances from face/back of kerb.
- 26. The tram pole along Flinders Street, which is less than one metre from the building line, shall be relocated to the satisfaction of the Responsible Authority.
- 27. The two security bollards located within the footpath along Flinders Street shall be removed to the satisfaction of the City of Melbourne.
- 28. Drainage connection underground

Prior to the commencement of the development, a stormwater drainage system, incorporating integrated water management design principles, must be submitted to and approved by the City of Melbourne. This system must be constructed prior to the occupation of the development and provision made to connect this system to the City of Melbourne's underground stormwater drainage system.

29. Demolish and construct access

Prior to the commencement of the use/occupation of the development, all necessary vehicle crossings must be constructed and all unnecessary vehicle crossings must be demolished and the footpath, kerb and channel reconstructed, in accordance with plans and specifications first approved by the City of Melbourne.

#### 30. Road

The road adjoining the site along Siddeley Street must be reconstructed together with associated works including widening and construction of footpath in sawn bluestone, reconstruction of median islands, the provision of drainage, street lighting and modification of services as necessary at the cost of the developer, in accordance with plans and specifications first approved by the City of Melbourne.

#### 31. Sawn bluestone footpath

The footpath adjoining the site along Flinders Street must be reconstructed in sawn bluestone together with associated works including the renewal of kerb and channel, removal of security bollards and modification/relocation of services as necessary at the cost of the developer, in accordance with plans and specifications first approved by the City of Melbourne.

32. Street levels not to be altered

Existing street levels in roads adjoining the site must not be altered for the purpose of constructing new vehicle crossings or pedestrian entrances without first obtaining approval from the City of Melbourne.

33. Existing street lighting not altered without approval

All street lighting assets temporarily removed or altered to facilitate construction works shall be reinstated once the need for removal or alteration has been ceased. Existing public street lighting must not be altered without first obtaining the written approval of the City of Melbourne.

34. Public lighting

Prior to the commencement of the development, excluding demolition and any other works as may be agreed with the City of Melbourne, a lighting plan must be prepared to the satisfaction of Council. The lighting plan should be generally consistent with Council's Lighting Strategy, and include the review of public lighting in Siddeley Street. The lighting works must be undertaken prior to the commencement of the use, in accordance with plans and specifications first approved by the City of Melbourne.

#### Waste Management

35. The waste storage and collection arrangements must be in accordance with the Waste Management Plan (WMP) prepared by Impact dated 27 February 2020.

The WMP must not be altered without prior consent of the City of Melbourne – Waste and Recycling.

#### 3D digital model

36. Prior to the commencement of the development, excluding demolition, a 3D digital model of the development must be submitted to and approved by the Responsible Authority in consultation with Melbourne City Council.

The model should be prepared having regard to the Advisory Note – 3D Digital Modelling Melbourne City Council. Digital Models provided to Council and other government organisations for planning purposes. The Council may also derive a representation of the model which is suitable for viewing and use within its own 3D modelling environment. In the event that substantial modifications are made

to the building envelope, a revised 3D digital model must be submitted to and approved by the Responsible Authority.

#### Permit expiry

37. This permit will expire if one of the following circumstances applies:

- a) The development is not started within three years of the date of this permit.
- b) The development is not completed within five years of the date of this permit.

The Responsible Authority may extend the permit if a request is made in writing before the permit expires, or within six months afterwards. The Responsible Authority may extend the time for completion of the development if a request is made in writing within 12 months after the permit expires and the development started lawfully before the permit expired.

#### NOTES

#### Land Survey

 The proposal projects over Siddeley Street (south western portion of the building). Siddeley Street is a Government Road and may require a Land Tenure Authorisation.

#### **Street Trees**

- In accordance with the Tree Retention and Removal Policy a bank guarantee must be:
  - 1. Issued to City of Melbourne, ABN: 55 370 219 287.
  - 2. From a recognised Australian bank.
  - 3. Unconditional (i.e. no end date)
  - 4. Executed (i.e. signed and dated with the bank stamp)
- Please note that insurance bonds are not accepted by the City of Melbourne. An acceptable bank guarantee is to be supplied to Council House 2, to a representative from Council's Urban Forest and Ecology Team. Please email trees@melbourne.vic.gov.au to arrange a suitable time for the bank guarantee to be received. A receipt will be provided at this time.
- At the time of lodgement of the bank guarantee the completed Project Arborist Confirmation Form must be provided. On completion of the works the bank guarantee will only be released when evidence is provided of Project Arborist supervision throughout the works and a final completion report confirms that the health of the subject public trees has not been compromised.
- Approval for any tree removal is subject to the Tree Retention and Removal Policy, Council's Delegations Policy and requirements for public notification, and a briefing paper to Councillors. It should be noted that certain tree removals including but not limited significant or controversial tree removals, may be subject to decision by Council or a Committee of Council.
- All costs in connection with the removal and replacement of public trees, including any payment for the amenity and ecological services value of a tree to be removed, must be met by the applicant/developer/owner of the site. The

costs of these works will be provided and must be agreed to before council remove the subject trees.

#### **Civil Design**

 All necessary approvals and permits are to be first obtained from the City of Melbourne and the works performed to the satisfaction of the Responsible Authority.